



# CITY OF HOUSTON

**John Whitmire**

Mayor



HOUSTON AIRPORT SYSTEM

George Bush Intercontinental ~ William P. Hobby ~ Ellington Airport

Jim Szczesniak  
Director of Aviation

March 13, 2024

**SUBJECT:** Addendum No. 4

**REFERENCE:** Invitation To Bid (ITB) for IAH Terminal D Conveyance Replacement at George Bush Intercontinental Airport (IAH), Solicitation No. HOA-CONREP-2024-015; Project No.1028

To: All Prospective Bidders:

This Addendum is issued for the following reasons:

## I. RESPOND TO QUESTIONS.

1. **Question:** I noticed that you all are requesting the modernization of this project's three (3) elevators. Would you also accept a rip-out and replacement of these if the elevators can fit?

**Response:** The conveyance equipment will be replaced with similar equipment, Re: elevator specification 14250.

2. **Question:** Does HAS happen to have the as-built drawings for the existing escalators, moving walkways, and elevators? That would be very helpful in determining what structural modifications need to be made.

**Response:** The successful bidder will receive a complete set of existing Terminal D legacy drawings.

3. **Question:** I need clarification on removing the glazing at three terminal points. The glazing will need to be removed and not replaced for the work at that site for 4-6 months due to the lack of laydown room for material and access to dumpsters on AOA. How long can the glazing be removed, and what is the procedure for temporarily covering the openings?

**Response:** Should a window be removed and not replaced, an alternative to the glazing shall protect the opening from the weather and passenger entry or exit. If wood is used, it shall be fire retardant. Oversized equipment delivery at designated locations identified on the walk can be done but requires a temporary amendment to our ASP and must be submitted by the PM to the TSA FSD's Office for approval 45 days before construction. Badge persons will be required on

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both sides during operation until the window is reinstalled. During multiple use times, the windows must be secured with a solid barricade each time until they are completed and the window is permanently reinstalled. This must be spelled out in the project's Security Plan (SP).

Material must also be cleared by K9 sweep before entering the sterile area. Access to the Terminal C loading dock utilizing the C13 Freight elevator is available for deliveries and waste removal. Permission has been granted for three additional locations where glazing can be removed for these tasks. An additional location may be approved, but the Contractor shall be required to request an amendment to the ASP with a security plan for TSA to review and approve.

4. **Question:** The removed glazing access point will be on the AOA at two points. We need clarification on what the access to the AOA will look like. Will we have unlimited access to the ramps before the access point? Will we be allowed to have dumpsters on the AOA? Will we have 50-foot-long delivery trucks on the AOA and ramps?

**Response:** Dumpsters are not permitted on the airside, and any work impacting the airside may only occur during limited hours. If debris has to be removed via the airside, it must be hauled off nightly. HAS will work with the contractor on routing, but please advise on the plan. Dumpster locations will be coordinated with the awarded Contractor at the Pre-Construction Meeting.

5. **Question:** Due to this project mainly consisting of night work, will BSG be doing the inspections, or will this need to be done by a 3rd party inspection company?

**Response:** Yes, the contractor needs to submit a request to BSG and get approval before starting the work.

6. **Question:** The security cameras at the top of the escalators and the beginning and end of each walk will need to be relocated for the project's duration at the sites. Will Airport security be the ones to relocate, or will it be the GC?

**Response:** Yes, the contractor will move the camera, and security coordination will be required 45 days before the relocation.

7. **Question:** Plans call for temp containment walls of framing and sheetrock due to the lack of space around some sites. We would need to use movable walls as an alternate option to give the site more room to work; what alternate will be approved?

**Response:** Most temporary wall systems on the market would be acceptable. We have seen Starc systems used in a recent project. Swiftwall.com offers several acceptable options.

8. **Question:** All material movement activities will be performed at night (10 PM - 6 AM). Please advise if all OTHER work on the project must also be performed between 10 PM and 6 AM. NOTE: the first several weeks of each escalator and moving walk installation will be loud. Still, noise during the remainder of the installation window of each unit will be sporadic, not constant. Performing all work at night will be expensive (Escalator installers are paid double time for all after-hours work) and inefficient – especially considering the long duration of the project overall.

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However - we do not want to create a situation of constant friction regarding noise throughout the project, as we cannot guarantee what level of noise will be considered disruptive to the owner. Please clarify the work schedule we should anticipate accordingly. This will need more discussion. Performing all work at night will be expensive (elevator installers are paid double time for all after-hours work) and inefficient – especially considering the long duration of the project overall.

**Response:** Excessive noise must be between 2200 and 0600 hours, especially during the first several weeks of each escalator and moving walk installation. HAS cannot guarantee what level of noise will be considered disruptive. We can work with the vendor for each area at other times, maybe into the first shift. Daily construction activities between 1200 and 2000 are primarily off-limits as this is the peak activity for international flights. Any impact tools that cause excessive floor vibrations, pounding, dust, etc., must be on their night shift(2200-0600). Working regular hours for work that does not create excessive noise, dust, or disturbance to passenger movement may be accomplished from 0600 to 2200.

9. **Question:** Is any escalator, moving walk, or elevator material to be saved for the owner's use? If so, please advise on the specifics and where to locate this.

**Response:** No.

10. **Question:** What is the maximum number of units that can be removed from service for modernization at any given time? NOTE: This will impact project duration, and workforce availability will limit capabilities.

**Response:** The sequence of elevators that can be modernized simultaneously are as follows:

1. Group 1: DMSW-1, DMSW-2, DMSW-3
2. Group 2: DE-15, DE-16, DE-17
3. Group 3: DMSW-4, DMSW-6, DMSW-7

11. **Question:** Escalators: The specifications note that “heavy-duty public transportation escalators are to be provided.” Equipment required to meet this load profile should be specified with a rated step load of 264 lbs. per step. This will drive all associated components to meet this “public transportation” standard. This specific step-loading requirement should be added to the specification.

**Response:** The requirements remain as stated.

12. **Question:** Escalators: As noted, is the only attic stock to be provided—six steps and three sets of comb segments per escalator—or is another needed? Please state it.

**Response:** If another is needed, HAS will request it later.

13. **Question:** Escalators: Please confirm the warranty service maintenance to be provided is “for one year commencing on the date of final acceptance” PER UNIT - not for the job in total.

**Response:** The one-year period will begin with the transfer to HAS for passenger use per unit.

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14. **Question:** Escalators: Please confirm that the owner will provide all required remote monitoring equipment and that we are to provide a contact interface only.

**Response:** The owner shall provide the remote monitoring.

15. **Question:** Escalators: An interior inspection of the existing unit drip pans was not performed during the walk-through, so their condition could not be determined. This material is usually REUSED for the modernization process. Please confirm that we can anticipate the drip pans are in reusable condition, and the cost adjustments will be allowed if they are later determined to be damaged.

**Response:** Reusing the drip pans is acceptable; the Design team anticipates the drip pans are in reusable condition, and the cost adjustments will be allowed if they are later determined to be damaged after exposure of drip pans during construction.

16. **Question:** Walks: As noted, is the only attic stock to be provided six pallets and three sets of comb segments per escalator? If another is needed, please state it.

**Response:** If another is needed, HAS will request it later.

17. **Question:** Walks: The requirement for a “truss air conditioner” should be removed from each unit’s specification.

**Response:** Provide an allowable operating temperature for the units without AC.

18. **Question:** Walks: The stainless steel type for the walks should be modified to read “type 304”, not 316. 316 is reserved for outdoor or corrosive environments only.

**Response:** Provide per specifications.

19. **Question:** Walks: Sleep mode is not applicable for walks and should be removed from the specification. (Page 12)

**Response:** Sleep mode is an allowed feature for walks. The requirement remains as stated.

20. **Question:** Walks: Please confirm that the owner will provide all remote monitoring equipment required and that we are to provide a contact interface only.

**Response:** Please refer to the response provided in Question #14.

21. **Question:** Walks: An interior inspection of the existing unit drip pans was not performed during the walk-through, so their condition could not be determined. This material is usually REUSED for the modernization process. Please confirm that we can anticipate the drip pans are in reusable condition, and the cost adjustments will be allowed if they are later determined to be damaged.



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**Response:** Please refer to the response provided in Question #15.

22. **Question:** Walks: Walk pallets must have sound-reducing material. This is typically not offered on walks and would be EXTREMELY expensive. We recommend that this be removed from the specifications.

**Response:** Provide decibel levels without reducing the materials.

23. **Question:** Walks: Cladding on the exterior of the walk is not provided by the walk manufacturer and should be removed from this specification section (page 14) and added to the work of the General Contractor if required.

**Response:** The contractor is responsible for all work.

24. **Question:** Walks: The fault finders for the walks (page 15) should be noted to be provided at the DRIVE END of the walk only. We recommend updating the specifications accordingly.

**Response:** Both ends are required.

25. **Question:** Elevators: Remote Monitoring and Diagnostics: Swift Monitoring only. This monitoring system is already in place on the current elevator equipment. Can you confirm that airport personnel will reuse and install the monitoring system? Or a contractor of the airport's choosing?

**Response:** Please refer to the response provided in Question #14.

26. **Question:** Elevators: Car Equipment, sills. Please confirm that all elevator Cab sills, D1, D7, and D8, will be replaced with nickel silver sills.

**Response:** Yes.

27. **Question:** Elevators: Please confirm Montgomery "C" numbers for D1, D7, D8.

**Response:** Yes. D1 CP-63559, D7 CP-63565, D8 CP-63569

28. **Question:** Elevators: ASME 17.1 (2019) Safety Code for elevators and escalators. Can IAH support the 2-way visual communications needs?

**Response:** No. The current Code does not require two-way visual communication. If HAS desires this feature, then they will need to confirm their ability to provide the monitoring.

29. **Question:** Elevators: Car Enclosure, interior cab upgrades. Please provide allowance/budget and detailed architectural drawings.

**Response:** Refer to Specifications, Section 14250 Elevator Modernization, Paragraph 2.02.F for elevator material and finishes.

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30. **Question:** Elevators: Delivery / Storage / Staging / and Dumpsters locations need to be confirmed for all units on the drawings. This will be critical to finalizing labor and expenses costs for this project. Please confirm the plan we should anticipate for each unit.

**Response:** Please refer to the response provided in Question #4.

31. **Question:** Elevators: The drawings (A3-04) show a large storage area adjacent to walk DMSW-4. Please confirm that this storage space will be available for the duration of the project.

**Response:** No issue between the unit and the north wall, using floor-to-ceiling dust wall.

32. **Question:** Elevators: During the walkthrough, it was discussed that the space on either side of walk DMSW-6 would be available for material storage during the installation of this unit. Please confirm that barricades can be built to store all delivered material for this unit in this space.

**Response:** Please refer to the response provided in Question #31.

33. **Question:** Escalators DE15 and 17—please confirm we can anticipate using the circle drive area on the ground floor for both deliveries, connex storage, and dumpsters. If not, please designate an alternate location.

**Response:** The bus station must remain in operation at D18. For IROPS, this is the only location where the COBUS unloads. This means the stairs inside the bus station and the elevator should be available for pax use.

34. **Question:** Elevators D7, D8, D1 – please confirm we can anticipate using the circle drive area on the ground floor for both deliveries / connex storage / and dumpsters. If not – please designate an alternate location.

**Response:** Please refer to the response provided in Question #33.

35. **Question:** Escalators DE13 and DE16 – please confirm that we can anticipate bringing material in at the gate level through a window opening near the top of escalator DE13. NOTE – this is approximately 1300 feet away from DE16. Please also confirm that this temporary opening through the window at the gate level can remain accessible until all old material has been removed and all NEW material has been taken to the barricaded space provided around each unit to be installed. This time frame will ultimately depend on how much space can be provided at each unit to receive and store the NEW material, but it should be anticipated to be an extended amount of time.

**Response:** The contractor will be awarded the opportunity to explain how security will be maintained and how the equipment will be routed to the location. The Contractor will be permitted to use a lift or crane during work at night only.

Access to the Terminal C loading dock, utilizing the C13 freight elevator, is available for deliveries and waste removal. Permission has been granted for three additional locations where glazing can be removed for these tasks. Additional locations may be approved, but the contractor shall

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coordinate with HAS Security to provide TSA with a 45-day notice to review and approve all ASP Amendments before the project starts. Dumpsters are not permitted on the airside, and any work impacting the airside may only occur during limited hours. If debris has to be removed via airside, it must be hauled off nightly. Should a window be removed and not replaced, an alternative to the glazing shall protect the opening from the weather passenger entry or exit the opening. If wood is used, it shall be fire retardant.

36. **Question:** Escalators DE8 and DE9 deliveries/storage and dumpster locations will be particularly problematic. The only two reasonable delivery locations discussed at the walk-through were at the opposite end of the train tunnel or from the main drop-off lane in front of the terminal, where windows would have to be removed and material lowered from the sidewalk in front of the terminal to the top landing of these units. The option for delivery at the front of the terminal is not addressed in the drawings and would need to be added as an acceptable option for the contractors to price the window removal / etc. If this is allowed, please add it to the contract documents or advise us on the best option we should use. Also, these units' material staging and dumpster locations must be confirmed, as these will be very important for labor calculations.

**Response:** Please refer to the response provided in Question #35. Entry Point #3 in the FIS area is planned for equipment delivery for DE8 and DE9 through the front entrance. See Sheet A2-01. However, oversized equipment delivery or additional locations identified on the walk can be done. Still, it will require a temporary amendment to the approved ASP, which must be submitted by the PM to the TSA FSD's Office for approval 45 days before construction. See Detail 1 on Sheet A3-04 for staging areas. Dumpsters are not permitted on the airside. Dumpster locations will be coordinated with the awarded Contractor at the Pre-Construction Meeting.

37. **Question:** Walks DMSW 4, 6, and 7 – please confirm we can anticipate either bringing material in on Freight Elevator C13 (approximately 1000 feet away) or through the window at the gate level adjacent to escalator DE13. The window approach would be far more efficient and convenient, but this window would need to be out for an EXTENDED period based upon the installation time frame and the amount of material that would need to be moved for these three walks. Please confirm that storage and dumpsters can also be located adjacent to these locations for these units.

**Response:** Please refer to the response provided in Question #35.

38. **Question:** Walks DMSW 1, 2, and 3 – please confirm we can anticipate bringing all material in through the window removal location shown in detail two on A2-02. This is approximately 200 feet from the end of walk #3 and approximately 1300 feet from the center of walks 1 and 2. This will be problematic based on the amount of material that will need to be removed and brought in, so please advise if there is a better alternative than what was not discussed at the walk-through.

**Response:** Please refer to the response provided in Question #35.

39. **Question:** Will flooring protection be required from each unit's designated entry point to the barricades? If so, please specify the material to use and any special considerations to make.

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**Response:** Review the equipment's weight and the capacity of the transportation equipment used to transport it. The contractor is responsible for any damage to the building during the equipment's movement. Floor protection is only required if the contractor needs it to protect the floor because the means of transport exceeds the floor's capacity to support it without damage.

40. **Question:** Please confirm that delivery path floor protection can remain in place for as long as needed for each unit. Please remember that many delivery paths are incredibly long, and the limited space allowed for installation barricades around each unit will force most material to be stored remotely. Daily removal of this protection is not feasible for these reasons.

**Response:** Please refer to the response provided in Question #39.

41. **Question:** Please advise specifics for dog sweeps of delivered material. Will all boxes and crates need to be opened? Will material be removed from delivery flatbeds before an inspection can occur? We need to be able to plan for the amount of time this will take per delivery (53- 53-foot-long flatbed). (Escalators will come on one truck per unit, / Walks will be delivered on approximately three trucks each.)

**Response:** Regarding specifics on dog sweeps. A K9 sweep must clear any equipment with concealed compartments before entering the sterile area. That must be coordinated with HAS Security at least one business day in advance. The contractor must provide the date/time/location/onsite POC for the sweep. The latest time slot that a K9 can be scheduled for is 1830. Coordinate early with HAS Security to work out logistics for any sweeps not at the dock (window locations).

42. **Question:** As the barricaded space for many of the units will be very restricted, can we anticipate storage containers and dumpsters can be kept in the egress locations as noted on the drawings? If not – where?

**Response:** Please refer to the response provided in Question #4.

43. **Question:** What special provisions will be required for dumpsters to be located on the tarmac?

**Response:** Please refer to the response provided in Question #4.

44. **Question:** Can electrical ground wires be modernized at the disconnect switch at each unit? If not – a notation should be made for this to be added to each unit by the General Contractor.

**Response:** The Contractor is responsible for all electrical grounding work in each unit being modernized that is brought to the 2023 NEC Code.

45. **Question:** The drawings should note that slight modifications to the newel end cladding (3 locations at the upper and lower ends of escalators DE8 and DE9) will be required and that the general contractor will perform them.

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**Response:** The Contractor is responsible for modernized work in each unit and shall be responsible for adjusting existing conditions to complete the work.

46. **Question:** Notation should be made on the drawings that minor wall/paint repair may be required where the new escalator decking butts up against adjacent wall finishes.

**Response:** The Contractor is responsible for all modernization work in each unit and for protecting adjacent finishes. If construction operations damage adjacent walls, the Contractor is responsible for repairing and painting surfaces to match the existing ones.

47. **Question:** Once the new walk decking has been installed, all exterior cladding on the outside of each walk will need to be removed and replaced. This requirement should be noted in the plans and specifications so that the GC can cover the cost.

**Response:** Please refer to the response provided in Question #45.

48. **Question:** The auxiliary decking between walks 4 and 7 must be modified based on the final location of the NEW DECKING provided for each unit. The new decking will be approximately 2 inches higher off the floor than the existing one. Notations should be added to the drawings for the General Contractor to include this work on their scope. Details of the new final condition should also be added to the contract documents.

**Response:** Please refer to the response provided in Question #45.

49. **Question:** All barricades and floor protection inside and outside work and storage locations should be noted as being provided by the general contractor. A specific list of floor protection types should also be provided. The path and distance of the floor protection required for each unit should also be added to the drawings for the general contractor's cost.

**Response:** Please refer to the response provided in Question #39.

50. **Question:** Please confirm that all floor protection can remain between the point of egress and the barricades for each unit can stay in place as long as needed. Placing this protection down and removing it on a nightly basis is not reasonable, given the distance that will need to be traveled and the amount of material that will need to be moved.

**Response:** Please refer to the response provided in Question #39.

51. **Question:** NOTE—barricades around ALL WALKS will require a minimum of 5'0" clear from the interior wall of the barricade to the nearest obstruction to allow for internal staging of material and to provide an efficient and safe workspace. Drawing notation should be made.

**Response:** The barricade layout, as noted in the drawing, provides for passenger traffic flow as required by code for an occupied and functional airport. The airport remains operational 365 days a year.

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52. **Question:** The general contractor should note the removal of windows and the provision of temporary entrance protection and ramps as required on the drawings. Remember that these windows will need to be out for an EXTENDED amount of time due to the amount of material that will need to be moved, and the temporary enclosure will need to be easily removable on a nightly basis.

**Response:** Please refer to the response provided in Question #35.

53. **Question:** The amount of space available at all egress locations for storage containers and dumpsters should be confirmed on the drawings.

**Response:** Access to the Terminal C loading dock, utilizing the C13 Freight elevator, is available for deliveries and waste removal. Permission has been granted for three additional locations where glazing can be removed for these tasks. Additional locations may be approved, but the Contractor will require an amendment to the ASP with a security plan for TSA to review and approve. Before the project, the contractor shall coordinate with HAS Security to provide TSA with a 45-day notice to review all ASP Amendments. Dumpsters are not permitted on the airside, and any work impacting the airside may only occur during limited hours. If debris has to be removed via airside, it must be hauled off nightly. HAS can work with you on routing, but please advise on the plan. Should a window be removed and not replaced, an alternative to the glazing shall protect the opening from the weather passenger entry or exit the opening. If wood is used, it shall be fire retardant. Dumpster locations will be coordinated with the awarded Contractor at the Pre-Construction Meeting.

54. **Question:** Headroom clearance at both the upper and lower ends of each escalator and walk will need to be a min of 7'6" clear for the escalators and 8'0" apparent on the walks to provide space for scaffolding and hoisting equipment necessary to demo and install equipment. Any obstruction, such as drop ceilings/lighting/sprinklers/conduit/security cameras / etc., must be removed and relocated to provide room for the installation. A notation should be made on the drawings that the general contractor will perform this work.

**Response:** Please refer to the response provided in Question #45.

55. **Question:** Walk #3 MAY have a condition that local AHJ may consider a code violation, which may need to be corrected before turnover for owner usage following the modernization. The wall at the end of the unit infringes upon what the code defines as the required safety zone at the end of the unit. This should be confirmed with the AHJ inspector to verify if modifications to this wall will be necessary. As applicable, notations should be made to the drawings to reflect the scope of work accordingly.

**Response:** This area is in an existing condition; the work will not modify any wall locations or change the current clearances.

56. **Question:** Assuming the escalator's step loading is noted in the following addendum to be required at 264 lbs. per step, the following will be the 3 phase electrical requirements for each unit. Please verify that this service will be provided to each unit. Controller over-current protection

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13.4 HP 10.0kw Full load Running amps 18.0 Power subtlly 480v Current rating type RK1 Time Delay fuse 25-30 amps Current rathing inverse time circuit breaker 25-45 amps.

**Response:** ESC 8/9 was designed based on a 20-hp motor with a 50-amp breaker. ESC 13/15/16/17 were created based on a 15-hp motor with a 40-amp breaker. Based on those numbers provided, no additional load will be added, but breaker sizes would need to be adjusted depending on the equipment purchased.

When issued, the Addendum shall automatically become part of the solicitation documents and supersede any previous specification(s) and provision(s) in conflict with the Addendum. The supplement will be incorporated into the Agreement as applicable. The bidder(s) is responsible for ensuring that all such letter(s) have been obtained. By submitting a bid on this project, bidder(s) shall be deemed to have received all Addenda and incorporated them into their bid.

If you need further clarification regarding this solicitation, please email Ola Alhammami, Senior Procurement Specialist, at [ola.alhammami@houstontx.gov](mailto:ola.alhammami@houstontx.gov).

DS



DocuSigned by:



02232028DE90414...

Cathy Vander Plaats  
Aviation Procurement Officer  
Houston Airport System

CVP/oa

cc: Alfredo Oracion  
Solicitation File

Attachments: S2-02- Terminal Baggage Ticketing Levels  
S2-22- Partial Framing Plans  
S2-23- Partial Framing Plans

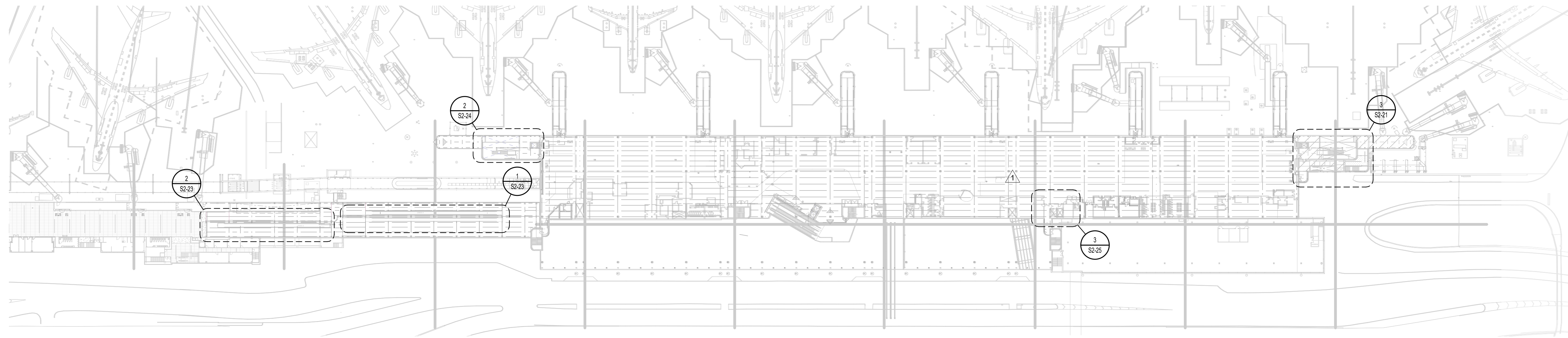


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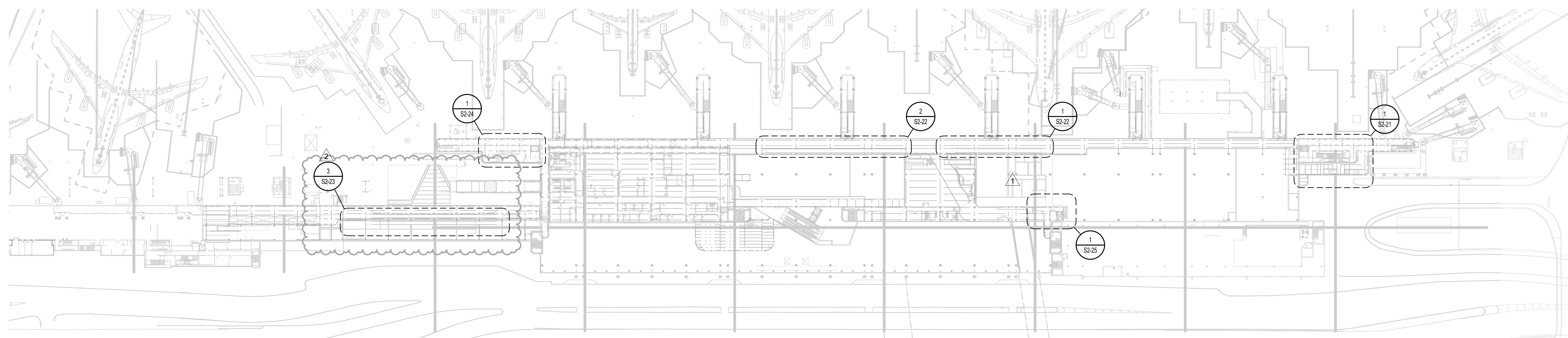
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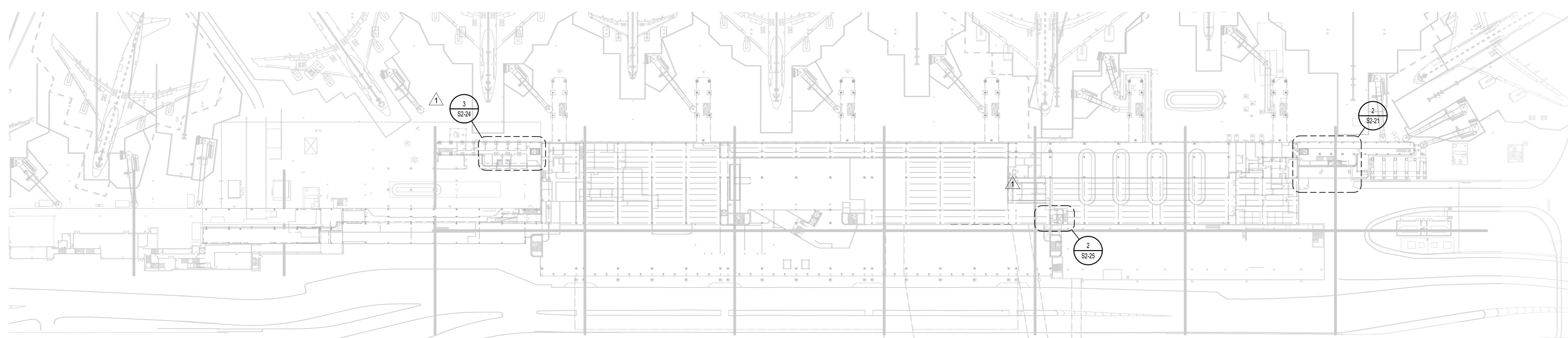
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**3** TERMINAL D INTERNATIONAL DEPARTURES  
SCALE: 1" = 75'-0"



**2** TERMINAL D INTERNATIONAL ARRIVALS  
SCALE: 1" = 75'-0"



**1** TERMINAL D APRON  
SCALE: 1" = 75'-0"



IAH TERMINAL D CONVEYANCE REPLACEMENT -  
2800 N TERMINAL RD, HOUSTON, TX 77032  
**IAH TERMINAL D CONVEYANCE  
REPLACEMENT**  
C.I.P. No. A.I.P. No.  
C.O.H. No. D.O.A. No.



DESIGNER PROJECT No.: 23-08  
PROJECT STATUS: ISSUED FOR ADDENDUM 03

**REVISIONS**

No.	DESCRIPTION	DATE	BY
0	ISSUE FOR PERMIT	05/16/2023	
1	PERMIT REVISION	11/28/2023	
2	ISSUE FOR ADDEN 03	02/28/2024	

DESIGNER: K. PATEL  
DRAWN BY: R. RODRIGUEZ  
CHECKED BY: E. ROGERS  
ISSUE DATE: 11/28/23  
APPROVED BY: E. ROGERS  
APPROVAL DATE:

DIRECTOR  
of  
HOUSTON AIRPORT SYSTEM

Review/ Approval Category

**IFP**  
ISSUED FOR PERMIT

Henderson Rogers  
Structural Engineers, LLC  
TBPE Firm Registration No. 8755

**TERMINAL "D"**  
SHEET NAME: TERMINAL BAGGAGE AND TICKETING LEVELS

SHEET No. S2-02 SCALE: 1" = 75'-0"

FILE PATH: C:\Users\John.Moore\OneDrive\Documents\230391\_IAH Terminal D Conveyance\_R20\_jmoore\K2YP.rvt

HAS FILE:

PLOT DATE:  
DOA DWG FILE:  
OLD DOA No.:

SHEET SIZE: 22"x34" ANSI-D

A

B

C

D

3

3

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2

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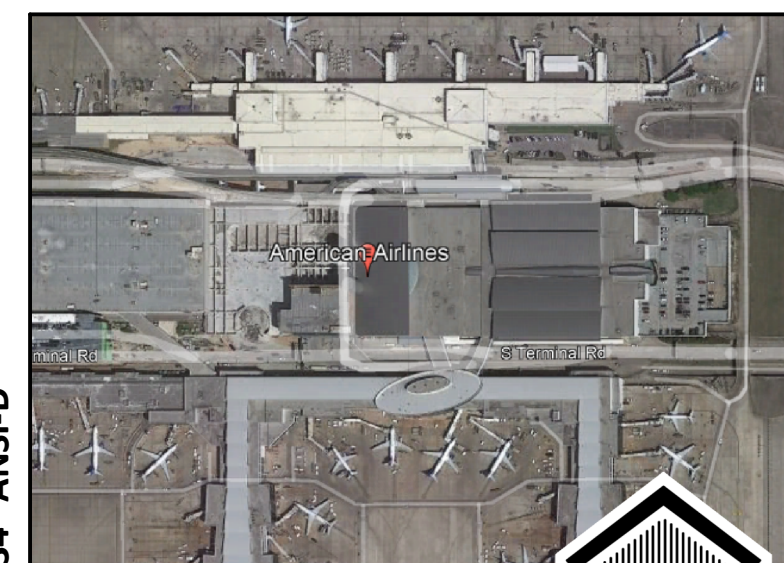
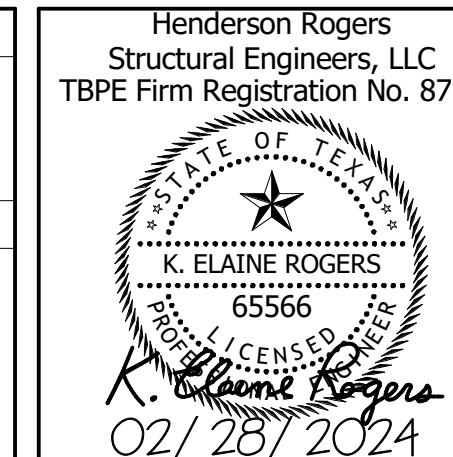
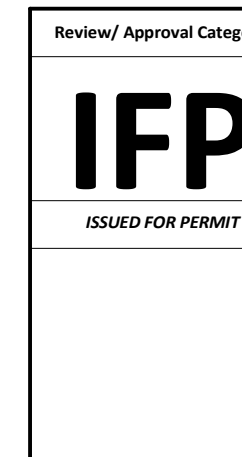


**REVISIONS**

No.	DESCRIPTION	DATE	BY
0	ISSUE FOR PERMIT	05/16/2023	
1	PERMIT REVISION	11/28/2023	
2	ISSUE FOR ADDEN 03	02/28/2024	

DESIGNER: K. PATEL  
DRAWN BY: R. RODRIGUEZ  
CHECKED BY: E. ROGERS  
ISSUE DATE: 05/16/2023  
APPROVED BY: E. ROGERS  
APPROVAL DATE:

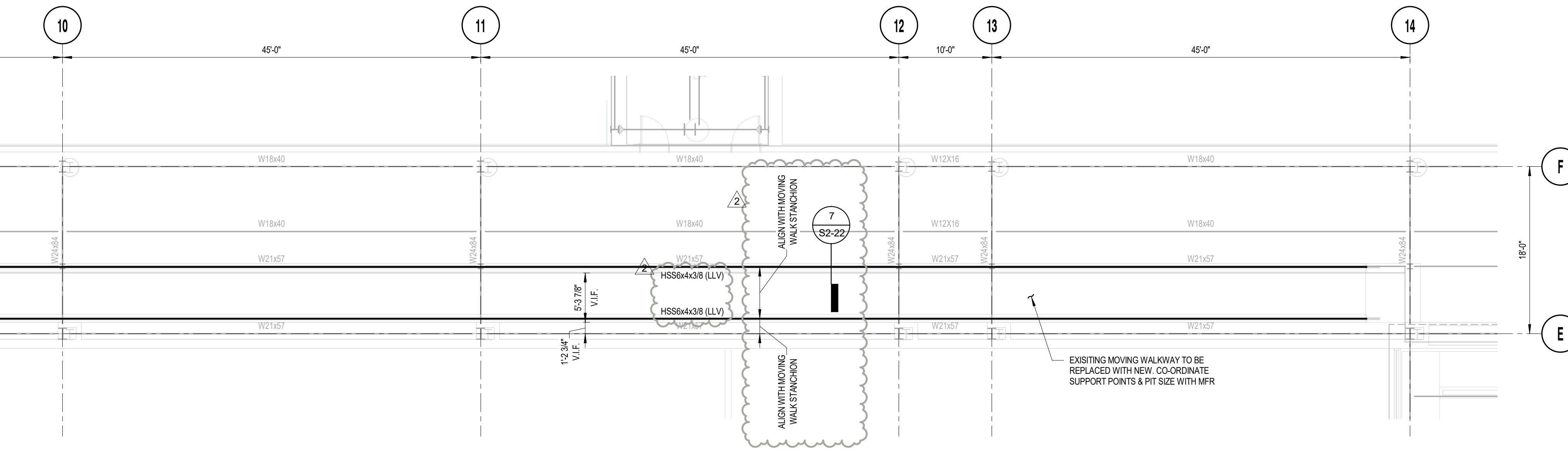
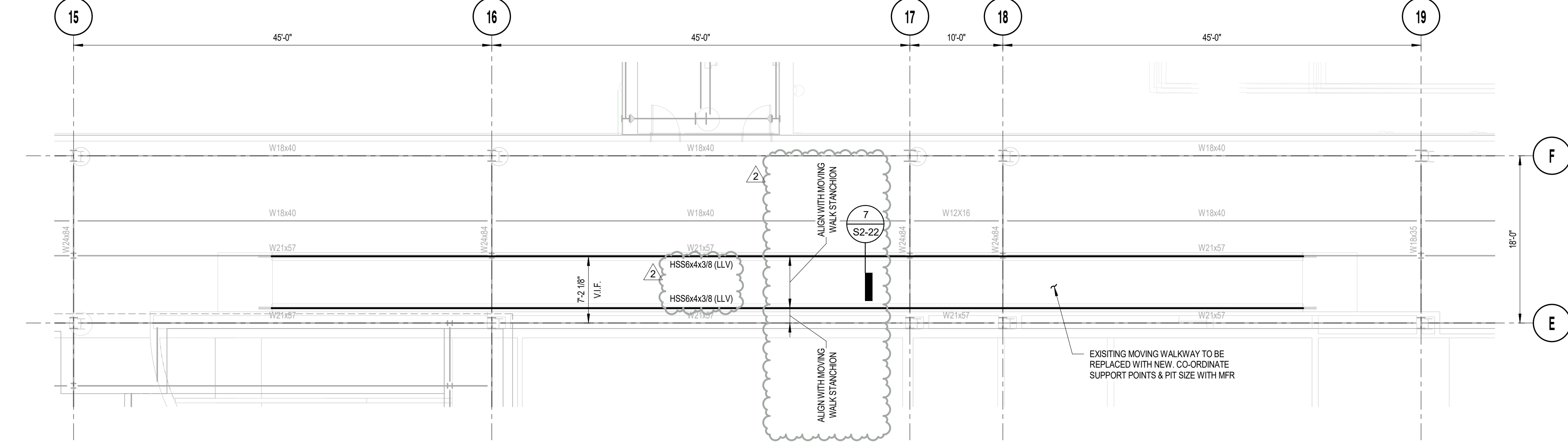
DIRECTOR  
of  
HOUSTON AIRPORT SYSTEM



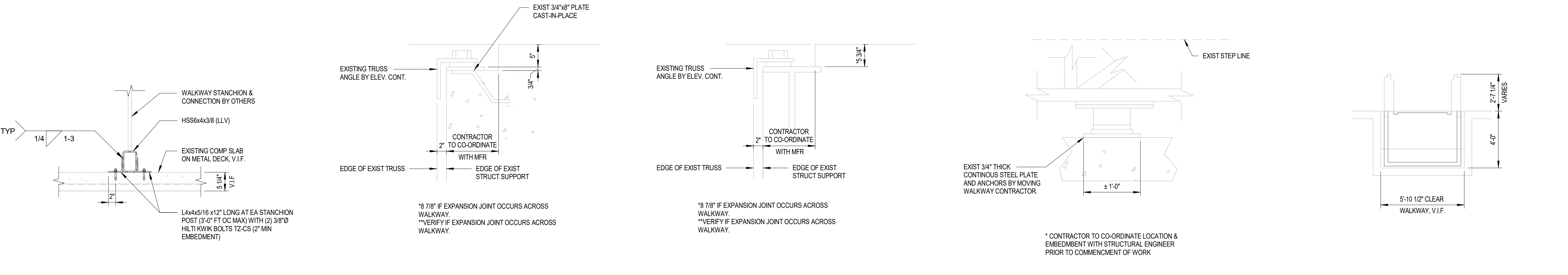
**DEMOLITION NOTES:**

- CONTRACTOR TO FIELD VERIFY ALL EXISTING CONDITIONS, DIMENSIONS & STRUCTURAL MEMBER LOCATIONS AND NOTIFY ARCHITECT/ENGINEER IF EXISTING CONDITIONS DIFFER FROM THOSE SHOWN ON ARCHITECTURAL AND STRUCTURAL DRAWINGS
- ELEVATOR AND ESCALATOR PIT DIMENSIONS AND LOCATIONS MUST BE CONFIRMED WITH MANUFACTURER(S) PRIOR TO CONSTRUCTION
- CONTRACTOR TO NOTIFY EOR IF THE REQUIRED ESCALATOR AND MOVING WALKWAY OPENINGS ARE LARGER THAN EXISTING PRIOR TO COMMENCEMENT OF WORK. CONTRACTOR TO CO-ORDINATE SUPPORT POINTS OF ESCALATORS & MOVING WALKWAY PRIOR TO CONSTRUCTION
- CONTRACTOR TO ENSURE THAT THE STAGING LOADS FOR THE ESCALATOR REPLACEMENT DO NOT EXCEED THE DESIGN LIVE LOADS NOTED ON DRAWINGS. IF STAGING LOADS ARE IN EXCESS FOR DESIGN LOADS PLEASE NOTIFY ENGINEER OF RECORD
- CONTRACTOR TO VERIFY HEADROOM, TYP

**1 MOVING WALKWAY DMSW-1**  
SCALE: 1/8" = 1'-0"



**2 MOVING WALKWAY DMSW-2**  
SCALE: 1/8" = 1'-0"



**7 DETAIL AT WALKWAY STANCHION SUPPORT**  
SCALE: 3/4" = 1'-0"

**6 CONCRETE SUPPORT DETAIL**  
SCALE: 1" = 1'-0"

**5 STEEL SUPPORT DETAIL**  
SCALE: 1" = 1'-0"

**4 INTERMEDIATE SUPPORT DETAIL**  
SCALE: 1" = 1'-0"

**3 WALKWAY SECTION - MOVING WALKWAY**  
SCALE: 1/4" = 1'-0"

FILE PATH: C:\Users\John.Moore\OneDrive\Documents\230391\_IAH Terminal D Conveyance\_R20\_jmoore\K2YP.rvt

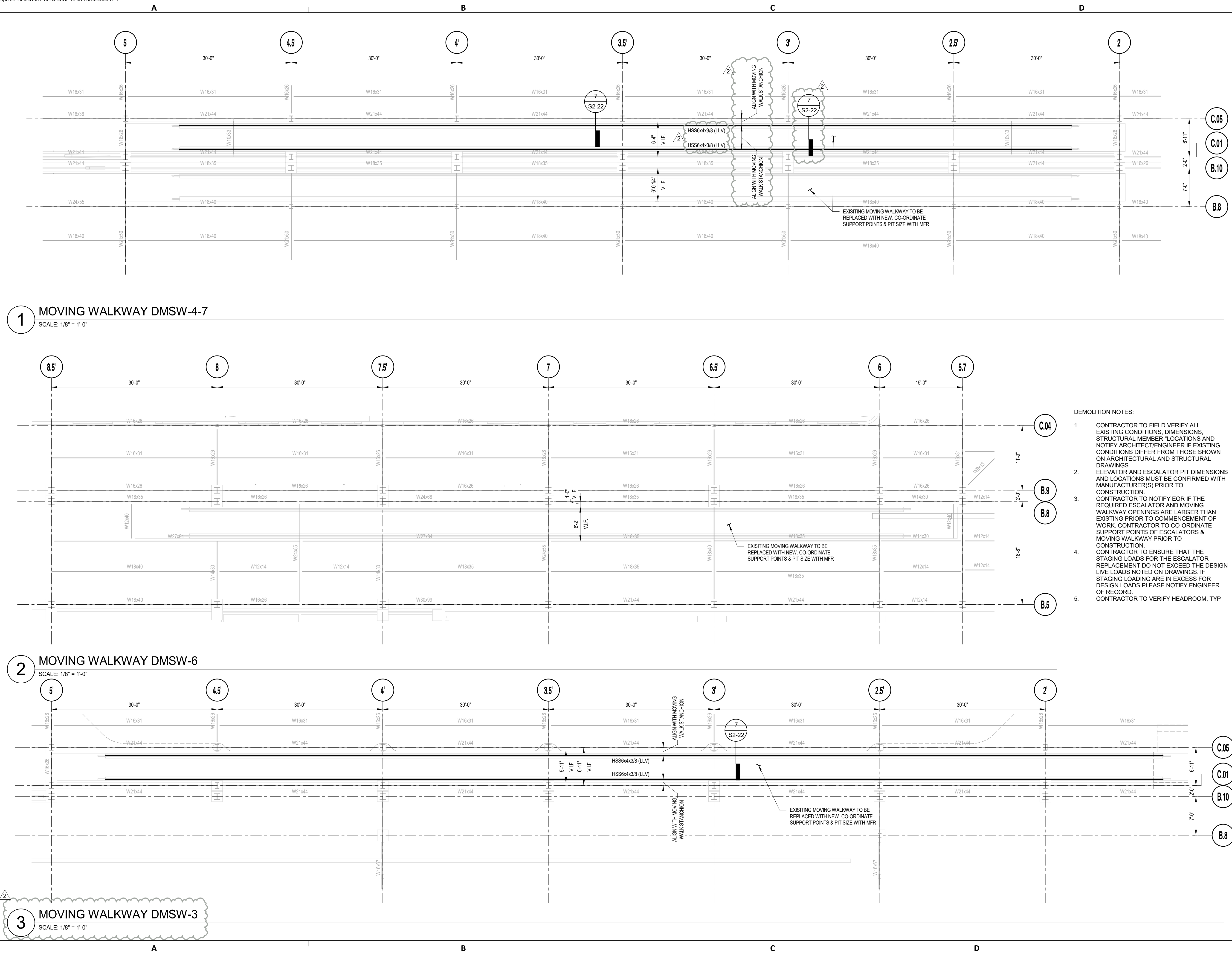
HAS FILE: PLOT DATE: DOA DWG FILE: OLD DOA No.:

SHEET SIZE: 22"x34" ANSI-D



FILE PATH: C:\Users\John.Moore\OneDrive\Documents\230391\_IAH Terminal D Conveyance\_R20\_jmoore\K2YP.rvt

HAS FILE: ...  
PLOT DATE: ...  
DOA DWG FILE: ...  
OLD DOA No.:



**1 MOVING WALKWAY DMSW-4-7**  
SCALE: 1/8" = 1'-0"

**2 MOVING WALKWAY DMSW-6**  
SCALE: 1/8" = 1'-0"

**3 MOVING WALKWAY DMSW-3**  
SCALE: 1/8" = 1'-0"

- DEMOLITION NOTES:**
- CONTRACTOR TO FIELD VERIFY ALL EXISTING CONDITIONS, DIMENSIONS, STRUCTURAL MEMBER LOCATIONS AND NOTIFY ARCHITECT/ENGINEER IF EXISTING CONDITIONS DIFFER FROM THOSE SHOWN ON ARCHITECTURAL AND STRUCTURAL DRAWINGS
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  - CONTRACTOR TO VERIFY HEADROOM, TYP



IAH TERMINAL D CONVEYANCE REPLACEMENT - 2800 N TERMINAL RD, HOUSTON, TX 77032  
**IAH TERMINAL D CONVEYANCE REPLACEMENT**  
C.I.P. No. A.I.P. No.  
C.O.H. No. D.O.A. No.



DESIGNER PROJECT No.: 23-08  
PROJECT STATUS: ISSUED FOR ADDENDUM 03

**REVISIONS**

No.	DESCRIPTION	DATE	BY
0	ISSUE FOR PERMIT	05/16/2023	
1	PERMIT REVISION	11/28/2023	
2	ISSUE FOR ADDEN 03	02/28/2024	

DESIGNER: K. PATEL  
DRAWN BY: R. RODRIGUEZ  
CHECKED BY: E. ROGERS  
ISSUE DATE: 05/16/2023  
APPROVED BY: E. ROGERS  
APPROVAL DATE:

DIRECTOR  
of  
HOUSTON AIRPORT SYSTEM

Review/ Approval Category

**IFP**  
ISSUED FOR PERMIT

Henderson Rogers  
Structural Engineers, LLC  
TBPE Firm Registration No. 8755

**TERMINAL "D" NORTH**

SHEET NAME: PARTIAL FRAMING PLANS

SHEET No. S2-23 SCALE: 1/8" = 1'-0"

SHEET SIZE: 22"x34" ANSI-D