



South Airfield Lighting Vault Repair and Rehabilitation Project

George Bush Intercontinental Airport (IAH)

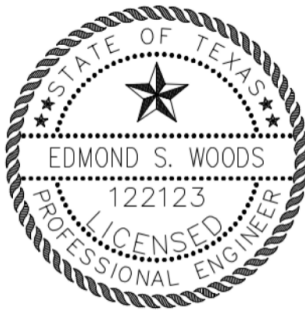
Construction Safety and Phasing Plan

March 15, 2024

Houston Airport System (HAS)

Project No. 952

ISSUE FOR CONSTRUCTION



Edmond Woods PE 03/15/24



IAH South Vault

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Document history and status

Revision	Date	Description	Author	Reviewed	Approved
0	02/13/2024	90% Design Submittal	E. Woods	D. Tompkins	A. Otto
1	03/15/2024	Issue for Construction	E. Woods	D. Tompkins	A. Otto
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3					
4					
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- AutoCAD background files provided by the HAS Airport Spatial Information System (ASIS), consisting of planimetric, subsurface utility information, and FAA Part 77 contours.

Any warranty/guarantee (expressed or implied) to the data, observations and findings in the report is excluded to the extent permitted by law. This report must be read in its entirety, and excerpts are not representative of the findings. The report has been prepared exclusively for the Houston Airport System and no liability is accepted for any use or reliance on the report by third parties.

Executive Summary

Project Description

In 2021, a condition assessment was performed to evaluate the existing South Airfield Lighting Vault and provide a report recommending repairs or replacement.

The Airfield Lighting (AFL) Constant Current Regulators (CCR), S1 Cutouts and the Airfield Lighting Control and Monitoring System (ALCMS) equipment was found to be functional; however, the equipment is being phased out by the manufacturer and the supply of key replacement parts are severely limited.

The Houston Airport System (HAS) has determined that the South Vault CCRs and ALCMS will be replaced in the existing building and the electrical systems will be upgraded in the vault. Replacement of the existing electrical equipment in the vault allows the existing airfield lighting 5KV cables to remain in place without the addition of cable splices in the manholes. The existing airfield lighting control and monitoring system (ALCMS) node would remain in the existing vault rather than installing a new system in a replacement vault and temporary operation from both vaults. The existing ALCMS fiberoptic ring was modified with the previous replacement of the West Vault. The existing telecommunications infrastructure in the vault and the United Airlines Sensis aircraft monitoring system in the vault would also remain in place.

Replacement for the South Airfield Lighting Vault CCR equipment will be installed in a phased progression in the vault to maintain the current operation while installing and testing new equipment to be followed by scheduled outages to transfer airfield lighting circuits from the existing regulators to the new regulators and demolition of existing regulators in phases.

New Airfield Lighting (AFL) Constant Current Regulators (CCR), S1 Cutouts and the Airfield Lighting Control and Monitoring System (ALCMS) equipment will be installed in the South vault. The ALCMS equipment in the air traffic control tower (ATCT) and airfield service center (ASC) will be replaced with new equipment coordinated with the new ALCMS. Upgrades will be implemented for the ALCMS interface equipment in the North and West airfield lighting vault for integration of the existing CCR door ALCMS equipment with the new ACLMS in the South Vault, air traffic control tower and airfield service center.

The original standby generator is not operational and a portable generator adjacent to the building is connected with temporary cables to the existing automatic transfer switch (ATS). The original ATS includes two ATS's in a common enclosure with one ATS to transfer from utility source A to utility source B and the second ATS to transfer to the generator in the event of loss of both utility feeders or upon signals from the air traffic control tower (ATCT) during inclement weather. The replacement ATS's will be two individual ATSs positioned adjacent to each other and wired for transfer from Utility A to Utility B and from Utility A/B to generator. The replacement ATS's will be isolation bypass type for equipment maintenance.

A permanent generator will be installed to replace the original standby generator. The new generator will be installed with a new above ground UL2085 diesel storage tank. A new electrical service yard will contain standby generator, diesel tank, generator switchgear and cam-lock enclosures for a future portable load bank and trailer mounted generator for future testing and maintenance. The service yard

will be enclosed with CMU or precast concrete walls, which will be designed to be bullet resistant to minimum UL 752 Level 4, as required by FAA regulations. The space available from the original generator location allows for pre-installation of the new main switchgear and the two automatic transfer switches prior to removal of the existing main switchgear and 3-way automatic transfer switch (ATS). The pre-installation of the new electrical equipment will allow for shorter duration outages for connection of the equipment to the electrical vault. The equipment connections will be scheduled in phases to limit the duration of the required outages.

The scope of work for the South Airfield Lighting Vault Repair and Rehabilitation project will include, but not limited to, the following:

1. Replacement of CCR equipment.
2. Replacement of AFL, CCR, S1 Cutouts, and ALCMS equipment.
3. Installation of new emergency generator in a CMU or precast concrete bullet resistant enclosure to replace original standby generator currently located in the existing vault.
4. Replacement and/or relocation of main switchboards, panels, step-down transformers, and service disconnects.
5. Sealing of wire vault penetrations.
6. Replacement of coping for roof membrane and metal parapets.

Construction Safety Phasing Plan

This Construction Safety and Phasing Plan (CSPP) was developed in accordance with Federal Aviation Administration (FAA) Advisory Circular (AC) 150/5370-2G (or latest edition), *Operational Safety on Airports During Construction*, for the South Airfield Lighting Vault Repair and Renovation project at George Bush Intercontinental Airport (IAH). This AC also includes an outline for the Safety Plan Compliance Document (SPCD) to be completed by each contractor awarded the construction of the South Airfield Lighting Vault Repair and Renovation project and is included in Appendix C. Acronyms for various construction and safety terms and elements are used throughout this Construction Safety Phasing Plan. Appendix C contains the FAA AC150/5370-2G in its entirety. For a list of terms and acronyms, see AC 150/5370-2G Appendix B: Terms and Acronyms.

Aviation safety is the primary consideration at airports, especially during construction. The airport operator's CSPP and the Contractor's SPCD are the primary tools to ensure safety compliance when coordinating construction activities with airport operations. These documents identify all aspects of the construction project that pose a potential safety hazard to airport operations and outline respective mitigation procedures for each hazard. They must provide information necessary for the Airport Operations department to conduct airfield inspections and expeditiously identify and correct unsafe conditions during construction. All aviation safety provisions included within the project drawings, contract specifications, and other related documents must also be reflected in the CSPP and SPCD. Requirements contained within this CSPP are enforceable by the contract outlined in Article 10 – Safety Precautions, Section 10.1 - Safety Programs, Section 10.2 – Pollutants and Pollutant Facilities, Section 10.3 – Safety of the Environment, Persons and Property, and Section 10.4 – Emergencies per Document 00700-General Conditions.

Safety Plan Compliance Document (SPCD) Requirements

Each Contractor shall submit to the HAS Project Manager in writing a detailed SPCD for their respective portion of the CSPP in accordance with AC 150/5370-2G or latest edition. The SPCD shall include, but not be limited to, installation sequence of traffic control items, barricades, fence and gate installation, haul routes within the work zone, staging, storage and stockpile areas, lockout/tagout procedures, electrical shutdowns, disconnect and re-connections, equipment height restrictions including crane operations, Foreign Object Debris (FOD) management plan, and inclement weather management plans (i.e. severe weather, tropical storms, hurricanes, etc.). The SPCD shall be submitted at least 14 calendar days prior to the start of construction. No work may commence until the SPCD is approved. The SPCD shall also contain the following:

1. A plan, by construction element and phase, for controlling construction equipment, personnel and vehicular movements in the AOA. The plan must include material haul roads. The plan shall detail the general requirements contained in the CSPP.
 - a) The Contractor shall provide a responsible safety coordinator whose duty shall be to direct all construction traffic near active runways, taxiways, taxilanes, aprons, haul roads, and highways. Paved surfaces shall always be kept clean and specifically must be kept free from all debris that have the potential to damage aircraft. The Contractor shall ensure all traffic control and warning devices are properly placed, maintained and operational daily.
2. The SPCD shall discuss in detail any minor deviations or topics that require additional detail in the CSPP. Should the SPCD include substantive changes to the CSPP requested by the Contractor, the revised CSPP must be submitted 60 days prior to the start of work for the Airport to obtain approval of such changes from the FAA.

The SPCD should be written to match the format of the CSPP without duplicating information in the CSPP and should include all the supplemental information that could not be included in the CSPP prior to award of the construction contract. Additionally, the SPCD should include a general statement from the Contractor indicating they have read and will abide by the CSPP – the Contractor's statement shall include the name of the Contractor, title of the project, CSPP approval date, and any reference to supplemental information. For example:

I, <Name of Contractor's Contract Signee/Company Name>, have read the IAH South Airfield Lighting Vault Repair and Renovation CSPP, approved on <Date>, and will abide by it as written and with the following additions as noted: <Insert Supplemental Information>.

Communications Plan

The organizational chart outlines the primary points of contacts and expected flow of communication required throughout the execution of the project, CSPP and SPCD to ensure safe and efficient operations, coordination of construction activities, and planned airfield closures. All communication shall be directed through the HAS Project Manager.

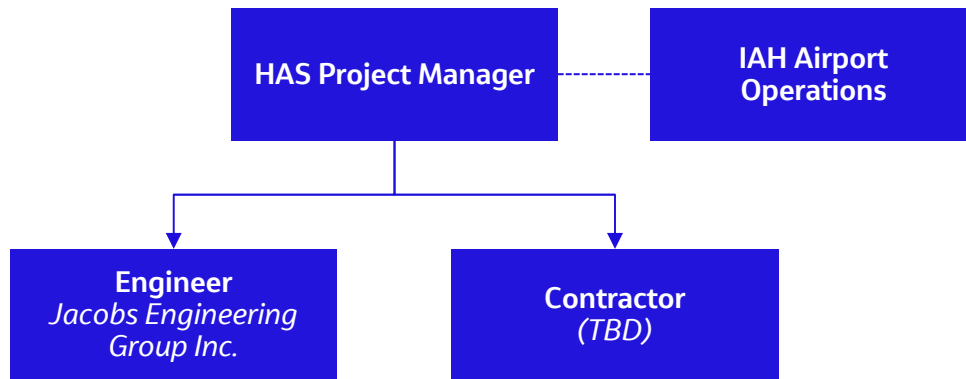


Figure 1 - Organizational Chart

1. Coordination (*Section 2.5*)

A. Meetings Before and During Project

1. **Pre-Construction Meeting.** Prior to the start of any operation on the airport, a pre-construction conference will be held by the HAS Project Manager. The purpose of the meeting is to review the work sequence schedule and safety, operational phasing, quality control/quality acceptance, and security procedures. Attendees will include the airport management and operations, program manager, project manager, engineer, and all Contractor's supervisory personnel. In addition, a pre-construction meeting will be held by the HAS Project Manager before the start of each phase of work described within this report and the Phasing Plans. A basic outline of topics to be covered at this meeting include:
 - a) Project Overview
 - b) Roles and Responsibilities
 - c) Communications Plan
 - d) Airport Security and Badging
 - e) Construction Safety Phasing Plan
 - f) Safety Plan Compliance Document
 - g) Construction Critical Path Method (CPM) Baseline Schedule
 - h) Construction Management and Quality Control Plan
 - i) RFIs, Submittals / Shop Drawings, Field Orders
 - j) Labor Requirements
 - k) Civil Rights Requirement
 - l) Submittal of Payment Requests

2. **Construction Progress Meetings.** Construction Progress Meetings will be held on a weekly basis by the HAS Project Manager. Attendees will include the HAS Project Manager, the IAH Project Manager, Engineer, and the Contractor's supervisory personnel. The weekly Construction Progress meetings will address the following items:
 - a) Operational Safety and Security
 - b) Navigation Aids (NAVAIDs)
 - c) FAA Facilities
 - d) Notice to Air Missions (NOTAMs)
 - e) Quality Control / Quality Assurance
 - f) Construction Progress / Schedule / 3-week look-ahead
 - g) ARFF Response Routes
 - h) Construction Issues
 - i) Administrative Issues

- B. **Scope or Schedule Changes.** Scope changes are not anticipated. Schedule changes may result from inclement weather or operational constraints imposed at the discretion of HAS. Changes in the scope or schedule of the project may necessitate revisions to the CSPP and Contractor's SPCD. Revisions to the CSPP and SPCD may require review and approval by the HAS Project Manager and/or FAA.
- C. **Provisions for FAA Air Traffic Organization (ATO) Coordination.** Coordination with the FAA ATO will be directed through the HAS Project Manager and IAH Airside Operations. Coordination will be made when scheduling FAA-owned facility shutdowns and/or restarts. The Contractor shall coordinate with HAS Project Manager and IAH Airside Operations regarding specific requirements related to shutdowns and/or restarts.
- D. **Safety Coordination Briefings.** A Safety Coordination Briefing will be performed with IAH Airside Operations before crews go onto the field and brief on activity, any special concerns (Weather/Special Actives/etc.), ARFF response routes, and general safety reminders and rules. This Safety Coordination Briefing will occur before each IAH Airside Operations' shift if multiple shifts are in operation to support the construction.

2. Phasing (*Section 2.6*)

Refer to Appendix D for detailed Phasing Plans. A summary of work for each phase is provided below.

- A. Phase 1 – Work will include:
 - 1. Placement of roadway signage and barricades as shown on the Phasing Plan
 - 2. Replacement of South Airfield Lighting Vault CCR equipment
 - 3. Installation of new Airfield Lighting (AFL) Constant Current Regulators (CCR), S1 Cutouts, and Airfield Lighting Control and Monitoring System (ALCMS)
 - 4. Replacement and/or relocation of main switchboards, panels, step-down transformers, and service disconnects
 - 5. Construction of CMU or precast concrete bullet resistant enclosure and site improvements including clearing, drainage, grading, and restoration. (In architecture phasing – interior building construction phasing, this construction will be performed in the last phase.)
 - 6. Removal of original standby generator; installation of new, permanent generator

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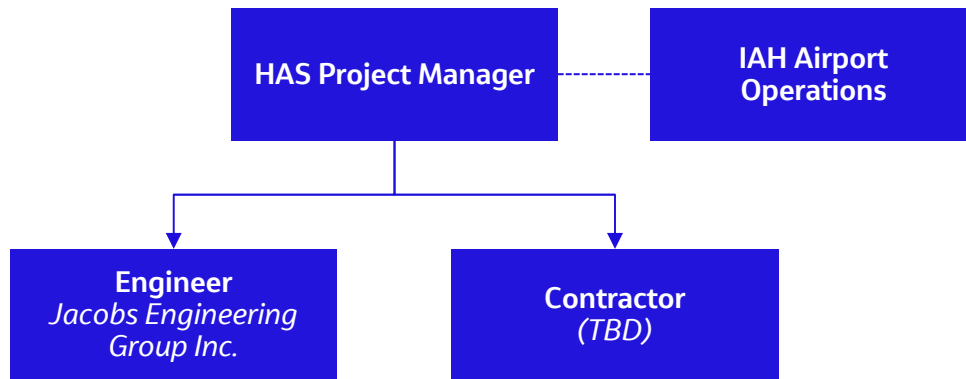


Figure 1 - Organizational Chart

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Refer to Appendix D for detailed Phasing Plans. A summary of work for each phase is provided below.

- A. Phase 1 – Work will include:
 - 1. Placement of roadway signage and barricades as shown on the Phasing Plan
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 - 5. Construction of CMU or precast concrete bullet resistant enclosure and site improvements including clearing, drainage, grading, and restoration. (In architecture phasing – interior building construction phasing, this construction will be performed in the last phase.)
 - 6. Removal of original standby generator; installation of new, permanent generator

3. Areas and Operations Affected by the Construction Activity (section 2.7)

Table 1 - Areas Affected by Construction Activity

Phase	Duration	Closure
1	900 Calendar Days	Work Hours: <ul style="list-style-type: none"> • 8:00 AM to 6:00 PM Monday through Saturday Runway Closures <ul style="list-style-type: none"> • None Taxiway Closures <ul style="list-style-type: none"> • None NAVAIDs Impacted: <ul style="list-style-type: none"> • None

- A. **Work Restrictions.** See table above for work restrictions within each proposed phase. Lockout/Tagout procedures are only permitted during daytime hours. The Contractor may not de-energize any electrical infrastructure at night or perform any work at night requiring lockout/tagout.
- B. **Mitigation of Effects.** No runway or taxiway closures are expected for this project. Lighted low-profile barricades will be utilized to notify users of runway and taxiway closures. Barricades shall be waterfilled and shall have lights mounted atop the barricades spaced at a maximum of 10 feet apart. Lighted runway closure devices are required for all runway closures. Markers shall be used in grassland islands to delineate phasing areas. See Appendix D *Phasing Plans* for more information. NOTAMs will be put in place for all closures and shall be coordinated 72 hours in advance with HAS Project Manager, IAH Airport Operations and ATCT. Contractor shall be required to submit a Work Area Notification (WAN) to IAH Airport Operations at least 2 weeks in advance of the planned closures. The link to submit a WAN is provided below.
https://hasonbase.houstonairportsystem.net/OnBaseWeb_Prod_WF/UnityForm.aspx?key=UFKey
- C. **Taxiway/Taxilane Safety Areas (TSA) and Object Free Areas (TOFA).** Contractor shall not enter the TSA or TOFA of any active taxiway/taxilane without approval from Airside Operations.
- D. **Runway Safety Areas (RSA) and Runway Object Free Areas (ROFA).** Contractor shall not enter the RSA or ROFA of any active runway under any circumstances.
- E. **Coordination with other Projects.** The South Airfield Lighting Vault Repair and

Rehabilitation project is not expected to overlap with other projects at the airport. If the project does overlap with other projects, the Contractor shall coordinate with other projects.

Table 2: Runway/Taxiway Safety and Object Free Areas

Runway/Taxiway	Airplane Design Group (ADG)	Safety Area	Object Free Area
Runway 9-27	ADG-V	500 foot width, 250 feet from runway centerline, extends 1000 feet beyond the end of the runway.	800 foot width, 400 feet from runway centerline, extends 1000 feet beyond the end of the runway.
Taxiway SC	ADG-V	214 foot width, 107 feet from taxiway centerline	285 foot width, 142.5 feet from taxiway centerline
Taxiway SF	ADG-V	214 foot width, 107 feet from taxiway centerline	285 foot width, 142.5 feet from taxiway centerline
Taxiway SA	ADG-V	214 foot width, 107 feet from taxiway centerline	285 foot width, 142.5 feet from taxiway centerline
Taxiway SB	ADG-V	214 foot width, 107 feet from taxiway centerline	285 foot width, 142.5 feet from taxiway centerline
Taxiway SG	ADG-V	214 foot width, 107 feet from taxiway centerline	285 foot width, 142.5 feet from taxiway centerline
Taxiway SH	ADG-V	214 foot width, 107 feet from taxiway centerline	285 foot width, 142.5 feet from taxiway centerline
Taxiway SJ	ADG-V	214 foot width, 107 feet from taxiway centerline	285 foot width, 142.5 feet from taxiway centerline
Taxiway SK	ADG-V	214 foot width, 107 feet from taxiway centerline	285 foot width, 142.5 feet from taxiway centerline

4. Navigation Aid Protection (*Section 2.8*)

- A. No NAVAIDS are anticipated to be impacted for this project.

5. Contractor Access (*Section 2.9*)

- A. Location of Stockpiled Construction Material - See Appendix D for location.

1. The Contractor shall only use the staging areas shown on the plans unless otherwise approved by the HAS Project Manager in advance of construction.
2. Stockpiles will be allowed in the Contractor's dedicated stockpile area in accordance with the approved 7460 for this project.
3. Stockpiles must be a minimum of 500 feet from the runway centerline, 160 feet from any taxiway centerline, and 30 feet from existing security fencing, edge lights, and services road edge of pavements.
4. Stockpiles must be prominently marked and lighted during hours of restricted visibility or darkness.
5. Any stockpiles shall require submission and approval of a 7460.
6. Stockpile materials that become a wildlife attractant or a FOD hazard shall be removed or relocated to a location acceptable to the HAS Project Manager and IAH Airport Operations and in accordance with the Contractor's FOD Management Plan.

B. Vehicle and Pedestrian Operations:

1. All personnel and Contractor vehicles shall remain within the construction area, along the designated haul route, or within the limits of the staging, storage, and stockpile areas as shown on the plans. The Contractor shall ensure that the Vehicle Service Road (VSR) remains clear at all times. See Appendix D for the Construction Safety Phasing Plan.
2. All vehicles shall be identified with a flashing/rotating amber light beacon or a 3-foot by 3-foot square flag with a 1-foot square orange and white checkered board pattern with company logos on both sides of the vehicle. Company logos shall be visible up to a distance of 200 feet. Flashing / rotating amber lights shall be in operation during night operations, inclement weather, and low-visibility conditions.
3. All construction site personnel shall always wear high visibility warning garments when working in the project area. Safety garments / vests shall be a minimum of Class II.
4. Access to the job site shall be through the designated security gate and haul routes as shown on sheet GC100 of the CSPP drawings in Appendix D.
5. Oversized loads will be coordinated and escorted by Airside Operations as needed. Refer to Section 8 for Hazardous Material Management.
6. IAH Airside Operations shall be contacted daily upon the activation of the haul route before its use. Refer to Section 9 for Airside Operations contact information.
7. All vehicles accessing the AOA shall be operated by badged personnel or under the escort of badged personnel.
8. No vehicle shall be within the movement area unless identified as described herein, is monitoring appropriate radio frequency, and under the escort of a designated airport operations escort in communication with ATCT when operating in the movement area.
9. All construction vehicles shall be thoroughly inspected prior to entering the AOA and movement areas including safety checks of lights, horns, beacons, flags, and logos as well as checking tires, tail gates, etc. from loose debris, dirt, etc. in accordance with the Contractor's FOD management plan.

10. It is the intent of the construction drawings to minimize interference to aircraft movement and emergency equipment. In all portions of the AOA, the aircraft and emergency equipment shall have the right-of-way.
 11. During the performance of this project, the airport runways, taxiways, taxilanes, and aircraft parking aprons shall remain in use by aircraft to the maximum extent possible. The contractor's work site shall be controlled to minimize disturbance to the airport operations. Aircraft and emergency equipment shall always have the right of way over construction traffic.
 12. Construction equipment and vehicles shall not exceed 15 miles per hour within the AOA.
 13. The contractor, at its own expense, shall repair any damaged pavements, markings, signs, lighting, or fencing to equal or better condition because of the Contractor's traffic and construction activities, and all work areas shall be returned in compliant condition with all applicable Advisory Circulars and to the satisfaction of HAS.
- C. There are three (3) access gates, which is Gate SV2, Gate AV7, and Gate AV9, identified on the Staging Area and Site Access Plan (refer to Appendix D). SV-2 and AV-9 are the AOA gates, while AV-7 is a non-AOA gate used to access AV-9. The Contractor shall utilize the access gate to limit crossing active pavements and to minimize interference with aircraft operations. No other access gates shall be permitted for accessing the project site unless expressed written consent is provided by the HAS – IAH Security and/or Airside Operations.
- D. The Contractor shall always keep all gates locked or guarded, except for the brief period required for passage of authorized vehicles and equipment. The Contractor must implement procedures in their SPCD to ensure that only authorized personnel and vehicles have access to the AOA and to prohibit "piggybacking" behind authorized vehicles.
- E. Aviation band radios aren't anticipated to be needed for this project.
- F. Badging and Escort Requirements
1. All Contractor employees, subcontractors, agents, vendors, invitees, etc., requiring access to the construction site shall, in accordance with the airport operations security program, be required to display airport-issued identification or be under airport-approved and badged escort personnel. These badges will be identified numerically and issued to individual employees with a permanent record maintained on each individual to whom a badge is issued. In addition, a \$55 non-refundable processing fee will be required for each badge. This fee must be paid before a badge is issued. No badge will be issued to any person until a review of the required paperwork by airport security and all requirements are met. Paperwork shall be submitted a minimum of 24 hours before issuance of a badge. The Contractor is responsible for personnel attending training and completing security badge applications, which will include air /ground radio, taxiway, and airport familiarization. Estimated time for completion is two (2) hours. Badge applications shall be submitted immediately upon issuance of

- Notice-to-Proceed and obtained prior to the start of construction.
2. Anyone who drives on the AOA will be required to obtain a “D” on their badge by completing the AOA Non-Movement Driver Training. The “D” badge endorsement will limit contractors to VSRs only. If access is required in the movement area, please coordinate with Airside Operations.
 3. Flaggers aren’t anticipated to be needed for this project.
 4. At the completion of the contract all badges will be returned to the airport. A charge of \$50 per badge will be assessed for all unreturned badges. Gate guards and escorts shall be considered under the flagger classification and shall be subject to the same requirements as flaggers.
 5. The Contractor is not anticipated to be crossing any taxiways as a part of this project. A maximum of 3 vehicles per escort will be permitted. Airport Operations (Airside) will not be providing escort in the non-movement/ramp area. This shall be the Contractor’s responsibility.
- G. **Situational Awareness:** All vehicles will stay on the VSR and the construction site perimeter will be bordered by barricades and lights to ensure contractors stay away from movement area. In addition, it is the responsibility of the escort vehicle driver to verify the movement/position of all escorted vehicles at any given time.

6. **Wildlife Management (Section 2.10)**

- A. **Trash.** The construction area shall be continuously monitored for trash and any trash shall be thrown away immediately. Food scraps shall be disposed of in a closed container.
- B. **Standing Water.** Standing water shall be removed from the construction site within 24 hours.
- C. **Tall Grass.** The Contractor shall be responsible for maintaining the work site, staging area and parking areas free from tall grass. The Contractor shall restore the staging area to equal or better conditions including Turf Reestablishment. The seed mixture used for turf reestablishment shall consist of 60% Bermuda, 20% Kentucky Bluegrass, and 20% Perennial Rye grasses. Millet seed shall NOT be used. Mandatory coordination shall be required according to HAS standards with the IAH Airport Qualified Wildlife Biologist.
- D. **Fencing and Gates.** The Contractor shall immediately report any damage to gates or fences to the airport. The Contractor shall be responsible for repairs to any gates or fences caused by negligence by the Contractor. Once the damage is reported, the contractor shall stay with the gate until relieved by Airport Operations. Refer to Section 9 for contacting HAS and Airside Operations.
- E. **Disruption of Wildlife Habitat.** In the event of a wildlife encounter within the AOA, the Contractor shall immediately contact the HAS Project Manager and IAH Airside Operations. The contractor shall avoid any contact with the wildlife or disturbance of a nest and call

Airside Operations.

- F. **Feeding of Wildlife.** Feeding of wildlife is strictly prohibited on the airport, including placement of food or other attractants.

7. **Foreign Object Debris (Section 2.11)**

- A. The Contractor will continuously look for any signs of FOD and remove it immediately. See Section 10 Inspection Requirements.
- B. The Contractor shall sweep/vacuum pavements adjacent to work areas and access/haul routes continuously to keep pavement free of loose debris at all times. All closed pavement shall be cleaned of all debris and inspected for FOD prior to opening.
- C. At least one fully-functioning vacuum sweepers shall be readily available at all times once construction operations are under way.

8. **Hazardous Material Management (Section 2.12)**

- A. No Hazardous materials will be brought onto or stored on the airfield as a result of the construction operations, with the exception of the oil, fuel, and hydraulic fluid contained in the tanks required to operate the equipment.
- B. Contractor shall provide spill containment resources in accordance with the SPCD and local, state and federal laws. On-airport fueling should be done in accordance with HAS regulations/codes, and as approved with coordinated sign-off from the local jurisdiction (Fire Marshal).
- C. Fuel spills or leaks shall be cleaned up immediately. Fuel spills on airport property should follow HAS procedures, which requires immediate notification and response to the HAS Project Manager and IAH Airside Operations.
- D. The encounter of hazardous material is not anticipated, but if encountered, the Contractor shall notify the HAS Project Manager and IAH Airside Operations immediately of any potentially hazardous material. For identification of how vehicles carrying HAZMAT materials will be identified, refer to Section 5.
- E. IAH Airport Operations shall be notified immediately of any HAZMAT situation. Refer to Spill Response Procedures Chart in Appendix G.

9. Notification of Construction Activities (Section 2.13)

- A. **List of Responsible Representatives.** The Contractor shall provide a list of all points of contact responsible for project execution. The list shall contain names, telephone numbers, and alternate contacts as required. Please see Table 4 for the list of point of contacts involved during construction. Contractor shall follow the organization flow chart provided in this CSPP, or as amended during construction. All communications shall be directed through the HAS Project Manager.

Table 4: Points of Contact

Organization	Role	Point of Contact	Contact Info.
Houston Airport System	HAS Project Manager	Christopher Bunn	Chris.Bunn@houstontx.gov (281) 233-1965
George Bush Intercontinental Airport	HAS Design Manager	Shahidur Rahman	Shahidur.Rahman@houstontx.gov (281) 233-1899
George Bush Intercontinental Airport	IAH 24-hr Emergency Dispatch	Comm. Center	(281) 230-1300
George Bush Intercontinental Airport	Construction Airspace Coordinator (7460 Notices of Construction)	Juan Pedracova	Juan.Pedracova@houstontx.gov (281) 230-8915
George Bush Intercontinental Airport	IAH Police	General	(281) 230-6800
George Bush Intercontinental Airport	IAH Airside Operations Center	Duty Phone	(713) 376-0254
George Bush Intercontinental Airport	IAH Landside Operations Center	Duty Phone	(281) 743-4367

Organization	Role	Point of Contact	Contact Info.
George Bush Intercontinental Airport	Airside Operations Construction Manager	Jeffrey Cruser	Jeffrey.Cruser@houstontx.gov (713) 534-7276
George Bush Intercontinental Airport	IAH SMS	Christopher Ford	Christopher.Ford@houstontx.gov (281) 233-7309
George Bush Intercontinental Airport	IAH Airfield Electrical and Maintenance	Jason Steen	Jason.Steen@houstontx.gov (281) 230-3091
George Bush Intercontinental Airport	HFD – ARFF	--	(281) 233-7960
George Bush Intercontinental Airport	IT	--	(281) 233-1900
Jacobs Engineering Group Inc.	Jacobs Project Manager	Art Otto	Art.Otto@jacobs.com (281) 721-8400
Federal Aviation Administration	FAA IAH SSC Manager	Ron Adair	Ronald.Adair@faa.gov (713) 847-1430
Federal Aviation Administration	FAA ATCT	Bryan Rogers	(281) 209-8621
Federal Aviation Administration	FAA, ATO Lead Planner, NAS	Moni Jacob	Moni.Jacob@faa.gov
Texas Poison Control	---	--	1-800-222-1222
Texas 811	Utility Locate	--	811

B. Notice to Air Missions (NOTAMS). Airside Operations shall initiate NOTAMS related to

construction activity as appropriate. While no NOTAM is anticipated, some may be needed for 7460s or crane operations. Please coordinate with Airside Operations Construction Manager in advance. The Contractor shall coordinate closures a minimum of 72 hours in advance to provide for timely initiation of NOTAMs. Contractor shall be required to submit a Work Area Notification (WAN) to IAH Airport Operations at least 2 weeks in advance of the planned closures. The link to submit a WAN is provided below.

https://hasonbase.houstonairportsystem.net/OnBaseWeb_Prod_WF/UnityForm.aspx?key=UFKey

- C. Emergency Notification Procedures.** In a life-threatening emergency, the Contractor shall call 911 immediately to initiate response. The contractor shall then immediately contact Airport Comm Center (281) 230-1300. Notification to IAH Airside Operations (713) 376-0254 or Landside Operations (281) 743-4367 depending on the location of the emergency shall occur immediately after this. All non-life-threatening emergencies shall be reported to Airside or Landside Operations. Notification to Airside Operations, Landside Operations (if impacted), and the HAS Project Manager shall occur after other parties are contacted.
- D. Coordination with Aircraft Rescue and Fire Fighting (ARFF).** Contractor shall coordinate any construction activities requiring torching, welding, burning, etc. through the HAS Project Manager for approval by the IAH Airside Operations.
1. Any welding activities on this project shall follow local codes and require a 1-hour fire watch in accordance with Airport, FAA and ARFF safety rules, regulations, and procedures. Any deactivation of water lines or hydrants, rerouting of access routes, or use of hazardous materials on the airfield shall not occur. However, if required, the activities shall be coordinated through the HAS Project Manager and approved by the airport's ARFF personnel prior to execution of such activities.
 2. Any affected ARFF emergency routes or alert locations shall be coordinated in advance with HAS Project Manager, IAH Airside Operations and ARFF. Contractor shall always provide a minimum 20-foot space between barricades along the edge of the work area closed to aircraft operations in support of maintaining ARFF access during construction.
 3. All construction activities shall be coordinated a minimum of 72 hours in advance of planned work.
- E. Notification to the FAA.** The Contractor shall ensure, through the HAS Project Manager that an aeronautical study (7460) is performed on all construction activities (stockpiles, equipment, temporary and permanent structures) and appropriate FAA regional or district offices prior to any construction activities.
1. No construction activities shall commence on this project without FAA approval of the CSPP, submission and approval of 7460s, and submission and approval of the Contractor's SPCD.

2. Significant changes to the CSPP during construction may require resubmission of 7460s and revisions to the Contractor's SPCD. Any significant changes to the CSPP and SPCD must be submitted prior to the start of work for the Airport to obtain approval of such changes from the FAA.

10. Inspection Requirements (*Section 2.14*)

- A. **Daily Inspections.** The Contractor shall conduct daily inspections to ensure conformance with the CSPP. Contractor shall refer to and utilize Appendix D, Construction Project Daily Safety Inspection Checklist, of FAA AC 150/5370-2G included with and attached to this CSPP as Appendix C.
 1. Airport Operations Daily Inspection: Operations conduct daily airfield inspections. These inspections of all airfield-paved areas and safety areas to ensure compliance with Federal Aviation Regulations (FAR) Part 139 and Advisory Circulars. IAH Security conducts perimeter, gate, and airport badge inspections for compliance with Transportation Security Regulation, CFR Part 1542. The Contractor shall not impede these inspections.
 2. All personnel shall be checked daily for proper identification and airport badges, safety vests, hard hats, foot, ear, and eye protection, radio, and cellular communication devices.
 3. All vehicles shall be checked daily for proper lighting, signage, markings, flagging, and ensure normal operation including working horns, lights, etc. All vehicles shall be inspected in accordance with the Contractor's FOD management plan and as described in this CSPP.
 4. All barricades shall be checked daily by the Contractor for signs of wear and tear and shall be repainted when deemed appropriate by IAH Airport Operations. No wooden barricades shall be used, only plastic, water-filled barricades are allowed. Water within barricades shall be checked daily and refilled to full water level line if necessary. The condition of lighting units shall be checked daily. All light fixtures shall be verified in operating condition and good working order by the Contractor daily, before the Contractor starts and ceases operations for the day and during construction activities. The barricade lights should be spaced no more than ten (10) feet apart. In addition to the daily barricade checks, all barricades shall be moved at least once each week and the Contractor shall sweep accumulated debris and remove and dispose of the debris offsite at an approved disposal facility in accordance with the Contractor's FOD management plan. The barricades shall then be replaced at the appropriate location.
 5. All service road, apron, taxilane, and taxiway pavements used for hauling and transporting operations, active or temporarily closed, shall be cleaned continuously, and prior to re-opening closed pavements, by the Contractor using approved vacuum sweepers. All damage caused by the contractor shall be repaired to compliant conditions.
 6. In the event of forecasted severe weather (i.e. thunderstorms, tropical storms, or

- hurricanes) Contractor shall inspect all active work areas to ensure that all work, temporary facilities, storage areas, and stockpiles are prepared and secured for high winds and/or flooding. A post-event inspection shall be made to assess damage. Contractor will mobilize immediately following a storm when safe to remove any debris and/or hazards to aircraft or vehicles.
7. Prior to ceasing daily operations, the contractor shall contact Airside Operations for inspection prior to the final crew leaving for the day.
- B. Final Inspections.** Closed areas shall be inspected by the Contractor, IAH Project Manager and IAH Airport Operations prior to opening to aircraft operations.
1. All dust, dirt and debris shall be removed from pavements in accordance with the Contractor's FOD management plan. All service road, apron, taxiway, taxiway and runway pavements used for hauling and transporting operations, active or temporarily closed, shall be cleaned continuously, and prior to re-opening closed pavements, by the Contractor using approved vacuum sweepers.
 2. All taxiway, taxiway and runway safety and object free areas shall be free of debris and excavations or pavement edge drops greater than of 3-inches in depth and grades in excess of 5% corrected prior to re-opening any closed pavements.
 3. Any ruts or depression caused by contractor vehicles shall be filled level with topsoil and covered with Bermuda sod, staked in place with biodegradable sod stakes sufficiently to resist jet blast effects.

11. Underground Utilities (*Section 2.15*)

- A. Underground utilities otherwise not shown on the plans are not likely to be encountered during construction, however, the Contractor shall verify, locate, and mark all utility lines prior to the start of work.
- B. Any utilities encountered not shown on the plans shall be reported by the Contractor to the HAS Project Manager and IAH Airport Operations immediately such that the utility can be identified, marked, logged, and filed for later as-built and updates to the airport's utility master plan.
- C. Any utilities cut or damaged will be at the Contractor's cost. Contractor shall immediately notify Airside Operations if this occurs. The Contractor shall provide their best effort to return the utility to service expeditiously.
- D. The primary utility owners/operators are:
 1. Houston Airport System: Storm drainage, airfield lighting, communications cables
 2. City of Houston: Water and wastewater lines
 3. FAA: NAVAIDs power, control, and communications cables

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12. Penalties (*Section 2.16*)

- A. Any monetary penalties assessed due to actions by the Contractor personnel will be the responsibility of the Contractor. See Tenant Violations (OI 05-03) for more information.
- B. Penalties for non-compliance with airport safety procedures may include but are not limited to a warning, removal of driving privileges or loss of access to the AOA.
- C. Penalties for violation of the safety plan are summarized in Appendix F – *Houston Airport System, Tenant Violations – Offenses, Charging Instrument, Due Process Provisions*.

13. Special Conditions (*Section 2.17*)

- A. In the event of an emergency (aircraft accident, security breach, etc.), the Contractor shall immediately suspend all construction activities and vacate the construction and staging area as required by the airport. Contractor shall first contact Airside Operations to determine if activities should cease. Other Special Conditions can include VIP Operations, SGMCS/Low-Visibility Operations, or MVFR or lower flight rules.
- B. Work impacting FAA NAVAIDs may not proceed during the period from the Friday before the Thanksgiving Holiday through January 1, due to a national moratorium observed by FAA Technical Operations personnel.
- C. In the event of severe weather (lighting or thunderstorms) or a forecasted tropical storm or hurricane, the Contractor shall implement the inclement weather and hurricane protection management plan including ceasing all construction activities and securing all materials, equipment, and flaggers. The Contractor's SPCD shall clearly outline and describe specific actions and measures to be implemented in the event of a forecasted weather event. Refer to Section 19. Other Limitations on Construction.
- D. All construction activities shall be halted, and equipment removed from the AOA in the event weather conditions fall below Visual Flight Rules (VFR) minimums (3 miles visibility, 1000-foot ceiling) as advised by IAH Airport Operations. Cranes may need to be lowered. Construction activity will be permitted to resume at such time weather condition are favorable for normal airport operations.

14. Runway and Taxiway Visual Aids (*Section 2.18*)

- A. **General.** No Runway or Taxiway closures are expected during this project. The closure of airfield pavements shall be clearly delineated by lighted low profile barricades and lighted runway closure devices, as shown on the plans, to provide visual notification of closed pavements and to create a barrier between the aircraft and vehicle movement

areas and construction areas.

- B. **Lockout/Tagout.** Any electrical work shall be performed in accordance with the Contractor's approved lockout / tagout procedures provided in the SPCD and shall be coordinated 72 hours in advance with HAS Project Manager, IAH Airport Operations, ATCT and Airport Electrical and Maintenance personnel. Lockout/Tagout is not permitted at night and shall be limited to the restricted work hours of sunrise to sunset. Any outage shall be coordinated with Airside Operations. Airfield circuits can be impacted by the work scheduled for this project. Therefore, the contractor shall coordinate with Airside Operations, who shall coordinate with the FAA Air Traffic Control Tower and HAS Electrical.
- C. **Markings.** Markings removed or damaged as a result of the Contractor's activities shall be restored to their original condition to a point of transition as determined by Airside Operations at the expense of the Contractor. The restoration of the markings shall adhere to the standards outlined in P-620 for the repainting of the markings. Markings shall be restored to the satisfaction of IAH Airside Operations.
- D. **Lighting and Visual NAVAIDS.** The need to protect all runway and taxiway edge lighting, centerline lighting, and guidance signs are not expected to be needed for this project, but in case of any additional scoping change, haul route changes, or construction entrance gate changes, the Contractor shall protect all runway and taxiway edge lighting, centerline lighting, and guidance signs, and remain clear of the Glide Slope and RTR critical areas during construction. The Contractor shall either remove and re-install, provide temporary coverings, or install temporary circuit jumpers and coverings of airfield signage and lighting as shown on the plans within and leading to closed pavements and shall be approved by HAS Project Manager and Airport Operations in advance of construction activities. Temporary circuit jumpers shall be installed in conduit, placed along the barricade lines on the pavement surface and secured, and shall be marked with electrical hazard warning tape.
- E. **Signage.** No signage impact is expected for this project. The Contractor shall modify and blank existing sign panels leading aircraft to closed areas in each phase. A modified sign schedule for each phase will be provided in the electrical plans. Temporary orange construction signs with black lettering reading "CONSTRUCTION AHEAD" will be evaluated on a case-by-case basis by HAS and IAH Airport Operations. Signs will be located outside the taxiway safety limits and ahead of construction areas, so pilots can take timely action. Mandatory signs will take precedence over orange construction signs.

15. Markings and Signage for Access Routes (*Section 2.19*)

- A. Access to the job site shall be through the designated gates and haul routes as shown the

plans.

1. A total of three (3) access gates are identified from which the Contractor can enter the airport as listed in Section 5 – Contractor Access. Access gates to specific work areas shall be strictly adhered to minimize crossing active airfield pavements. Location of access gates are provided in Appendix D.
 2. The Contractor may utilize more than one access gate at one time, with prior written authorization from the HAS Project Manager, Airside Operations and Security, to minimize interference with aircraft operations, and facilitate construction in accordance with the individual work areas, approved schedule, phasing and sequence of construction operations, and equipment maneuverability limitations.
- B. Sufficient barricades shall be placed at the entrance of the construction work area to create a barrier between vehicle movement areas and construction areas. Coordination and placement of barricades will be approved by Airside Operations on an as needed basis. See Construction Safety and Phasing Plan for location of barricades. A minimum 20-foot space along the edge of pavement / shoulder shall be provided to maintain ARFF access to the airfield and closed pavement areas.

16. Hazard Marking, Lighting, and Signage (Sections 2.20 & 2.21)

- A. Areas Impacted by Construction Operations.
1. Sufficient water-filled low-profile barricades with red flashing solar powered lighting shall be installed to delineate the work site. Barricades are to be interlocked, except as noted in Section 15.B – ARFF Access. This is intended to prevent aircraft from entering the work site and to prevent construction personnel from entering areas open to aircraft.
 2. Open excavation and stockpiled materials shall be conspicuously marked with barricades, signage and/or lighting. See Appendix B - 7460 and Appendix D – Phasing Plans for stockpile location.
- B. All vehicles and equipment shall be identified flashing/rotating amber light beacon or a 3 foot by 3-foot square flag with 1-foot square orange and white checkered board pattern, and with company logos on both sides of the vehicle visible within 200 feet. Flashing / rotating amber lights shall be in operation during night operations, inclement weather and low-visibility conditions.
- C. All tall equipment more than 12 feet shall be equipped with red obstruction lighting, in addition to the orange and white checked flags. All construction equipment, regardless of height, shall require submission and approval of a 7460. All cranes shall be lowered when not in operation.

- D. All construction site personnel shall always wear high visibility warning garments (min. Class II) when working in the project area.

17. Work Zone Lighting for Nighttime Construction

- A. Nighttime hours of operation shall be defined as 1 hour before official sunset and 1 hour after official sunrise.
- B. During nighttime work, lighting equipment shall be provided to adequately illuminate the work area. Light towers shall be positioned and adjusted to aim away from ATCT cabs and active runways to prevent blinding effects. Airside Operations will approve the set-up in order to ensure no impact to Air Traffic Control Tower or aircraft.
- C. Lockout/Tagout procedures are not permitted at night.

18. Protection Runway and Taxiway Areas, Zones and Surfaces (Section 2.22)

- A. **Runway Safety Area (RSA).** The Contractor staging area, haul route, and work site are outside the Runway Safety Area. No work shall be permitted within the RSA when the runway is active and opened to aircraft operations. No penetrations of the RSA shall be permitted unless approved by IAH Airport Operations. No work shall be conducted within a Runway Safety Area while the runway is open unless escorted by Airside Operations. Elevated Lighted Runway Closure X's shall be placed at each end of the runway, on top of the runway designations, or as shown on the plans, when the runway is closed to aircraft operations. Low-Profile Barricades and Taxiway Closure Markers shall be placed at the holding position on each taxiway connector that leads to an active runway when the taxiway is closed. No equipment or stockpiles shall be permitted in the RSA when the runway is active and opened to aircraft operations.
- B. **Runway Object Free Area (ROFA).** The Contractor staging area, haul route, and work site are outside the Runway Object Free Area. No work shall be permitted within the ROFA when the runway is active and opened to aircraft operations. No penetrations of the ROFA shall be permitted unless approved by IAH Airport Operations. The work site shall be delineated by barricades or other means approved by the airport. The Contractor personnel shall be limited to the identified work areas. Work activities within the ROFA shall be limited and coordinated 72 hours in advance with HAS Project Manager, IAH Airport Operations and ATCT. No equipment or stockpiles shall be permitted in the ROFA when the runway is active and opened to aircraft operations.
- C. **Taxiway Safety Area (TSA).** The work site is located outside of the TSA. The Contractor personnel shall be limited to the identified work areas. Work within the TSA shall require closure of the taxiway. No work shall be performed within the TSA when the taxiway is active and opened to aircraft operations. No equipment or stockpiles shall be permitted in

the TSA when the taxiway is active and opened to aircraft operations.

- D. **Taxiway Object Free Area (TOFA).** The work site adjacent to the TOFA shall be delineated by barricades or other means approved by the airport. The Contractor personnel shall be limited to the identified work areas. Work activities within the TOFA shall be limited and coordinated 72 hours in advance with HAS Project Manager, IAH Airport Operations and ATCT. No work shall be performed within the TOFA when the taxiway is active and opened to aircraft operations.
- E. **Obstacle Free Zone (OFZ).** The Contractor staging areas, haul routes, and work sites are outside the OFZ. Haul routes shall be coordinated in advance with HAS Project Manager, IAH Airport Operations and ATCT. The Contractor personnel shall be limited to the identified work areas and shall not cross through the OFZ unless under escort by IAH Airport Operations and approval of ATCT. No penetrations of the OFZ shall be permitted when the runway is active and opened to aircraft operations. In the event construction activities are required within the OFZ, Contractor shall coordinate closure of the runway with HAS Project Manager, IAH Airport Operations and ATCT a minimum of 72 hours in advance of planned construction activities.
- F. **Approach and Departure Surfaces.** The Contractor staging area, haul route, and work site are outside the approach and departure surfaces. No work shall be permitted within the approach or departure surfaces when the runway is active and opened to aircraft operations. No penetrations of the approach and departure surfaces shall be permitted. In the event construction activities are required within the approach / departure surfaces, Contractor shall coordinate closure of the runway with HAS Project Manager, IAH Airport Operations and ATCT a minimum of 72 hours in advance of planned construction activities.

19. Other Limitations on Construction (*Section 2.23*)

A. Prohibitions

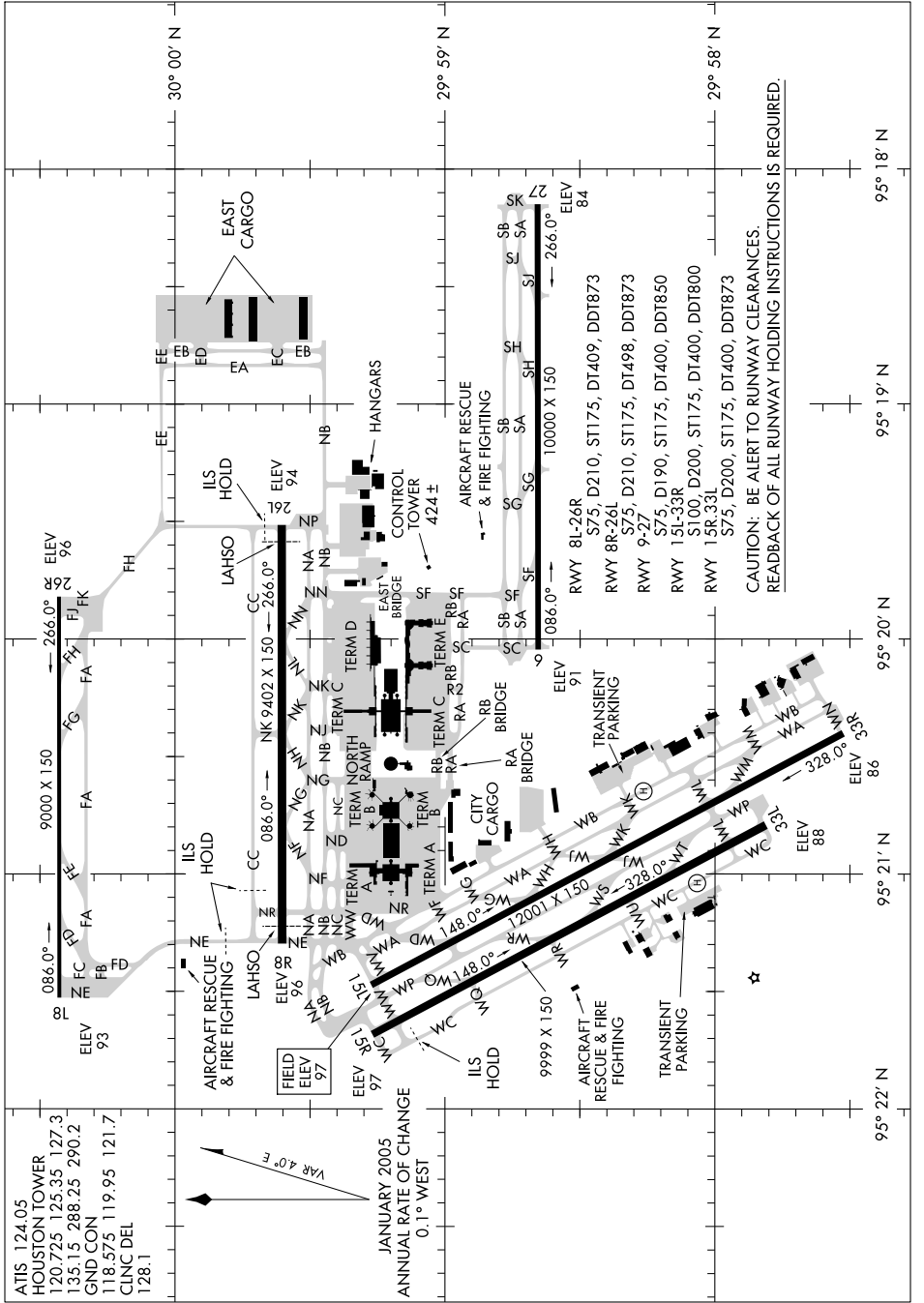
1. All construction equipment requires the submission of a 7460.
2. The use of open flames, torching or welding is not allowed unless approved in advance by Airside Operations.
3. The use of explosives is not allowed.

B. Restrictions

1. All construction operations, hauling, and transporting across active runways, taxiways, and taxilanes shall be under control of a designated IAH Airport Operations escort. Airport Operations will be the point of contact.
2. All construction activities shall be halted in the event weather conditions require SMGCS operations or other visibility limiting phenomena occurs. Construction activity will be permitted to resume when weather conditions are favorable for normal airport operations. Refer to Section 13: Special Conditions.

Appendix A. Airport Diagram & Project Exhibit

SC-5, 15 MAR 2007 to 12 APR 2007



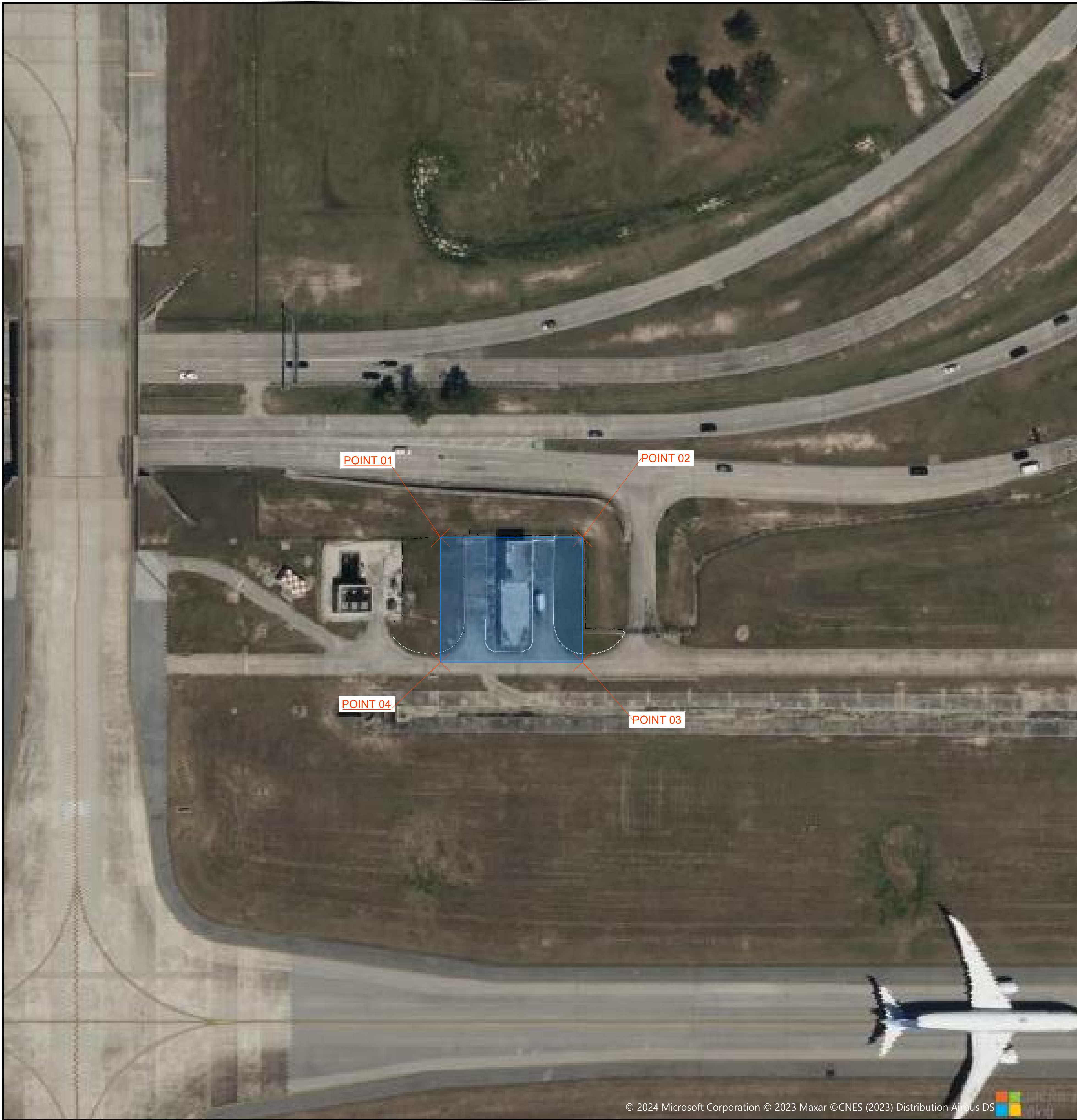
SC-5, 15 MAR 2007 to 12 APR 2007

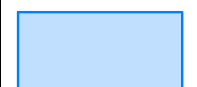

ATIS 124.05
HOUSTON TOWER 120.725 125.35 127.3
135.15 288.25 290.2
GND CON 118.575 119.95 121.7
CLNC DEL 128.1

VAR 40° E
JANUARY 2005
ANNUAL RATE OF CHANGE
0.1° WEST

CAUTION: BE ALERT TO RUNWAY CLEARANCES.
READBACK OF ALL RUNWAY HOLDING INSTRUCTIONS IS REQUIRED.

Appendix B. 7460s



LEGEND	
	PROJECT WORK AREA
	PROJECT WORK AREA

Point Table							
POINT #	EXISTING GROUND ELEVATION	MAX. EQUIPMENT HEIGHT	MAX. EQUIPMENT HEIGHT	Northing	Easting	LATITUDE	LONGITUDE
POINT 01	88.000	33'	121	13922497.9296	3130475.3090	N029° 58' 51.46"	W095° 19' 44.99"
POINT 02	88.000	33'	121	13922497.9296	3130609.7090	N029° 58' 51.42"	W095° 19' 43.46"
POINT 03	87.000	33'	120	13922379.6275	3130609.7090	N029° 58' 50.25"	W095° 19' 43.50"
POINT 04	86.000	33'	119	13922379.6275	3130475.3090	N029° 58' 50.29"	W095° 19' 45.03"

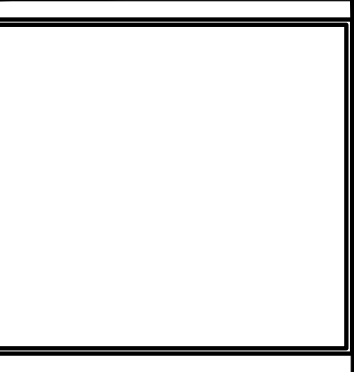
REVISIONS

NO.	DESCRIPTION	DATE

HOUSTON AIRPORT SYSTEM
PROJECT 952 SOUTH LIGHTING VAULT RENOVATION
GEORGE BUSH INTERCONTINENTAL AIRPORT / HOUSTON
4104 WILL CLAYTON PARKWAY, HOUSTON, TX 77032

7460 EXHIBIT
TEMPORARY FIELD SERVICES EQUIPMENT HEIGHT GEOTECHNICAL DRILL RIG

PROJECT MGR: AEO
DESIGNER: JM
DRAWN BY: JW
CHECK BY: EW
DATE: 02/01/24



APPROVED BY:

DIRECTOR
HOUSTON AIRPORT SYSTEM

JACOBS NO.

A.I.P. NO.

C.I.P. NO.

H.A.S. NO.

SHEET NO.

EXH - 1



LEGEND	
	PROJECT WORK AREA
	POINT

7460 Boundary Point Table							
POINT #	EXISTING GROUND ELEVATION	MAX. EQUIPMENT HEIGHT	MAX. EQUIPMENT HEIGHT	Northing	Easting	LATITUDE	LONGITUDE
POINT 01	88.000	33'	121	13922497.9296	3130475.3090	N029° 58' 51.46"	W095° 19' 44.99"
POINT 02	88.000	33'	121	13922497.9296	3130609.7090	N029° 58' 51.42"	W095° 19' 43.46"
POINT 03	87.000	33'	120	13922379.6275	3130609.7090	N029° 58' 50.25"	W095° 19' 43.50"
POINT 04	86.000	33'	119	13922379.6275	3130475.3090	N029° 58' 50.29"	W095° 19' 45.03"

Existing Building Point Table							
POINT #	EXISTING GROUND ELEVATION	MAX. STRUCTURE HEIGHT	MAX. STRUCTURE ELEVATION	Northing	Easting	LATITUDE	LONGITUDE
BUILDING 1	88.600	16	104.6	13922399.7323	3130527.4479	N029° 58' 50.47"	W095° 19' 44.43"
BUILDING 2	88.600	16	104.6	13922399.7323	3130562.1467	N029° 58' 50.46"	W095° 19' 44.03"
BUILDING 3	88.700	27	115.7	13922495.1987	3130562.1467	N029° 58' 51.40"	W095° 19' 44.00"
BUILDING 4	88.700	27	115.7	13922495.1986	3130527.4479	N029° 58' 51.42"	W095° 19' 44.39"

AOA Fence Point Table							
POINT #	EXISTING GROUND ELEVATION	MAX. STRUCTURE HEIGHT	MAX. STRUCTURE ELEVATION	Northing	Easting	LATITUDE	LONGITUDE
AOA FENCE 1	85.000	10'	95	13922410.4524	3130649.4205	N029° 58' 50.54"	W095° 19' 43.04"
AOA FENCE 2	88.000	10'	98	13922411.3507	3130612.5110	N029° 58' 50.56"	W095° 19' 43.46"
AOA FENCE 3	88.000	10'	98	13922499.3551	3130612.1712	N029° 58' 51.43"	W095° 19' 43.43"
AOA FENCE 4	88.000	10'	98	13922498.6083	3130474.8419	N029° 58' 51.47"	W095° 19' 44.99"
AOA FENCE 5	88.000	10'	98'	13922495.7875	3130439.3362	N029° 58' 51.45"	W095° 19' 45.40"
AOA FENCE 6	88.000	10'	98'	13922421.4805	3130439.1460	N029° 58' 50.71"	W095° 19' 45.42"

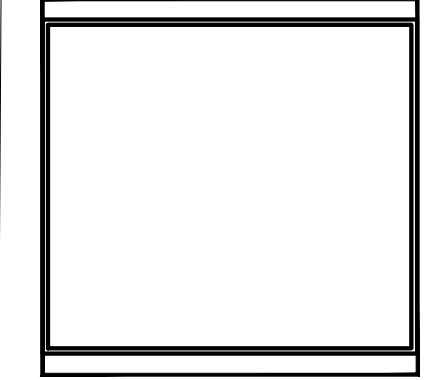
HOUSTON AIRPORT SYSTEM

818 Town & Country Blvd.
Suite 500
Houston, TX 77024
(281) 721-3400
www.jacobs.com TBPE Firm #2968

REVISIONS		
NO.	DESCRIPTION	DATE

HOUSTON AIRPORT SYSTEM
 PROJECT 952 SOUTH LIGHTING VAULT RENOVATION
 GEORGE BUSH INTERCONTINENTAL AIRPORT / HOUSTON
 4104 WILL CLAYTON PARKWAY, HOUSTON, TX 77032
 7460 EXHIBIT
 PERMANENT STRUCTURE HEIGHTS

PROJECT MGR: AEO
 DESIGNER: JM
 DRAWN BY: JW
 CHECK BY: EW
 DATE: 02/02/24



APPROVED BY:

 DIRECTOR
 HOUSTON AIRPORT SYSTEM
 JACOBS NO. _____
 A.I.P. NO. _____
 C.I.P. NO. _____
 H.A.S. NO. _____
 SHEET NO. _____

Appendix C. FAA AC 150/5370-2G



U.S. Department
of Transportation
**Federal Aviation
Administration**

Advisory Circular

Subject: Operational Safety on
Airports During Construction

Date: 12/13/2017
Initiated By: AAS-100

AC No: 150/5370-2G
Change:

1 **Purpose.**

This AC sets forth guidelines for operational safety on airports during construction.

2 **Cancellation.**

This AC cancels AC 150/5370-2F, *Operational Safety on Airports during Construction*, dated September 29, 2011.

3 **Application.**

This AC assists airport operators in complying with Title 14 Code of Federal Regulations (CFR) Part 139, *Certification of Airports*. For those certificated airports, this AC provides one way, but not the only way, of meeting those requirements. The use of this AC is mandatory for those airport construction projects receiving funds under the Airport Improvement Program (AIP). See Grant Assurance No. 34, *Policies, Standards, and Specifications*. While we do not require non-certificated airports without grant agreements or airports using Passenger Facility Charge (PFC) Program funds for construction projects to adhere to these guidelines, we recommend that they do so to help these airports maintain operational safety during construction.

4 **Related Documents.**

ACs and Orders referenced in the text of this AC do not include a revision letter, as they refer to the latest version. [Appendix A](#) contains a list of reading material on airport construction, design, and potential safety hazards during construction, as well as instructions for obtaining these documents.

5 **Principal Changes.**

The AC incorporates the following principal changes:

1. Notification about impacts to both airport owned and FAA-owned NAVAIDs was added. See paragraph [2.13.5.3](#), NAVAIDs.

2. Guidance for the use of orange construction signs was added. See paragraph 2.18.4.2, Temporary Signs.
3. Open trenches or excavations may be permitted in the taxiway safety area while the taxiway is open to aircraft operations, subject to restrictions. See paragraph 2.22.3.4, Excavations.
4. Guidance for temporary shortened runways and displaced thresholds has been enhanced. See Figure 2-1 and Figure 2-2.
5. Figures have been improved and a new Appendix F on the placement of orange construction signs has been added.

Hyperlinks (allowing the reader to access documents located on the internet and to maneuver within this document) are provided throughout this document and are identified with underlined text. When navigating within this document, return to the previously viewed page by pressing the “ALT” and “ ← ” keys simultaneously.

Figures in this document are schematic representations and are not to scale.

6 **Use of Metrics.**

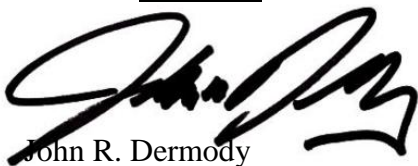
Throughout this AC, U.S. customary units are used followed with “soft” (rounded) conversion to metric units. The U.S. customary units govern.

7 **Where to Find this AC.**

You can view a list of all ACs at http://www.faa.gov/regulations_policies/advisory_circulars/. You can view the Federal Aviation Regulations at http://www.faa.gov/regulations_policies/faa_regulations/.

8 **Feedback on this AC.**

If you have suggestions for improving this AC, you may use the Advisory Circular Feedback form at the end of this AC.



John R. Dermody
Director of Airport Safety and Standards

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CHAPTER 1. PLANNING AN AIRFIELD CONSTRUCTION PROJECT

1.1 Overview.

Airports are complex environments, and procedures and conditions associated with construction activities often affect aircraft operations and can jeopardize operational safety. Safety considerations are paramount and may make operational impacts unavoidable. However, careful planning, scheduling, and coordination of construction activities can minimize disruption of normal aircraft operations and avoid situations that compromise the airport's operational safety. The airport operator must understand how construction activities and aircraft operations affect one another to be able to develop an effective plan to complete the project. While the guidance in this AC is primarily used for construction operations, the concepts, methods and procedures described may also enhance the day-to-day airport maintenance operations, such as lighting maintenance and snow removal operations.

1.2 Plan for Safety.

Safety, maintaining aircraft operations, and construction costs are all interrelated. Since safety must not be compromised, the airport operator must strike a balance between maintaining aircraft operations and construction costs. This balance will vary widely depending on the operational needs and resources of the airport and will require early coordination with airport users and the FAA. As the project design progresses, the necessary construction locations, activities, and associated costs will be identified and their impact to airport operations must be assessed. Adjustments are made to the proposed construction activities, often by phasing the project, and/or to airport operations to maintain operational safety. This planning effort will ultimately result in a project Construction Safety and Phasing Plan (CSPP). The development of the CSPP takes place through the following five steps:

1.2.1 Identify Affected Areas.

The airport operator must determine the geographic areas on the airport affected by the construction project. Some, such as a runway extension, will be defined by the project. Others may be variable, such as the location of haul routes and material stockpiles.

1.2.2 Describe Current Operations.

Identify the normal airport operations in each affected area for each phase of the project. This becomes the baseline from which the impact on operations by construction activities can be measured. This should include a narrative of the typical users and aircraft operating within the affected areas. It should also include information related to airport operations: the Aircraft Approach Category (AAC) and Airplane Design Group (ADG) of the airplanes that operate on each runway; the ADG and Taxiway Design Group (TDG)¹ for each affected taxiway; designated approach visibility minimums;

¹ Find Taxiway Design Group information in [AC 150/5300-13, Airport Design](#).

available approach and departure procedures; most demanding aircraft; declared distances; available air traffic control services; airport Surface Movement Guidance and Control System (SMGCS) plan; and others. The applicable seasons, days and times for certain operations should also be identified as applicable.

1.2.3 Allow for Temporary Changes to Operations.

To the extent practical, current airport operations should be maintained during the construction. In consultation with airport users, Aircraft Rescue and Fire Fighting (ARFF) personnel, and FAA Air Traffic Organization (ATO) personnel, the airport operator should identify and prioritize the airport's most important operations. The construction activities should be planned, through project phasing if necessary, to safely accommodate these operations. When the construction activities cannot be adjusted to safely maintain current operations, regardless of their importance, then the operations must be revised accordingly. Allowable changes include temporary revisions to approach procedures, restricting certain aircraft to specific runways and taxiways, suspension of certain operations, decreased weights for some aircraft due to shortened runways, and other changes. An example of a table showing temporary operations versus current operations is shown in Appendix E.

1.2.4 Take Required Measures to Revise Operations.

Once the level and type of aircraft operations to be maintained are identified, the airport operator must determine the measures required to safely conduct the planned operations during the construction. These measures will result in associated costs, which can be broadly interpreted to include not only direct construction costs, but also loss of revenue from impacted operations. Analysis of costs may indicate a need to reevaluate allowable changes to operations. As aircraft operations and allowable changes will vary widely among airports, this AC presents general guidance on those subjects.

1.2.5 Manage Safety Risk.

The FAA is committed to incorporating proactive safety risk management (SRM) tools into its decision-making processes. FAA Order 5200.11, *FAA Airports (ARP) Safety Management System (SMS)*, requires the FAA to conduct a Safety Assessment for certain triggering actions. Certain airport projects may require the airport operator to provide a Project Proposal Summary to help the FAA determine whether a Safety Assessment is required prior to FAA approval of the CSPP. The airport operator must coordinate with the appropriate FAA Airports Regional or District Office early in the development of the CSPP to determine the need for a Safety Risk Assessment. If the FAA requires an assessment, the airport operator must at a minimum:

1. Notify the appropriate FAA Airports Regional or District Office during the project "scope development" phase of any project requiring a CSPP.
2. Provide documents identified by the FAA as necessary to conduct SRM.
3. Participate in the SRM process for airport projects.
4. Provide a representative to participate on the SRM panel.

5. Ensure that all applicable SRM identified risks elements are recorded and mitigated within the CSPP.

1.3 **Develop a Construction Safety and Phasing Plan (CSPP).**

Development of an effective CSPP will require familiarity with many other documents referenced throughout this AC. See Appendix A for a list of related reading material.

1.3.1 List Requirements.

A CSPP must be developed for each on-airfield construction project funded by the Airport Improvement Program (AIP) or located on an airport certificated under Part 139. For on-airfield construction projects at Part 139 airports funded without AIP funds, the preparation of a CSPP represents an acceptable method the certificate holder may use to meet Part 139 requirements during airfield construction activity. As per FAA Order 5200.11, projects that require Safety Assessments do not include construction, rehabilitation, or change of any facility that is entirely outside the air operations area, does not involve any expansion of the facility envelope and does not involve construction equipment, haul routes or placement of material in locations that require access to the air operations area, increase the facility envelope, or impact line-of-sight. Such facilities may include passenger terminals and parking or other structures. However, extraordinary circumstances may trigger the need for a Safety Assessment and a CSPP. The CSPP is subject to subsequent review and approval under the FAA's Safety Risk Management procedures (see paragraph 1.2.5).

1.3.2 Prepare a Safety Plan Compliance Document (SPCD).

The Safety Plan Compliance Document (SPCD) details how the contractor will comply with the CSPP. Also, it will not be possible to determine all safety plan details (for example specific hazard equipment and lighting, contractor's points of contact, construction equipment heights) during the development of the CSPP. The successful contractor must define such details by preparing an SPCD that the airport operator reviews for approval prior to issuance of a notice-to-proceed. The SPCD is a subset of the CSPP, similar to how a shop drawing review is a subset to the technical specifications.

1.3.3 Assume Responsibility for the CSPP.

The airport operator is responsible for establishing and enforcing the CSPP. The airport operator may use the services of an engineering consultant to help develop the CSPP. However, writing the CSPP cannot be delegated to the construction contractor. Only those details the airport operator determines cannot be addressed before contract award are developed by the contractor and submitted for approval as the SPCD. The SPCD does not restate nor propose differences to provisions already addressed in the CSPP.

1.4 **Who Is Responsible for Safety During Construction?**

1.4.1 Establish a Safety Culture.

Everyone has a role in operational safety on airports during construction: the airport operator, the airport's consultants, the construction contractor and subcontractors, airport users, airport tenants, ARFF personnel, Air Traffic personnel, including Technical Operations personnel, FAA Airports Division personnel, and others, such as military personnel at any airport supporting military operations (e.g. national guard or a joint use facility). Close communication and coordination between all affected parties is the key to maintaining safe operations. Such communication and coordination should start at the project scoping meeting and continue through the completion of the project. The airport operator and contractor should conduct onsite safety inspections throughout the project and immediately remedy any deficiencies, whether caused by negligence, oversight, or project scope change.

1.4.2 Assess Airport Operator's Responsibilities.

An airport operator has overall responsibility for all activities on an airport, including construction. This includes the predesign, design, preconstruction, construction, and inspection phases. Additional information on the responsibilities listed below can be found throughout this AC. The airport operator must:

- 1.4.2.1 Develop a CSPP that complies with the safety guidelines of Chapter 2, Construction Safety and Phasing Plans, and Chapter 3, Guidelines for Writing a CSPP. The airport operator may develop the CSPP internally or have a consultant develop the CSPP for approval by the airport operator. For tenant sponsored projects, approve a CSPP developed by the tenant or its consultant.
- 1.4.2.2 Require, review and approve the SPCD by the contractor that indicates how it will comply with the CSPP and provides details that cannot be determined before contract award.
- 1.4.2.3 Convene a preconstruction meeting with the construction contractor, consultant, airport employees and, if appropriate, tenant sponsor and other tenants to review and discuss project safety before beginning construction activity. The appropriate FAA representatives should be invited to attend the meeting. See AC 150/5370-12, Quality Management for Federally Funded Airport Construction Projects. (Note “FAA” refers to the Airports Regional or District Office, the Air Traffic Organization, Flight Standards Service, and other offices that support airport operations, flight regulations, and construction/environmental policies.)
- 1.4.2.4 Ensure contact information is accurate for each representative/point of contact identified in the CSPP and SPCD.
- 1.4.2.5 Hold weekly or, if necessary, daily safety meetings with all affected parties to coordinate activities.
- 1.4.2.6 Notify users, ARFF personnel, and FAA ATO personnel of construction and conditions that may adversely affect the operational safety of the airport via Notices to Airmen (NOTAM) and other methods, as appropriate. Convene a meeting for review and discussion if necessary.
- 1.4.2.7 Ensure construction personnel know applicable airport procedures and changes to those procedures that may affect their work.
- 1.4.2.8 Ensure that all temporary construction signs are located per the scheduled list for each phase of the project.
- 1.4.2.9 Ensure construction contractors and subcontractors undergo training required by the CSPP and SPCD.
- 1.4.2.10 Ensure vehicle and pedestrian operations addressed in the CSPP and SPCD are coordinated with airport tenants, the airport traffic control tower (ATCT), and construction contractors.
- 1.4.2.11 At certificated airports, ensure each CSPP and SPCD is consistent with Part 139.

- 1.4.2.12 Conduct inspections sufficiently frequently to ensure construction contractors and tenants comply with the CSPP and SPCD and that there are no altered construction activities that could create potential safety hazards.
 - 1.4.2.13 Take immediate action to resolve safety deficiencies.
 - 1.4.2.14 At airports subject to 49 CFR Part 1542, *Airport Security*, ensure construction access complies with the security requirements of that regulation.
 - 1.4.2.15 Notify appropriate parties when conditions exist that invoke provisions of the CSPP and SPCD (for example, implementation of low-visibility operations).
 - 1.4.2.16 Ensure prompt submittal of a Notice of Proposed Construction or Alteration (Form 7460-1) for conducting an aeronautical study of potential obstructions such as tall equipment (cranes, concrete pumps, other), stock piles, and haul routes. A separate form may be filed for each potential obstruction, or one form may be filed describing the entire construction area and maximum equipment height. In the latter case, a separate form must be filed for any object beyond or higher than the originally evaluated area/height. The FAA encourages online submittal of forms for expediency at <https://oeaaa.faa.gov/oeaaa/external/portal.jsp>. The appropriate FAA Airports Regional or District Office can provide assistance in determining which objects require an aeronautical study.
 - 1.4.2.17 Ensure prompt transmission of the Airport Sponsor Strategic Event Submission, FAA Form 6000-26, located at https://oeaaa.faa.gov/oeaaa/external/content/AIRPORT_SPONSOR_STRATEGIC_EVENT_SUBMISSION_FORM.pdf, to assure proper coordination for NAS Strategic Interruption per Service Level Agreement with ATO.
 - 1.4.2.18 Promptly notify the FAA Airports Regional or District Office of any proposed changes to the CSPP prior to implementation of the change. Changes to the CSPP require review and approval by the airport operator and the FAA. The FAA Airports Regional or District office will determine if further coordination within the FAA is needed. Coordinate with appropriate local and other federal government agencies, such as Environmental Protection Agency (EPA), Occupational Safety and Health Administration (OSHA), Transportation Security Administration (TSA), and the state environmental agency.
- 1.4.3 Define Construction Contractor's Responsibilities.
The contractor is responsible for complying with the CSPP and SPCD. The contractor must:

- 1.4.3.1 Submit a Safety Plan Compliance Document (SPCD) to the airport operator describing how it will comply with the requirements of the CSPP and supply any details that could not be determined before contract award. The SPCD must include a certification statement by the contractor, indicating an understanding of the operational safety requirements of the CSPP and the assertion of compliance with the approved CSPP and SPCD unless written approval is granted by the airport operator. Any construction practice proposed by the contractor that does not conform to the CSPP and SPCD may impact the airport's operational safety and will require a revision to the CSPP and SPCD and re-coordination with the airport operator and the FAA in advance.
- 1.4.3.2 Have available at all times copies of the CSPP and SPCD for reference by the airport operator and its representatives, and by subcontractors and contractor employees.
- 1.4.3.3 Ensure that construction personnel are familiar with safety procedures and regulations on the airport. Provide a point of contact who will coordinate an immediate response to correct any construction-related activity that may adversely affect the operational safety of the airport. Many projects will require 24-hour coverage.
- 1.4.3.4 Identify in the SPCD the contractor's on-site employees responsible for monitoring compliance with the CSPP and SPCD during construction. At least one of these employees must be on-site when active construction is taking place.
- 1.4.3.5 Conduct sufficient inspections to ensure construction personnel comply with the CSPP and SPCD and that there are no altered construction activities that could create potential safety hazards.
- 1.4.3.6 Restrict movement of construction vehicles and personnel to permitted construction areas by flagging, barricading, erecting temporary fencing, or providing escorts, as appropriate, and as specified in the CSPP and SPCD.
- 1.4.3.7 Ensure that no contractor employees, employees of subcontractors or suppliers, or other persons enter any part of the air operations area (AOA) from the construction site unless authorized.
- 1.4.3.8 Ensure prompt submittal through the airport operator of Form 7460-1 for the purpose of conducting an aeronautical study of contractor equipment such as tall equipment (cranes, concrete pumps, and other equipment), stock piles, and haul routes when different from cases previously filed by the airport operator. The FAA encourages online submittal of forms for expediency at <https://oeaaa.faa.gov/oeaaa/external/portal.jsp>.

- 1.4.3.9 Ensure that all necessary safety mitigations are understood by all parties involved, and any special requirements of each construction phase will be fulfilled per the approved timeframe.
- 1.4.3.10 Participate in pre-construction meetings to review construction limits, safety mitigations, NOTAMs, and understand all special airport operational needs during each phase of the project.

1.4.4 Define Tenant's Responsibilities.

If planning construction activities on leased property, Airport tenants, such as airline operators, fixed base operators, and FAA ATO/Technical Operations sponsoring construction are strongly encouraged to:

1. Develop, or have a consultant develop, a project specific CSPP and submit it to the airport operator. The airport operator may forgo a complete CSPP submittal and instead incorporate appropriate operational safety principles and measures addressed in the advisory circular within their tenant lease agreements.
2. In coordination with its contractor, develop an SPCD and submit it to the airport operator for approval issued prior to issuance of a Notice to Proceed.
3. Ensure that construction personnel are familiar with safety procedures and regulations on the airport during all phases of the construction.
4. Provide a point of contact of who will coordinate an immediate response to correct any construction-related activity that may adversely affect the operational safety of the airport.
5. Identify in the SPCD the contractor's on-site employees responsible for monitoring compliance with the CSPP and SPCD during construction. At least one of these employees must be on-site when active construction is taking place.
6. Ensure that no tenant or contractor employees, employees of subcontractors or suppliers, or any other persons enter any part of the AOA from the construction site unless authorized.
7. Restrict movement of construction vehicles to construction areas by flagging and barricading, erecting temporary fencing, or providing escorts, as appropriate, as specified in the CSPP and SPCD.
8. Ensure prompt submittal through the airport operator of Form 7460-1 for conducting an aeronautical study of contractor equipment such as tall equipment (cranes, concrete pumps, other), stock piles, and haul routes. The FAA encourages online submittal of forms for expediency at <https://oeaaa.faa.gov/oeaaa/external/portal.jsp>.
9. Participate in pre-construction meetings to review construction limits, safety mitigations, NOTAMs, and understand all special airport operational needs during each phase of the project.

CHAPTER 2. CONSTRUCTION SAFETY AND PHASING PLANS

2.1 **Overview.**

Aviation safety is the primary consideration at airports, especially during construction. The airport operator's CSPP and the contractor's Safety Plan Compliance Document (SPCD) are the primary tools to ensure safety compliance when coordinating construction activities with airport operations. These documents identify all aspects of the construction project that pose a potential safety hazard to airport operations and outline respective mitigation procedures for each hazard. They must provide information necessary for the Airport Operations department to conduct airfield inspections and expeditiously identify and correct unsafe conditions during construction. All aviation safety provisions included within the project drawings, contract specifications, and other related documents must also be reflected in the CSPP and SPCD.

2.2 **Assume Responsibility.**

Operational safety on the airport remains the airport operator's responsibility at all times. The airport operator must develop, certify, and submit for FAA approval each CSPP. It is the airport operator's responsibility to apply the requirements of the FAA approved CSPP. The airport operator must revise the CSPP when conditions warrant changes and must submit the revised CSPP to the FAA for approval. The airport operator must also require and approve a SPCD from the project contractor.

2.3 **Submit the CSPP.**

Construction Safety and Phasing Plans should be developed concurrently with the project design. Milestone versions of the CSPP should be submitted for review and approval as follows. While these milestones are not mandatory, early submission will help to avoid delays. Submittals are preferred in 8.5 × 11 inch or 11 × 17 inch format for compatibility with the FAA's Obstruction Evaluation / Airport Airspace Analysis (OE / AAA) process.

2.3.1 Submit an Outline/Draft.

By the time approximately 25% to 30% of the project design is completed, the principal elements of the CSPP should be established. Airport operators are encouraged to submit an outline or draft, detailing all CSPP provisions developed to date, to the FAA for review at this stage of the project design.

2.3.2 Submit a CSPP.

The CSPP should be formally submitted for FAA approval when the project design is 80 percent to 90 percent complete. Since provisions in the CSPP will influence contract costs, it is important to obtain FAA approval in time to include all such provisions in the procurement contract.

2.3.3 Submit an SPCD.

The contractor should submit the SPCD to the airport operator for approval to be issued prior to the Notice to Proceed.

2.3.4 Submit CSPP Revisions.

All revisions to a previously approved CSPP must be re-submitted to the FAA for review and approval/disapproval action.

2.4 **Meet CSPP Requirements.**

2.4.1 To the extent possible, the CSPP should address the following as outlined in Chapter 3, Guidelines for Writing a CSPP. Details that cannot be determined at this stage are to be included in the SPCD.

1. Coordination.
 - a. Contractor progress meetings.
 - b. Scope or schedule changes.
 - c. FAA ATO coordination.
2. Phasing.
 - a. Phase elements.
 - b. Construction safety drawings.
3. Areas and operations affected by the construction activity.
 - a. Identification of affected areas.
 - b. Mitigation of effects.
4. Protection of navigation aids (NAVAIDs).
5. Contractor access.
 - a. Location of stockpiled construction materials.
 - b. Vehicle and pedestrian operations.
6. Wildlife management.
 - a. Trash.
 - b. Standing water.
 - c. Tall grass and seeds.
 - d. Poorly maintained fencing and gates.
 - e. Disruption of existing wildlife habitat.
7. Foreign Object Debris (FOD) management.
8. Hazardous materials (HAZMAT) management.
9. Notification of construction activities.

- a. Maintenance of a list of responsible representatives/ points of contact.
 - b. NOTAM.
 - c. Emergency notification procedures.
 - d. Coordination with ARFF Personnel.
 - e. Notification to the FAA.
10. Inspection requirements.
 - a. Daily (or more frequent) inspections.
 - b. Final inspections.
 11. Underground utilities.
 12. Penalties.
 13. Special conditions.
 14. Runway and taxiway visual aids. Marking, lighting, signs, and visual NAVAIDs.
 - a. General.
 - b. Markings.
 - c. Lighting and visual NAVAIDs.
 - d. Signs, temporary, including orange construction signs, and permanent signs.
 15. Marking and signs for access routes.
 16. Hazard marking and lighting.
 - a. Purpose.
 - b. Equipment.
 17. Work zone lighting for nighttime construction (if applicable).
 18. Protection of runway and taxiway safety areas, object free areas, obstacle free zones, and approach/departure surfaces.
 - a. Runway Safety Area (RSA).
 - b. Runway Object Free Area (ROFA).
 - c. Taxiway Safety Area (TSA). Provide details for any adjustments to Taxiway Safety Area width to allow continued operation of smaller aircraft. See paragraph 2.22.3.
 - d. Taxiway Object Free Area (TOFA). Provide details for any continued aircraft operations while construction occurs within the TOFA. See paragraph 2.22.4.
 - e. Obstacle Free Zone (OFZ).
 - f. Runway approach/departure surfaces.
 19. Other limitations on construction.
 - a. Prohibitions.

b. Restrictions.

2.4.2 The Safety Plan Compliance Document (SPCD) should include a general statement by the construction contractor that he/she has read and will abide by the CSPP. In addition, the SPCD must include all supplemental information that could not be included in the CSPP prior to the contract award. The contractor statement should include the name of the contractor, the title of the project CSPP, the approval date of the CSPP, and a reference to any supplemental information (that is, “I, (Name of Contractor), have read the (Title of Project) CSPP, approved on (Date), and will abide by it as written and with the following additions as noted:”). The supplemental information in the SPCD should be written to match the format of the CSPP indicating each subject by corresponding CSPP subject number and title. If no supplemental information is necessary for any specific subject, the statement, “No supplemental information,” should be written after the corresponding subject title. The SPCD should not duplicate information in the CSPP:

1. Coordination. Discuss details of proposed safety meetings with the airport operator and with contractor employees and subcontractors.
2. Phasing. Discuss proposed construction schedule elements, including:
 - a. Duration of each phase.
 - b. Daily start and finish of construction, including “night only” construction.
 - c. Duration of construction activities during:
 - i. Normal runway operations.
 - ii. Closed runway operations.
 - iii. Modified runway “Aircraft Reference Code” usage.
3. Areas and operations affected by the construction activity. These areas and operations should be identified in the CSPP and should not require an entry in the SPCD.
4. Protection of NAVAIDs. Discuss specific methods proposed to protect operating NAVAIDs.
5. Contractor access. Provide the following:
 - a. Details on how the contractor will maintain the integrity of the airport security fence (gate guards, daily log of construction personnel, and other).
 - b. Listing of individuals requiring driver training (for certificated airports and as requested).
 - c. Radio communications.
 - i. Types of radios and backup capabilities.
 - ii. Who will be monitoring radios.
 - iii. Who to contact if the ATCT cannot reach the contractor’s designated person by radio.

- d. Details on how the contractor will escort material delivery vehicles.
6. Wildlife management. Discuss the following:
 - a. Methods and procedures to prevent wildlife attraction.
 - b. Wildlife reporting procedures.
7. Foreign Object Debris (FOD) management. Discuss equipment and methods for control of FOD, including construction debris and dust.
8. Hazardous Materials (HAZMAT) management. Discuss equipment and methods for responding to hazardous spills.
9. Notification of construction activities. Provide the following:
 - a. Contractor points of contact.
 - b. Contractor emergency contact.
 - c. Listing of tall or other requested equipment proposed for use on the airport and the timeframe for submitting 7460-1 forms not previously submitted by the airport operator.
 - d. Batch plant details, including 7460-1 submittal.
10. Inspection requirements. Discuss daily (or more frequent) inspections and special inspection procedures.
11. Underground utilities. Discuss proposed methods of identifying and protecting underground utilities.
12. Penalties. Penalties should be identified in the CSPP and should not require an entry in the SPCD.
13. Special conditions. Discuss proposed actions for each special condition identified in the CSPP.
14. Runway and taxiway visual aids. Including marking, lighting, signs, and visual NAVAIDs. Discuss proposed visual aids including the following:
 - a. Equipment and methods for covering signage and airfield lights.
 - b. Equipment and methods for temporary closure markings (paint, fabric, other).
 - c. Temporary orange construction signs.
 - d. Types of temporary Visual Guidance Slope Indicators (VGSI).
15. Marking and signs for access routes. Discuss proposed methods of demarcating access routes for vehicle drivers.
16. Hazard marking and lighting. Discuss proposed equipment and methods for identifying excavation areas.
17. Work zone lighting for nighttime construction (if applicable). Discuss proposed equipment, locations, aiming, and shielding to prevent interference with air traffic control and aircraft operations.

18. Protection of runway and taxiway safety areas, object free areas, obstacle free zones, and approach/departure surfaces. Discuss proposed methods of identifying, demarcating, and protecting airport surfaces including:
 - a. Equipment and methods for maintaining Taxiway Safety Area standards.
 - b. Equipment and methods to ensure the safe passage of aircraft where Taxiway Safety Area or Taxiway Object Free Area standards cannot be maintained.
 - c. Equipment and methods for separation of construction operations from aircraft operations, including details of barricades.
19. Other limitations on construction should be identified in the CSPP and should not require an entry in the SPCD.

2.5 **Coordination.**

Airport operators, or tenants responsible for design, bidding and conducting construction on their leased properties, should ensure at all project developmental stages, such as predesign, prebid, and preconstruction conferences, they capture the subject of airport operational safety during construction (see [AC 150/5370-12, *Quality Management for Federally Funded Airport Construction Projects*](#)). In addition, the following should be coordinated as required:

2.5.1 Progress Meetings.

Operational safety should be a standing agenda item for discussion during progress meetings throughout the project developmental stages.

2.5.2 Scope or Schedule Changes.

Changes in the scope or duration at any of the project stages may require revisions to the CSPP and review and approval by the airport operator and the FAA (see paragraph [1.4.2.17](#)).

2.5.3 FAA ATO Coordination.

Early coordination with FAA ATO is highly recommended during the design phase and is required for scheduling Technical Operations shutdowns prior to construction. Coordination is critical to restarts of NAVAID services and to the establishment of any special procedures for the movement of aircraft. Formal agreements between the airport operator and appropriate FAA offices are recommended. All relocation or adjustments to NAVAIDs, or changes to final grades in critical areas, should be coordinated with FAA ATO and may require an FAA flight inspection prior to restarting the facility. Flight inspections must be coordinated and scheduled well in advance of the intended facility restart. Flight inspections may require a reimbursable agreement between the airport operator and FAA ATO. Reimbursable agreements should be coordinated a minimum of 12 months prior to the start of construction. (See paragraph [2.13.5.3.2](#) for required FAA notification regarding FAA-owned NAVAIDs.)

2.6 **Phasing.**

Once it has been determined what types and levels of airport operations will be maintained, the most efficient sequence of construction may not be feasible. In this case, the sequence of construction may be phased to gain maximum efficiency while allowing for the required operations. The development of the resulting construction phases should be coordinated with local Air Traffic personnel and airport users. The sequenced construction phases established in the CSPP must be incorporated into the project design and must be reflected in the contract drawings and specifications.

2.6.1 Phase Elements.

For each phase the CSPP should detail:

- Areas closed to aircraft operations.
- Duration of closures.
- Taxi routes and/or areas of reduced TSA and TOFA to reflect reduced ADG use.
- ARFF access routes.
- Construction staging, disposal, and cleanout areas.
- Construction access and haul routes.
- Impacts to NAVAIDs.
- Lighting, marking, and signing changes.
- Available runway length and/or reduced RSA and ROFA to reflect reduced ADG use.
- Declared distances (if applicable).
- Required hazard marking, lighting, and signing.
- Work zone lighting for nighttime construction (if applicable).
- Lead times for required notifications.

2.6.2 Construction Safety Drawings.

Drawings specifically indicating operational safety procedures and methods in affected areas (i.e., construction safety drawings) should be developed for each construction phase. Such drawings should be included in the CSPP as referenced attachments and should also be included in the contract drawing package.

2.7 **Areas and Operations Affected by Construction Activity.**

Runways and taxiways should remain in use by aircraft to the maximum extent possible without compromising safety. Pre-meetings with the FAA ATO will support operational simulations. See Appendix E for an example of a table showing temporary operations versus current operations. The tables in Appendix E can be useful for coordination among all interested parties, including FAA Lines of Business.

2.7.1 Identification of Affected Areas.

Identifying areas and operations affected by the construction helps to determine possible safety problems. The affected areas should be identified in the construction safety drawings for each construction phase. (See paragraph 2.6.2.) Of particular concern are:

2.7.1.1 **Closing, or Partial Closing, of Runways, Taxiways and Aprons, and Displaced Thresholds.**

When a runway is partially closed, a portion of the pavement is unavailable for any aircraft operation, meaning taxiing, landing, or takeoff in either direction on that pavement is prohibited. A displaced threshold, by contrast, is established to ensure obstacle clearance and adequate safety area for landing aircraft. The pavement prior to the displaced threshold is normally available for take-off in the direction of the displacement and for landing and takeoff in the opposite direction. Misunderstanding this difference, may result in issuance of an inaccurate NOTAM, and can lead to a hazardous condition.

2.7.1.1.1 Partially Closed Runways.

The temporarily closed portion of a partially closed runway will generally extend from the threshold to a taxiway that may be used for entering and exiting the runway. If the closed portion extends to a point between taxiways, pilots will have to back-taxi on the runway, which is an undesirable operation. See Figure 2-1 for a desirable configuration.

2.7.1.1.2 Displaced Thresholds.

Since the portion of the runway pavement between the permanent threshold and a standard displaced threshold is available for takeoff and for landing in the opposite direction, the temporary displaced threshold need not be located at an entrance/exit taxiway. See Figure 2-2.

2.7.1.2 Closing of aircraft rescue and fire fighting access routes.

2.7.1.3 Closing of access routes used by airport and airline support vehicles.

2.7.1.4 Interruption of utilities, including water supplies for fire fighting.

2.7.1.5 Approach/departure surfaces affected by heights of objects.

2.7.1.6 Construction areas, storage areas, and access routes near runways, taxiways, aprons, or helipads.

Figure 2-1. Temporary Partially Closed Runway

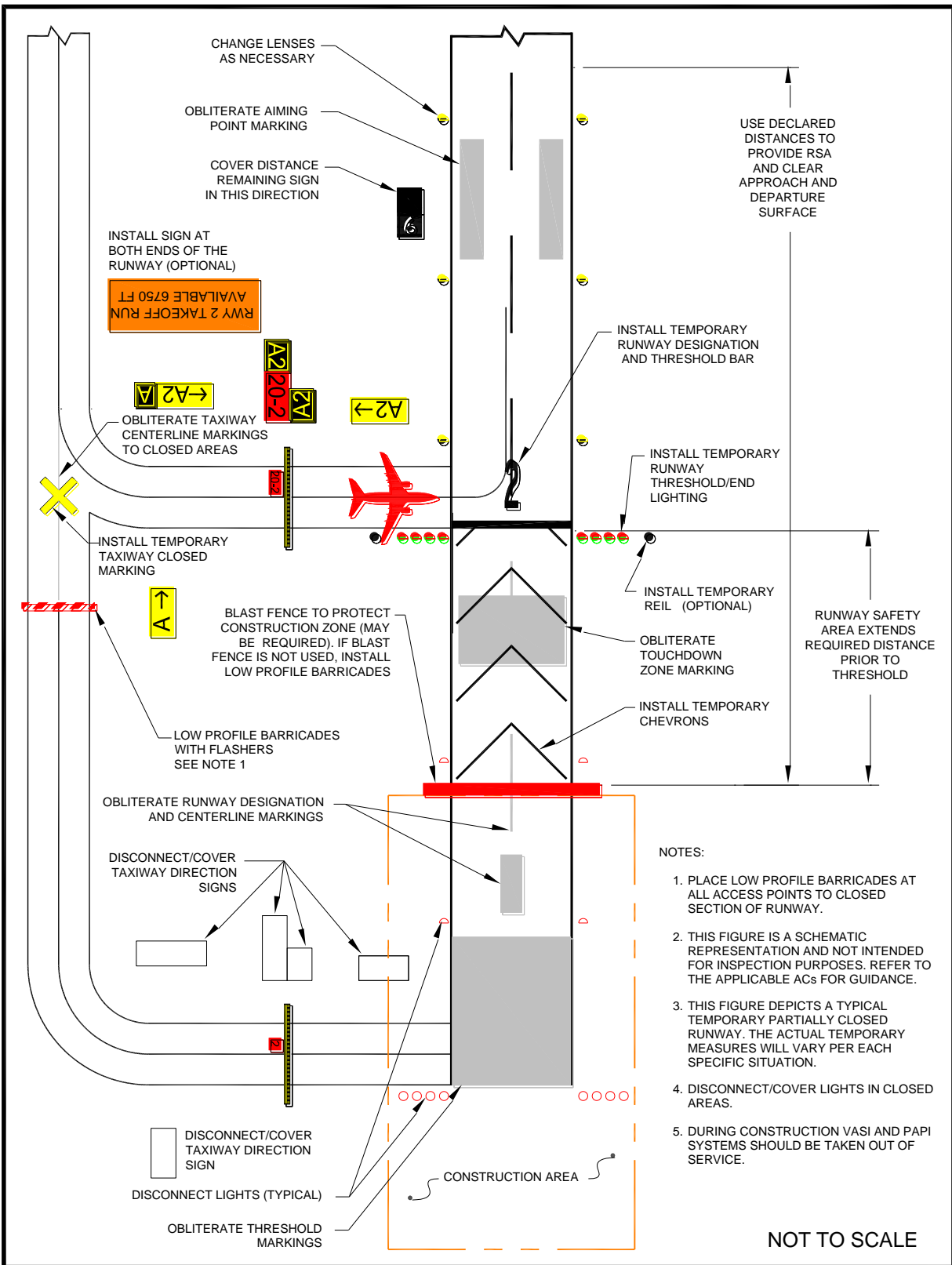
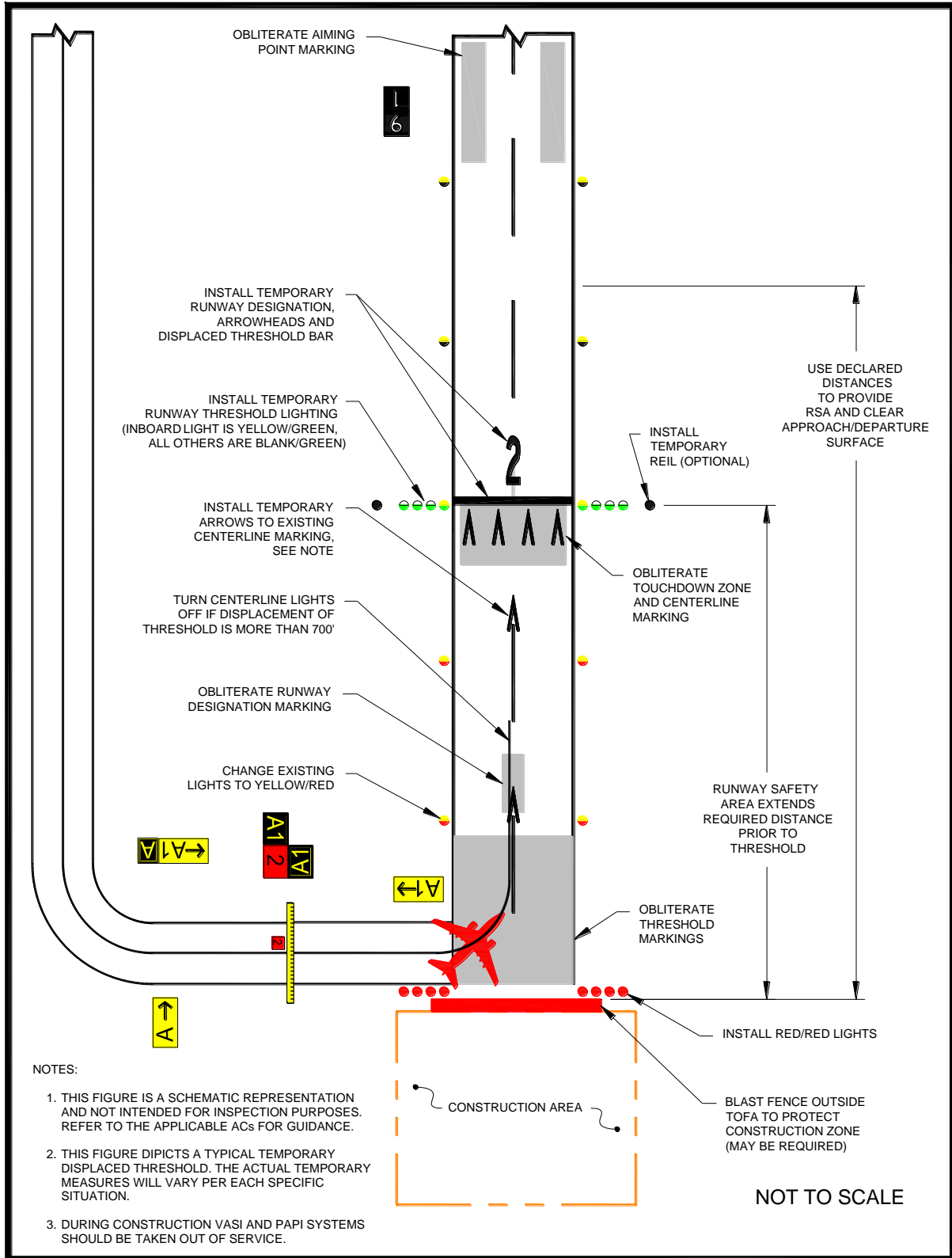


Figure 2-2. Temporary Displaced Threshold



Note: See paragraph 2.18.2.5.

2.7.2 Mitigation of Effects.

Establishment of specific procedures is necessary to maintain the safety and efficiency of airport operations. The CSPP must address:

- 2.7.2.1 Temporary changes to runway and/or taxi operations.
- 2.7.2.2 Detours for ARFF and other airport vehicles.
- 2.7.2.3 Maintenance of essential utilities.
- 2.7.2.4 Temporary changes to air traffic control procedures. Such changes must be coordinated with the ATO.

2.8 **Navigation Aid (NAVAID) Protection.**

Before commencing construction activity, parking vehicles, or storing construction equipment and materials near a NAVAID, coordinate with the appropriate FAA ATO/Technical Operations office to evaluate the effect of construction activity and the required distance and direction from the NAVAID. (See paragraph 2.13.5.3.) Construction activities, materials/equipment storage, and vehicle parking near electronic NAVAIDs require special consideration since they may interfere with signals essential to air navigation. If any NAVAID may be affected, the CSPP and SPCD must show an understanding of the “critical area” associated with each NAVAID and describe how it will be protected. Where applicable, the operational critical areas of NAVAIDs should be graphically delineated on the project drawings. Pay particular attention to stockpiling material, as well as to movement and parking of equipment that may interfere with line of sight from the ATCT or with electronic emissions. Interference from construction equipment and activities may require NAVAID shutdown or adjustment of instrument approach minimums for low visibility operations. This condition requires that a NOTAM be filed (see paragraph 2.13.2.) Construction activities and materials/equipment storage near a NAVAID must not obstruct access to the equipment and instruments for maintenance. Submittal of a 7460-1 form is required for construction vehicles operating near FAA NAVAIDs. (See paragraph 2.13.5.3.)

2.9 **Contractor Access.**

The CSPP must detail the areas to which the contractor must have access, and explain how contractor personnel will access those areas. Specifically address:

2.9.1 Location of Stockpiled Construction Materials.

Stockpiled materials and equipment storage are not permitted within the RSA and OFZ, and if possible should not be permitted within the Object Free Area (OFA) of an operational runway. Stockpiling material in the OFA requires submittal of a 7460-1 form and justification provided to the appropriate FAA Airports Regional or District Office for approval. The airport operator must ensure that stockpiled materials and equipment adjacent to these areas are prominently marked and lighted during hours of restricted visibility or darkness. (See paragraph 2.18.2.) This includes determining and

verifying that materials are stabilized and stored at an approved location so as not to be a hazard to aircraft operations and to prevent attraction of wildlife and foreign object damage from blowing or tracked material. See paragraphs 2.10 and 2.11.

2.9.2 Vehicle and Pedestrian Operations.

The CSPP should include specific vehicle and pedestrian requirements. Vehicle and pedestrian access routes for airport construction projects must be controlled to prevent inadvertent or unauthorized entry of persons, vehicles, or animals onto the AOA. The airport operator should coordinate requirements for vehicle operations with airport tenants, contractors, and the FAA air traffic manager. In regard to vehicle and pedestrian operations, the CSPP should include the following, with associated training requirements:

2.9.2.1 **Construction Site Parking.**

Designate in advance vehicle parking areas for contractor employees to prevent any unauthorized entry of persons or vehicles onto the AOA. These areas should provide reasonable contractor employee access to the job site.

2.9.2.2 **Construction Equipment Parking.**

Contractor employees must park and service all construction vehicles in an area designated by the airport operator outside the OFZ and never in the safety area of an active runway or taxiway. Unless a complex setup procedure makes movement of specialized equipment infeasible, inactive equipment must not be parked on a closed taxiway or runway. If it is necessary to leave specialized equipment on a closed taxiway or runway at night, the equipment must be well lighted. Employees should also park construction vehicles outside the OFA when not in use by construction personnel (for example, overnight, on weekends, or during other periods when construction is not active). Parking areas must not obstruct the clear line of sight by the ATCT to any taxiways or runways under air traffic control nor obstruct any runway visual aids, signs, or navigation aids. The FAA must also study those areas to determine effects on airport design criteria, surfaces established by 14 CFR Part 77, Safe, Efficient Use, and Preservation of the Navigable Airspace (Part 77), and on NAVAIDs and Instrument Approach Procedures (IAP). See paragraph 2.13.1 for further information.

2.9.2.3 **Access and Haul Roads.**

Determine the construction contractor's access to the construction sites and haul roads. Do not permit the construction contractor to use any access or haul roads other than those approved. Access routes used by contractor vehicles must be clearly marked to prevent inadvertent entry to areas open to airport operations. Pay special attention to ensure that if construction traffic is to share or cross any ARFF routes that ARFF right of way is not impeded at any time, and that construction traffic on haul

roads does not interfere with NAVAIDs or approach surfaces of operational runways. Address whether access gates will be blocked or inoperative or if a rally point will be blocked or inaccessible.

- 2.9.2.4 Marking and lighting of vehicles in accordance with AC 150/5210-5, *Painting, Marking, and Lighting of Vehicles Used on an Airport*.
- 2.9.2.5 Description of proper vehicle operations on various areas under normal, lost communications, and emergency conditions.
- 2.9.2.6 Required escorts.
- 2.9.2.7 **Training Requirements for Vehicle Drivers to Ensure Compliance with the Airport Operator's Vehicle Rules and Regulations.**
Specific training should be provided to vehicle operators, including those providing escorts. See AC 150/5210-20, *Ground Vehicle Operations on Airports*, for information on training and records maintenance requirements.
- 2.9.2.8 **Situational Awareness.**
Vehicle drivers must confirm by personal observation that no aircraft is approaching their position (either in the air or on the ground) when given clearance to cross a runway, taxiway, or any other area open to airport operations. In addition, it is the responsibility of the escort vehicle driver to verify the movement/position of all escorted vehicles at any given time. At non-towered airports, all aircraft movements and flight operations rely on aircraft operators to self-report their positions and intentions. However, there is no requirement for an aircraft to have radio communications. Because aircraft do not always broadcast their positions or intentions, visual checking, radio monitoring, and situational awareness of the surroundings is critical to safety.
- 2.9.2.9 **Two-Way Radio Communication Procedures.**
- 2.9.2.9.1 General.
The airport operator must ensure that tenant and construction contractor personnel engaged in activities involving unescorted operation on aircraft movement areas observe the proper procedures for communications, including using appropriate radio frequencies at airports with and without ATCT. When operating vehicles on or near open runways or taxiways, construction personnel must understand the critical importance of maintaining radio contact, as directed by the airport operator, with:
1. Airport operations
 2. ATCT

3. Common Traffic Advisory Frequency (CTAF), which may include UNICOM, MULTICOM.
4. Automatic Terminal Information Service (ATIS). This frequency is useful for monitoring conditions on the airport. Local air traffic will broadcast information regarding construction related runway closures and “shortened” runways on the ATIS frequency.

2.9.2.9.2 Areas Requiring Two-Way Radio Communication with the ATCT.

Vehicular traffic crossing active movement areas must be controlled either by two-way radio with the ATCT, escort, flagman, signal light, or other means appropriate for the particular airport.

2.9.2.9.3 Frequencies to be Used.

The airport operator will specify the frequencies to be used by the contractor, which may include the CTAF for monitoring of aircraft operations. Frequencies may also be assigned by the airport operator for other communications, including any radio frequency in compliance with Federal Communications Commission requirements. At airports with an ATCT, the airport operator will specify the frequency assigned by the ATCT to be used between contractor vehicles and the ATCT.

2.9.2.9.4 Proper radio usage, including read back requirements.

2.9.2.9.5 Proper phraseology, including the International Phonetic Alphabet.

2.9.2.9.6 Light Gun Signals.

Even though radio communication is maintained, escort vehicle drivers must also familiarize themselves with ATCT light gun signals in the event of radio failure. See the FAA safety placard “Ground Vehicle Guide to Airport Signs and Markings.” This safety placard may be downloaded through the Runway Safety Program Web site at http://www.faa.gov/airports/runway_safety/publications/ (see “Signs & Markings Vehicle Dashboard Sticker”) or obtained from the FAA Airports Regional Office.

2.9.2.10 **Maintenance of the secured area of the airport, including:**

2.9.2.10.1 Fencing and Gates.

Airport operators and contractors must take care to maintain security during construction when access points are created in the security fencing to permit the passage of construction vehicles or personnel. Temporary gates should be equipped so they can be securely closed and locked to prevent access by animals and unauthorized people. Procedures should be in place to ensure that only authorized persons and vehicles have access to the AOA and to prohibit “piggybacking” behind another person or vehicle. The Department of Transportation (DOT) document DOT/FAA/AR-

00/52, *Recommended Security Guidelines for Airport Planning and Construction*, provides more specific information on fencing. A copy of this document can be obtained from the Airport Consultants Council, Airports Council International, or American Association of Airport Executives.

2.9.2.10.2 Badging Requirements.

Airports subject to 49 CFR Part 1542, *Airport Security*, must meet standards for access control, movement of ground vehicles, and identification of construction contractor and tenant personnel.

2.10 **Wildlife Management.**

The CSPP and SPCD must be in accordance with the airport operator's wildlife hazard management plan, if applicable. See AC 150/5200-33, *Hazardous Wildlife Attractants On or Near Airports*, and CertAlert 98-05, *Grasses Attractive to Hazardous Wildlife*. Construction contractors must carefully control and continuously remove waste or loose materials that might attract wildlife. Contractor personnel must be aware of and avoid construction activities that can create wildlife hazards on airports, such as:

2.10.1 Trash.

Food scraps must be collected from construction personnel activity.

2.10.2 Standing Water.

2.10.3 Tall Grass and Seeds.

Requirements for turf establishment can be at odds with requirements for wildlife control. Grass seed is attractive to birds. Lower quality seed mixtures can contain seeds of plants (such as clover) that attract larger wildlife. Seeding should comply with the guidance in AC 150/5370-10, *Standards for Specifying Construction of Airports*, Item T-901, Seeding. Contact the local office of the United States Department of Agriculture Soil Conservation Service or the State University Agricultural Extension Service (County Agent or equivalent) for assistance and recommendations. These agencies can also provide liming and fertilizer recommendations.

2.10.4 Poorly Maintained Fencing and Gates.

See paragraph 2.9.2.10.1.

2.10.5 Disruption of Existing Wildlife Habitat.

While this will frequently be unavoidable due to the nature of the project, the CSPP should specify under what circumstances (location, wildlife type) contractor personnel should immediately notify the airport operator of wildlife sightings.

2.11 Foreign Object Debris (FOD) Management.

Waste and loose materials, commonly referred to as FOD, are capable of causing damage to aircraft landing gears, propellers, and jet engines. Construction contractors must not leave or place FOD on or near active aircraft movement areas. Materials capable of creating FOD must be continuously removed during the construction project. Fencing (other than security fencing) or covers may be necessary to contain material that can be carried by wind into areas where aircraft operate. See AC 150/5210-24, *Foreign Object Debris (FOD) Management*.

2.12 Hazardous Materials (HAZMAT) Management.

Contractors operating construction vehicles and equipment on the airport must be prepared to expeditiously contain and clean-up spills resulting from fuel or hydraulic fluid leaks. Transport and handling of other hazardous materials on an airport also requires special procedures. See AC 150/5320-15, *Management of Airport Industrial Waste*.

2.13 Notification of Construction Activities.

The CSPP and SPCD must detail procedures for the immediate notification of airport users and the FAA of any conditions adversely affecting the operational safety of the airport. It must address the notification actions described below, as applicable.

2.13.1 List of Responsible Representatives/points of contact for all involved parties, and procedures for contacting each of them, including after hours.

2.13.2 NOTAMs.

Only the airport operator may initiate or cancel NOTAMs on airport conditions, and is the only entity that can close or open a runway. The airport operator must coordinate the issuance, maintenance, and cancellation of NOTAMs about airport conditions resulting from construction activities with tenants and the local air traffic facility (control tower, approach control, or air traffic control center), and must either enter the NOTAM into NOTAM Manager, or provide information on closed or hazardous conditions on airport movement areas to the FAA Flight Service Station (FSS) so it can issue a NOTAM. The airport operator must file and maintain a list of authorized representatives with the FSS. Refer to AC 150/5200-28, *Notices to Airmen (NOTAMs) for Airport Operators*, for a sample NOTAM form. Only the FAA may issue or cancel NOTAMs on shutdown or irregular operation of FAA owned facilities. Any person having reason to believe that a NOTAM is missing, incomplete, or inaccurate must notify the airport operator. See paragraph 2.7.1.1 about issuing NOTAMs for partially closed runways versus runways with displaced thresholds.

2.13.3 Emergency notification procedures for medical, fire fighting, and police response.

2.13.4 Coordination with ARFF.

The CSPP must detail procedures for coordinating through the airport sponsor with ARFF personnel, mutual aid providers, and other emergency services if construction requires:

1. The deactivation and subsequent reactivation of water lines or fire hydrants, or
2. The rerouting, blocking and restoration of emergency access routes, or
3. The use of hazardous materials on the airfield.

2.13.5 Notification to the FAA.

2.13.5.1 **Part 77.**

Any person proposing construction or alteration of objects that affect navigable airspace, as defined in Part 77, must notify the FAA. This includes construction equipment and proposed parking areas for this equipment (i.e., cranes, graders, other equipment) on airports. FAA Form 7460-1, *Notice of Proposed Construction or Alteration*, can be used for this purpose and submitted to the appropriate FAA Airports Regional or District Office. See Appendix A to download the form. Further guidance is available on the FAA web site at oeaaa.faa.gov.

2.13.5.2 **Part 157.**

With some exceptions, Title 14 CFR Part 157, *Notice of Construction, Alteration, Activation, and Deactivation of Airports*, requires that the airport operator notify the FAA in writing whenever a non-Federally funded project involves the construction of a new airport; the construction, realigning, altering, activating, or abandoning of a runway, landing strip, or associated taxiway; or the deactivation or abandoning of an entire airport. Notification involves submitting FAA Form 7480-1, *Notice of Landing Area Proposal*, to the nearest FAA Airports Regional or District Office. See Appendix A to download the form.

2.13.5.3 **NAVAIDs.**

For emergency (short-notice) notification about impacts to both airport owned and FAA owned NAVAIDs, contact: 866-432-2622.

2.13.5.3.1 Airport Owned/FAA Maintained.

If construction operations require a shutdown of 24 hours or greater in duration, or more than 4 hours daily on consecutive days, of a NAVAID owned by the airport but maintained by the FAA, provide a 45-day minimum notice to FAA ATO/Technical Operations prior to facility shutdown, using Strategic Event Coordination (SEC) Form 6000.26 contained within FAA Order 6000.15, *General Maintenance Handbook for National Airspace System (NAS) Facilities*.

2.13.5.3.2 FAA Owned.

1. The airport operator must notify the appropriate FAA ATO Service Area Planning and Requirements (P&R) Group a minimum of 45 days prior to implementing an event that causes impacts to NAVAIDs, using SEC Form 6000.26.
2. Coordinate work for an FAA owned NAVAID shutdown with the local FAA ATO/Technical Operations office, including any necessary reimbursable agreements and flight checks. Detail procedures that address unanticipated utility outages and cable cuts that could impact FAA NAVAIDs. Refer to active Service Level Agreement with ATO for specifics.

2.14 **Inspection Requirements.**

2.14.1 Daily Inspections.

Inspections should be conducted at least daily, but more frequently if necessary to ensure conformance with the CSPP. A sample checklist is provided in Appendix D, Construction Project Daily Safety Inspection Checklist. See also AC 150/5200-18, Airport Safety Self-Inspection. Airport operators holding a Part 139 certificate are required to conduct self-inspections during unusual conditions, such as construction activities, that may affect safe air carrier operations.

2.14.2 Interim Inspections.

Inspections should be conducted of all areas to be (re)opened to aircraft traffic to ensure the proper operation of lights and signs, for correct markings, and absence of FOD. The contractor should conduct an inspection of the work area with airport operations personnel. The contractor should ensure that all construction materials have been secured, all pavement surfaces have been swept clean, all transition ramps have been properly constructed, and that surfaces have been appropriately marked for aircraft to operate safely. Only if all items on the list meet with the airport operator's approval should the air traffic control tower be notified to open the area to aircraft operations. The contractor should be required to retain a suitable workforce and the necessary equipment at the work area for any last minute cleanup that may be requested by the airport operator prior to opening the area.

2.14.3 Final Inspections.

New runways and extended runway closures may require safety inspections at certificated airports prior to allowing air carrier service. Coordinate with the FAA Airport Certification Safety Inspector (ACSI) to determine if a final inspection will be necessary.

2.15 Underground Utilities.

The CSPP and/or SPCD must include procedures for locating and protecting existing underground utilities, cables, wires, pipelines, and other underground facilities in excavation areas. This may involve coordinating with public utilities and FAA ATO/Technical Operations. Note that “One Call” or “Miss Utility” services do not include FAA ATO/Technical Operations.

2.16 Penalties.

The CSPP should detail penalty provisions for noncompliance with airport rules and regulations and the safety plans (for example, if a vehicle is involved in a runway incursion). Such penalties typically include rescission of driving privileges or access to the AOA.

2.17 Special Conditions.

The CSPP must detail any special conditions that affect the operation of the airport and will require the activation of any special procedures (for example, low-visibility operations, snow removal, aircraft in distress, aircraft accident, security breach, Vehicle / Pedestrian Deviation (VPD) and other activities requiring construction suspension/resumption).

2.18 Runway and Taxiway Visual Aids.

This includes marking, lighting, signs, and visual NAVAIDs. The CSPP must ensure that areas where aircraft will be operating are clearly and visibly separated from construction areas, including closed runways. Throughout the duration of the construction project, verify that these areas remain clearly marked and visible at all times and that marking, lighting, signs, and visual NAVAIDs that are to continue to perform their functions during construction remain in place and operational. Visual NAVAIDs that are not serving their intended function during construction must be temporarily disabled, covered, or modified as necessary. The CSPP must address the following, as appropriate:

2.18.1 General.

Airport markings, lighting, signs, and visual NAVAIDs must be clearly visible to pilots, not misleading, confusing, or deceptive. All must be secured in place to prevent movement by prop wash, jet blast, wing vortices, and other wind currents and constructed of materials that will minimize damage to an aircraft in the event of inadvertent contact. Items used to secure such markings must be of a color similar to the marking.

2.18.2 Markings.

During the course of construction projects, temporary pavement markings are often required to allow for aircraft operations during or between work periods. During the design phase of the project, the designer should coordinate with the project manager,

airport operations, airport users, the FAA Airports project manager, and Airport Certification Safety Inspector for Part 139 airports to determine minimum temporary markings. The FAA Airports project manager will, wherever a runway is closed, coordinate with the appropriate FAA Flight Standards Office and disseminate findings to all parties. Where possible, the temporary markings on finish grade pavements should be placed to mirror the dimensions of the final markings. Markings must be in compliance with the standards of AC 150/5340-1, *Standards for Airport Markings*, except as noted herein. Runways and runway exit taxiways closed to aircraft operations are marked with a yellow X. The preferred visual aid to depict temporary runway closure is the lighted X signal placed on or near the runway designation numbers. (See paragraph 2.18.2.1.2.)

2.18.2.1 **Closed Runways and Taxiways.**

2.18.2.1.1 Permanently Closed Runways.

For runways, obliterate the threshold marking, runway designation marking, and touchdown zone markings, and place an X at each end and at 1,000-foot (300 m) intervals. For a multiple runway environment, if the lighted X on a designated number will be located in the RSA of an adjacent active runway, locate the lighted X farther down the closed runway to clear the RSA of the active runway. In addition, the closed runway numbers located in the RSA of an active runway must be marked with a flat yellow X.

2.18.2.1.2 Temporarily Closed Runways.

For runways that have been temporarily closed, place an X at each end of the runway directly on or as near as practicable to the runway designation numbers. For a multiple runway environment, if the lighted X on a designated number will be located in the RSA of an adjacent active runway, locate the lighted X farther down the closed runway to clear the RSA of the active runway. In addition, the closed runway numbers located in the RSA of an active runway must be marked with a flat yellow X. See Figure 2-3. See also paragraph 2.18.3.3.

2.18.2.1.3 Partially Closed Runways and Displaced Thresholds.

When threshold markings are needed to identify the temporary beginning of the runway that is available for landing, the markings must comply with AC 150/5340-1. An X is not used on a partially closed runway or a runway with a displaced threshold. See paragraph 2.7.1.1 for the difference between partially closed runways and runways with displaced thresholds. Because of the temporary nature of threshold displacement due to construction, it is not necessary to re-adjust the existing runway centerline markings to meet standard spacing for a runway with a visual approach. Some of the requirements below may be waived in the cases of low-activity airports and/or short duration changes that are measured in days rather than weeks. Consider whether the presence of an airport traffic

control tower allows for the development of special procedures. Contact the appropriate FAA Airports Regional or District Office for assistance.

Figure 2-3. Markings for a Temporarily Closed Runway

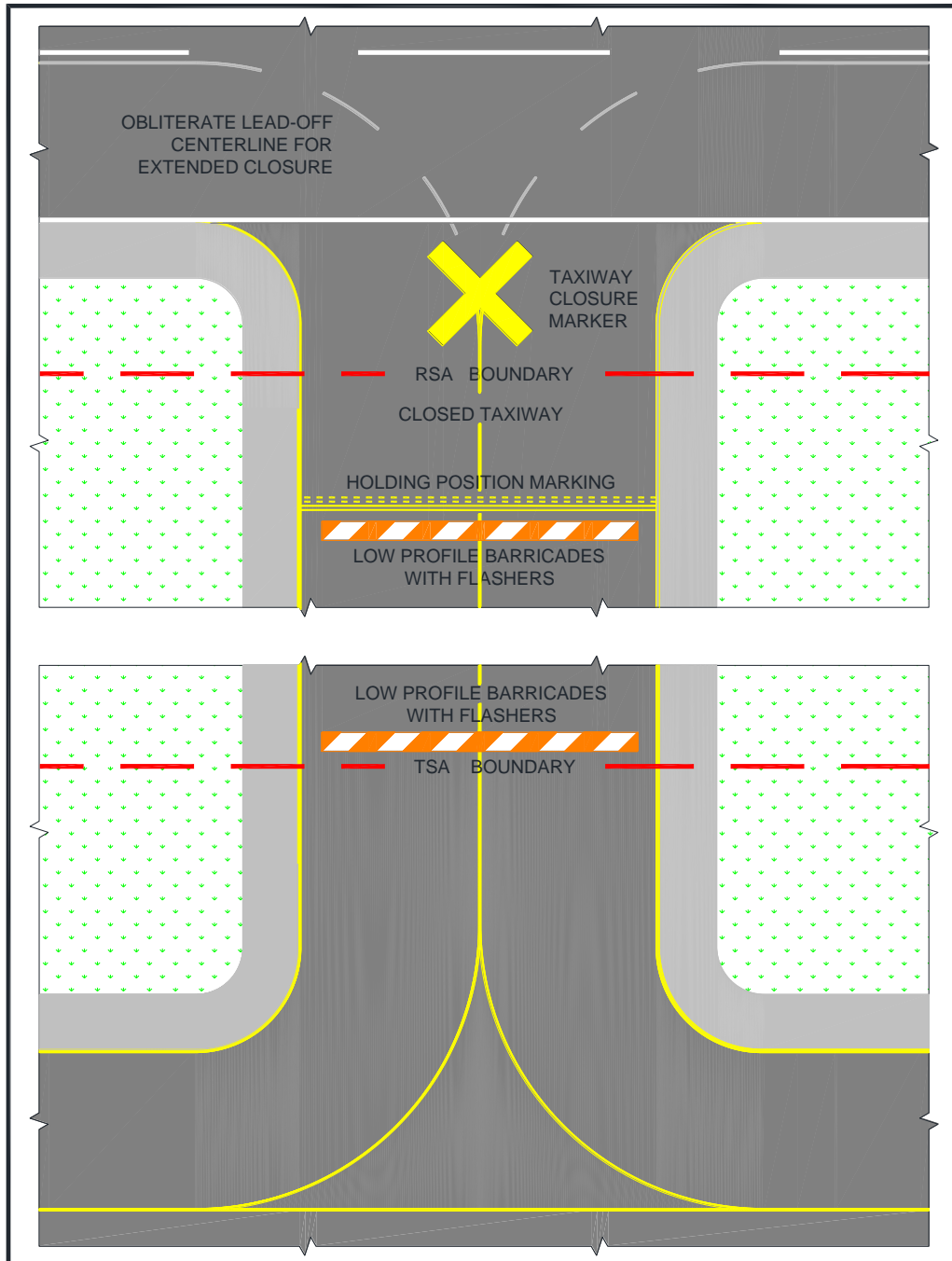


1. **Partially Closed Runways.** Pavement markings for temporary closed portions of the runway consist of a runway threshold bar, runway designation, and yellow chevrons to identify pavement areas that are unsuitable for takeoff or landing (see [AC 150/5340-1](#)). Obliterate or cover markings prior to the moved threshold. Existing touchdown zone markings beyond the moved threshold may remain in place. Obliterate aiming point markings. Issue appropriate NOTAMs regarding any nonstandard markings. See [Figure 2-4](#).
2. **Displaced Thresholds.** Pavement markings for a displaced threshold consist of a runway threshold bar, runway designation, and white arrowheads with and without arrow shafts. These markings are required to identify the portion of the runway before the displaced threshold to provide centerline guidance for pilots during approaches, takeoffs, and landing rollouts from the opposite direction. See [AC 150/5340-1](#). Obliterate markings prior to the displaced threshold. Existing touchdown zone markings beyond the displaced threshold may remain in place. Obliterate aiming point markings. Issue appropriate NOTAMs regarding any nonstandard markings. See [Figure 2-2](#).

2.18.2.1.4 Taxiways.

1. **Permanently Closed Taxiways.** *AC 150/5300-13 Airport Design*, notes that it is preferable to remove the pavement, but for pavement that is to remain, place an X at the entrance to both ends of the closed section. Obliterate taxiway centerline markings, including runway leadoff lines, leading to the closed taxiway. See [Figure 2-4](#).

Figure 2-4. Temporary Taxiway Closure



2. **Temporarily Closed Taxiways.** Place barricades outside the safety area of intersecting taxiways. For runway/taxiway intersections, place an X at the entrance to the closed taxiway from the runway. If the taxiway will be closed for an extended period, obliterate taxiway centerline markings, including runway leadoff lines and taxiway to taxiway turns, leading to the closed section. Always obliterate runway lead-off lines for high speed exits, regardless of the duration of the closure. If the centerline markings will be reused upon reopening the taxiway, it is preferable to paint over the marking. This will result in less damage to the pavement when the upper layer of paint is ultimately removed. See Figure 2-4.

2.18.2.1.5 Temporarily Closed Airport.

When the airport is closed temporarily, mark all the runways as closed.

- 2.18.2.2 If unable to paint temporary markings on the pavement, construct them from any of the following materials: fabric, colored plastic, painted sheets of plywood, or similar materials. They must be properly configured and appropriately secured to prevent movement by prop wash, jet blast, or other wind currents. Items used to secure such markings must be of a color similar to the marking.

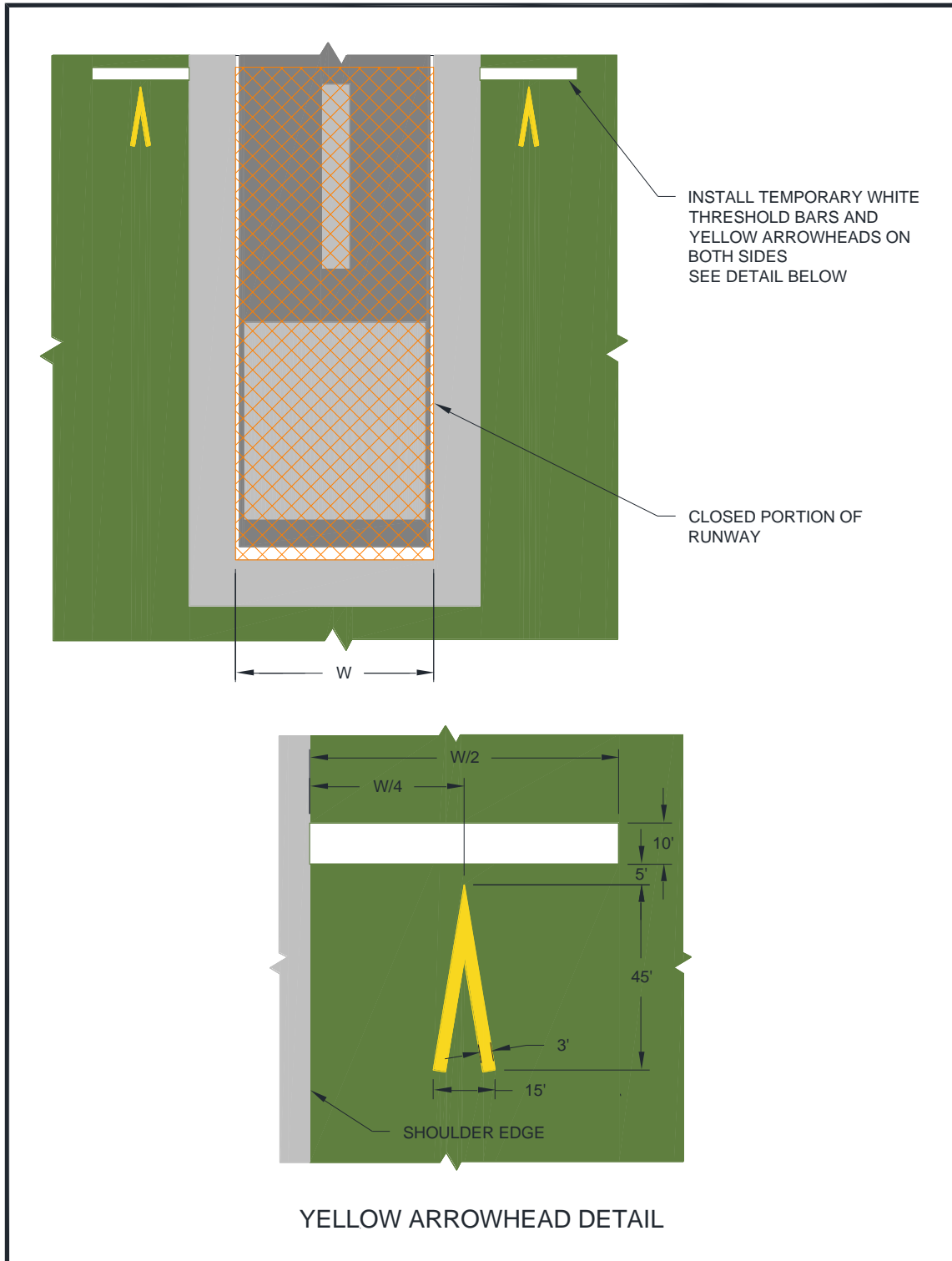
- 2.18.2.3 It may be necessary to remove or cover runway markings, including but not limited to, runway designation markings, threshold markings, centerline markings, edge stripes, touchdown zone markings and aiming point markings, depending on the length of construction and type of activity at the airport. When removing runway markings, apply the same treatment to areas between stripes or numbers, as the cleaned area will appear to pilots as a marking in the shape of the treated area.

- 2.18.2.4 If it is not possible to install threshold bars, chevrons, and arrows on the pavement, “temporary outboard white threshold bars and yellow arrowheads”, see Figure 2-5, may be used. Locate them outside of the runway pavement surface on both sides of the runway. The dimensions must be as shown in Figure 2-5. If the markings are not discernible on grass or snow, apply a black background with appropriate material over the ground to ensure they are clearly visible.

- 2.18.2.5 The application rate of paint to mark a short-term temporary runway and taxiway markings may deviate from the standard (see Item P-620, “Runway and Taxiway Painting,” in AC 150/5370-10), but the dimensions must meet the existing standards. When applying temporary markings at night, it is recommended that the fast curing, Type II paint be used to help offset the higher humidity and cooler temperatures often experienced at night. Diluting the paint will substantially increase cure time and is not recommended. Glass beads are not recommended for temporary markings. Striated markings may also be used for certain temporary markings. AC

150/5340-1, *Standards for Airport Markings*, has additional guidance on temporary markings.

Figure 2-5. Temporary Outboard White Threshold Bars and Yellow Arrowheads



2.18.3 Lighting and Visual NAVAIDs.

This paragraph refers to standard runway and taxiway lighting systems. See below for hazard lighting. Lighting installation must be in conformance with AC 150/5340-30, *Design and Installation Details for Airport Visual Aids*, and fixture design in conformance with AC 150/5345-50, *Specification for Portable Runway and Taxiway Lights*. When disconnecting runway and taxiway lighting fixtures, disconnect the associated isolation transformers. See AC 150/5340-26, *Maintenance of Airport Visual Aid Facilities*, for disconnect procedures and safety precautions. Alternately, cover the light fixture in such a way as to prevent light leakage. Avoid removing the lamp from energized fixtures because an excessive number of isolation transformers with open secondaries may damage the regulators and/or increase the current above its normal value. Secure, identify, and place any above ground temporary wiring in conduit to prevent electrocution and fire ignition sources. Maintain mandatory hold signs to operate normally in any situation where pilots or vehicle drivers could mistakenly be in that location. At towered airports certificated under Part 139, holding position signs are required to be illuminated on open taxiways crossing to closed or inactive runways. If the holding position sign is installed on the runway circuit for the closed runway, install a jumper to the taxiway circuit to provide power to the holding position sign for nighttime operations. Where it is not possible to maintain power to signs that would normally be operational, install barricades to exclude aircraft. Figure 2-1, Figure 2-2, Figure 2-3, and Figure 2-4 illustrate temporary changes to lighting and visual NAVAIDs.

2.18.3.1 **Permanently Closed Runways and Taxiways.**

For runways and taxiways that have been permanently closed, disconnect the lighting circuits.

2.18.3.2 **Temporarily Closed Runways and New Runways Not Yet Open to Air Traffic.**

If available, use a lighted X, both at night and during the day, placed at each end of the runway on or near the runway designation numbers facing the approach. (Note that the lighted X must be illuminated at all times that it is on a runway.) The use of a lighted X is required if night work requires runway lighting to be on. See AC 150/5345-55, *Specification for L-893, Lighted Visual Aid to Indicate Temporary Runway Closure*. For runways that have been temporarily closed, but for an extended period, and for those with pilot controlled lighting, disconnect the lighting circuits or secure switches to prevent inadvertent activation. For runways that will be opened periodically, coordinate procedures with the FAA air traffic manager or, at airports without an ATCT, the airport operator. Activate stop bars if available. Figure 2-6 shows a lighted X by day. Figure 2-7 shows a lighted X at night.

Figure 2-6. Lighted X in Daytime**Figure 2-7. Lighted X at Night**

2.18.3.3 **Partially Closed Runways and Displaced Thresholds.**

When a runway is partially closed, a portion of the pavement is unavailable for any aircraft operation, meaning taxiing and landing or taking off in either direction. A displaced threshold, by contrast, is put in place to ensure obstacle clearance by landing aircraft. The pavement prior to the displaced threshold is available for takeoff in the direction of the displacement, and for landing and takeoff in the opposite direction. Misunderstanding this difference and issuance of a subsequently inaccurate NOTAM can result in a hazardous situation. For both partially

closed runways and displaced thresholds, approach lighting systems at the affected end must be placed out of service.

- 2.18.3.3.1 Partially Closed Runways.
Disconnect edge and threshold lights on that part of the runway at and behind the threshold (that is, the portion of the runway that is closed). Alternately, cover the light fixtures in such a way as to prevent light leakage. See Figure 2-1.
- 2.18.3.3.2 Temporary Displaced Thresholds.
Edge lighting in the area of the displacement emits red light in the direction of approach and yellow light (white for visual runways) in the opposite direction. If the displacement is 700 feet or less, blank out centerline lights in the direction of approach or place the centerline lights out of service. If the displacement is over 700 feet, place the centerline lights out of service. See AC 150/5340-30 for details on lighting displaced thresholds. See Figure 2-2.
- 2.18.3.3.3 Temporary runway thresholds and runway ends must be lighted if the runway is lighted and it is the intended threshold for night landings or instrument meteorological conditions.
- 2.18.3.3.4 A temporary threshold on an unlighted runway may be marked by retroreflective, elevated markers in addition to markings noted in paragraph 2.18.2.1.3. Markers seen by aircraft on approach are green. Markers at the rollout end of the runway are red. At certificated airports, temporary elevated threshold markers must be mounted with a frangible fitting (see 14 CFR Part 139.309). At non-certificated airports, the temporary elevated threshold markings may either be mounted with a frangible fitting or be flexible. See AC 150/5345-39, *Specification for L-853, Runway and Taxiway Retroreflective Markers*.
- 2.18.3.3.5 Temporary threshold lights and runway end lights and related visual NAVAIDs are installed outboard of the edges of the full-strength pavement only when they cannot be installed on the pavement. They are installed with bases at grade level or as low as possible, but not more than 3 inch (7.6 cm) above ground. (The standard above ground height for airport lighting fixtures is 14 inches (35 cm)). When any portion of a base is above grade, place properly compacted fill around the base to minimize the rate of gradient change so aircraft can, in an emergency, cross at normal landing or takeoff speeds without incurring significant damage. See AC 150/5370-10.
- 2.18.3.3.6 Maintain threshold and edge lighting color and spacing standards as described in AC 150/5340-30. Battery powered, solar, or portable lights that meet the criteria in AC 150/5345-50 may be used. These systems are intended primarily for visual flight rules (VFR) aircraft operations but may

be used for instrument flight rules (IFR) aircraft operations, upon individual approval from the Flight Standards Division of the applicable FAA Regional Office.

- 2.18.3.3.7 When runway thresholds are temporarily displaced, reconfigure yellow lenses (caution zone), as necessary, and place the centerline lights out of service.
- 2.18.3.3.8 Relocate the Visual Glide Slope Indicator (VGSI), such as Visual Approach Slope Indicator (VASI) and Precision Approach Path Indicator (PAPI); other airport lights, such as Runway End Identifier Lights (REIL); and approach lights to identify the temporary threshold. Another option is to disable the VGSI or any equipment that would give misleading indications to pilots as to the new threshold location. Installation of temporary visual aids may be necessary to provide adequate guidance to pilots on approach to the affected runway. If the FAA owns and operates the VGSI, coordinate its installation or disabling with the local ATO/Technical Operations Office. Relocation of such visual aids will depend on the duration of the project and the benefits gained from the relocation, as this can result in great expense. See FAA JO 6850.2, *Visual Guidance Lighting Systems*, for installation criteria for FAA owned and operated NAVAIDs.
- 2.18.3.3.9 Issue a NOTAM to inform pilots of temporary lighting conditions.

2.18.3.4 **Temporarily Closed Taxiways.**

If possible, deactivate the taxiway lighting circuits. When deactivation is not possible (for example other taxiways on the same circuit are to remain open), cover the light fixture in a way as to prevent light leakage.

2.18.4 Signs.

To the extent possible, signs must be in conformance with AC 150/5345-44, *Specification for Runway and Taxiway Signs*, and AC 150/5340-18, *Standard for Airport Sign Systems*.

2.18.4.1 **Existing Signs.**

Runway exit signs are to be covered for closed runway exits. Outbound destination signs are to be covered for closed runways. Any time a sign does not serve its normal function or would provide conflicting information, it must be covered or removed to prevent misdirecting pilots. Note that information signs identifying a crossing taxiway continue to perform their normal function even if the crossing taxiway is closed. For long term construction projects, consider relocating signs, especially runway distance remaining signs.

2.18.4.2 **Temporary Signs.**

Orange construction signs comprise a message in black on an orange background. Orange construction signs may help pilots be aware of changed conditions. The airport operator may choose to introduce these signs as part of a movement area construction project to increase situational awareness when needed. Locate signs outside the taxiway safety limits and ahead of construction areas so pilots can take timely action. Use temporary signs judiciously, striking a balance between the need for information and the increase in pilot workload. When there is a concern of pilot “information overload,” the applicability of mandatory hold signs must take precedence over orange construction signs recommended during construction. Temporary signs must meet the standards for such signs in Engineering Brief 93, *Guidance for the Assembly and Installation of Temporary Orange Construction Signs*. Many criteria in AC 150/5345-44, *Specification for Runway and Taxiway Signs*, are referenced in the Engineering Brief. Permissible sign legends are:

1. CONSTRUCTION AHEAD,
2. CONSTRUCTION ON RAMP, and
3. RWY XX TAKEOFF RUN AVAILABLE XXX FT.

Phasing, supported by drawings and sign schedule, for the installation of orange construction signs must be included in the CSPP or SPCD.

2.18.4.2.1 Takeoff Run Available (TORA) signs.

Recommended: Where a runway has been shortened for takeoff, install orange TORA signs well before the hold lines, such as on a parallel taxiway prior to a turn to a runway hold position. See EB 93 for sign size and location.

2.18.4.2.2 Sign legends are shown in Figure F-1.

Note: See Figure E-1, Figure E-2, Figure E-3, Figure F-2, and Figure F-3 for examples of orange construction sign locations.

2.19 **Marking and Signs for Access Routes.**

The CSPP should indicate that pavement markings and signs for construction personnel will conform to AC 150/5340-18 and, to the extent practicable, with the Federal Highway Administration Manual on Uniform Traffic Control Devices (MUTCD) and/or State highway specifications. Signs adjacent to areas used by aircraft must comply with the frangibility requirements of AC 150/5220-23, *Frangible Connections*, which may require modification to size and height guidance in the MUTCD.

2.20 **Hazard Marking, Lighting and Signing.**

2.20.1 Hazard marking, lighting, and signing prevent pilots from entering areas closed to aircraft, and prevent construction personnel from entering areas open to aircraft. The CSPP must specify prominent, comprehensible warning indicators for any area affected by construction that is normally accessible to aircraft, personnel, or vehicles. Hazard marking and lighting must also be specified to identify open manholes, small areas under repair, stockpiled material, waste areas, and areas subject to jet blast. Also consider less obvious construction-related hazards and include markings to identify FAA, airport, and National Weather Service facilities cables and power lines; instrument landing system (ILS) critical areas; airport surfaces, such as RSA, OFA, and OFZ; and other sensitive areas to make it easier for contractor personnel to avoid these areas.

2.20.2 Equipment.

2.20.2.1 **Barricades.**

Low profile barricades, including traffic cones, (weighted or sturdily attached to the surface) are acceptable methods used to identify and define the limits of construction and hazardous areas on airports. Careful consideration must be given to selecting equipment that poses the least danger to aircraft but is sturdy enough to remain in place when subjected to typical winds, prop wash and jet blast. The spacing of barricades must be such that a breach is physically prevented barring a deliberate act. For example, if barricades are intended to exclude aircraft, gaps between barricades must be smaller than the wingspan of the smallest aircraft to be excluded; if barricades are intended to exclude vehicles, gaps between barricades must be smaller than the width of the excluded vehicles, generally 4 feet (1.2 meters). Provision must be made for ARFF access if necessary. If barricades are intended to exclude pedestrians, they must be continuously linked. Continuous linking may be accomplished through the use of ropes, securely attached to prevent FOD.

2.20.2.2 **Lights.**

Lights must be red, either steady burning or flashing, and must meet the luminance requirements of the State Highway Department. Batteries powering lights will last longer if lights flash. Lights must be mounted on barricades and spaced at no more than 10 feet (3 meters). Lights must be operated between sunset and sunrise and during periods of low visibility whenever the airport is open for operations. They may be operated by photocell, but this may require that the contractor turn them on manually during periods of low visibility during daytime hours.

2.20.2.3 **Supplement Barricades with Signs (for example) As Necessary.**

Examples are “No Entry” and “No Vehicles.” Be aware of the increased effects of wind and jet blast on barricades with attached signs.

2.20.2.4 **Air Operations Area – General.**

Barricades are not permitted in any active safety area or on the runway side of a runway hold line. Within a runway or taxiway object free area, and on aprons, use orange traffic cones, flashing or steady burning red lights as noted above, highly reflective collapsible barricades marked with diagonal, alternating orange and white stripes; and/or signs to separate all construction/maintenance areas from the movement area. Barricades may be supplemented with alternating orange and white flags at least 20 by 20 inch (50 by 50 cm) square and securely fastened to eliminate FOD. All barricades adjacent to any open runway or taxiway / taxilane safety area, or apron must be as low as possible to the ground, and no more than 18 inches high, exclusive of supplementary lights and flags. Barricades must be of low mass; easily collapsible upon contact with an aircraft or any of its components; and weighted or sturdily attached to the surface to prevent displacement from prop wash, jet blast, wing vortex, and other surface wind currents. If affixed to the surface, they must be frangible at grade level or as low as possible, but not to exceed 3 inch (7.6 cm) above the ground. [Figure 2-8](#) and [Figure 2-9](#) show sample barricades with proper coloring and flags.

Figure 2-8. Interlocking Barricades



Figure 2-9. Low Profile Barricades**2.20.2.5 Air Operations Area – Runway/Taxiway Intersections.**

Use highly reflective barricades with lights to close taxiways leading to closed runways. Evaluate all operating factors when determining how to mark temporary closures that can last from 10 to 15 minutes to a much longer period of time. However, even for closures of relatively short duration, close all taxiway/runway intersections with barricades. The use of traffic cones is appropriate for short duration closures.

2.20.2.6 Air Operations Area – Other.

Beyond runway and taxiway object free areas and aprons, barricades intended for construction vehicles and personnel may be many different shapes and made from various materials, including railroad ties, sawhorses, jersey barriers, or barrels.

2.20.2.7 Maintenance.

The construction specifications must include a provision requiring the contractor to have a person on call 24 hours a day for emergency maintenance of airport hazard lighting and barricades. The contractor must file the contact person's information with the airport operator. Lighting should be checked for proper operation at least once per day, preferably at dusk.

2.21 Work Zone Lighting for Nighttime Construction.

Lighting equipment must adequately illuminate the work area if the construction is to be performed during nighttime hours. Refer to [AC 150/5370-10](#) for minimum illumination levels for nighttime paving projects. Additionally, it is recommended that all support equipment, except haul trucks, be equipped with artificial illumination to safely

illuminate the area immediately surrounding their work areas. The lights should be positioned to provide the most natural color illumination and contrast with a minimum of shadows. The spacing must be determined by trial. Light towers should be positioned and adjusted to aim away from ATCT cabs and active runways to prevent blinding effects. Shielding may be necessary. Light towers should be removed from the construction site when the area is reopened to aircraft operations. Construction lighting units should be identified and generally located on the construction phasing plans in relationship to the ATCT and active runways and taxiways.

2.22 **Protection of Runway and Taxiway Safety Areas.**

Runway and taxiway safety areas, OFZs, OFAs, and approach surfaces are described in [AC 150/5300-13](#). Protection of these areas includes limitations on the location and height of equipment and stockpiled material. An FAA airspace study may be required. Coordinate with the appropriate FAA Airports Regional or District Office if there is any doubt as to requirements or dimensions (see paragraph [2.13.5](#)) as soon as the location and height of materials or equipment are known. The CSPP should include drawings showing all safety areas, object free areas, obstacle free zones and approach departure surfaces affected by construction.

2.22.1 Runway Safety Area (RSA).

A runway safety area is the defined surface surrounding the runway prepared or suitable for reducing the risk of damage to airplanes in the event of an undershoot, overshoot, or excursion from the runway (see [AC 150/5300-13](#)). Construction activities within the existing RSA are subject to the following conditions:

- 2.22.1.1 No construction may occur within the existing RSA while the runway is open for aircraft operations. The RSA dimensions may be temporarily adjusted if the runway is restricted to aircraft operations requiring an RSA that is equal to the RSA width and length beyond the runway ends available during construction. (See [AC 150/5300-13](#)). The temporary use of declared distances and/or partial runway closures may provide the necessary RSA under certain circumstances. Coordinate with the appropriate FAA Airports Regional or District Office to have declared distances information published, and appropriate NOTAMs issued. See [AC 150/5300-13](#) for guidance on the use of declared distances.
- 2.22.1.2 The airport operator must coordinate the adjustment of RSA dimensions as permitted above with the appropriate FAA Airports Regional or District Office and the local FAA air traffic manager and issue a NOTAM.
- 2.22.1.3 The CSPP and SPCD must provide procedures for ensuring adequate distance for protection from blasting operations, if required by operational considerations.

2.22.1.4 Excavations.

2.22.1.4.1 Open trenches or excavations are not permitted within the RSA while the runway is open. Backfill trenches before the runway is opened. If backfilling excavations before the runway must be opened is impracticable, cover the excavations appropriately. Covering for open trenches must be designed to allow the safe operation of the heaviest aircraft operating on the runway across the trench without damage to the aircraft.

2.22.1.4.2 Construction contractors must prominently mark open trenches and excavations at the construction site with red or orange flags, as approved by the airport operator, and light them with red lights during hours of restricted visibility or darkness.

2.22.1.5 Erosion Control.

Soil erosion must be controlled to maintain RSA standards, that is, the RSA must be cleared and graded and have no potentially hazardous ruts, humps, depressions, or other surface variations, and capable, under dry conditions, of supporting snow removal equipment, aircraft rescue and fire fighting equipment, and the occasional passage of aircraft without causing structural damage to the aircraft.

2.22.2 Runway Object Free Area (ROFA).

Construction, including excavations, may be permitted in the ROFA. However, equipment must be removed from the ROFA when not in use, and material should not be stockpiled in the ROFA if not necessary. Stockpiling material in the OFA requires submittal of a 7460-1 form and justification provided to the appropriate FAA Airports Regional or District Office for approval.

2.22.3 Taxiway Safety Area (TSA).

2.22.3.1 A taxiway safety area is a defined surface alongside the taxiway prepared or suitable for reducing the risk of damage to an airplane unintentionally departing the taxiway. (See AC 150/5300-13.) Since the width of the TSA is equal to the wingspan of the design aircraft, no construction may occur within the TSA while the taxiway is open for aircraft operations. The TSA dimensions may be temporarily adjusted if the taxiway is restricted to aircraft operations requiring a TSA that is equal to the TSA width available during construction. Give special consideration to TSA dimensions at taxiway turns and intersections. (see AC 150/5300-13).

2.22.3.2 The airport operator must coordinate the adjustment of the TSA width as permitted above with the appropriate FAA Airports Regional or District Office and the FAA air traffic manager and issue a NOTAM.

2.22.3.3 The CSPP and SPCD must provide procedures for ensuring adequate distance for protection from blasting operations.

2.22.3.4 **Excavations.**

1. Curves. Open trenches or excavations are not permitted within the TSA while the taxiway is open. Trenches should be backfilled before the taxiway is opened. If backfilling excavations before the taxiway must be opened is impracticable, cover the excavations appropriately. Covering for open trenches must be designed to allow the safe operation of the heaviest aircraft operating on the taxiway across the trench without damage to the aircraft.
2. Straight Sections. Open trenches or excavations are not permitted within the TSA while the taxiway is open for unrestricted aircraft operations. Trenches should be backfilled before the taxiway is opened. If backfilling excavations before the taxiway must be opened is impracticable, cover the excavations to allow the safe passage of ARFF equipment and of the heaviest aircraft operating on the taxiway across the trench without causing damage to the equipment or aircraft. In rare circumstances where the section of taxiway is indispensable for aircraft movement, open trenches or excavations may be permitted in the TSA while the taxiway is open to aircraft operations, subject to the following restrictions:
 - a. Taxiing speed is limited to 10 mph.
 - b. Appropriate NOTAMs are issued.
 - c. Marking and lighting meeting the provisions of paragraphs 2.18 and 2.20 are implemented.
 - d. Low mass, low-profile lighted barricades are installed.
 - e. Appropriate temporary orange construction signs are installed.
3. Construction contractors must prominently mark open trenches and excavations at the construction site with red or orange flags, as approved by the airport operator, and light them with red lights during hours of restricted visibility or darkness.

2.22.3.5 **Erosion control.**

Soil erosion must be controlled to maintain TSA standards, that is, the TSA must be cleared and graded and have no potentially hazardous ruts, humps, depressions, or other surface variations, and capable, under dry conditions, of supporting snow removal equipment, aircraft rescue and firefighting equipment, and the occasional passage of aircraft without causing structural damage to the aircraft.

2.22.4 Taxiway Object Free Area (TOFA).

Unlike the Runway Object Free Area, aircraft wings regularly penetrate the taxiway object free area during normal operations. Thus, the restrictions are more stringent. Except as provided below, no construction may occur within the taxiway object free area while the taxiway is open for aircraft operations.

- 2.22.4.1 The taxiway object free area dimensions may be temporarily adjusted if the taxiway is restricted to aircraft operations requiring a taxiway object free area that is equal to the taxiway object free area width available. Give special consideration to TOFA dimensions at taxiway turns and intersections.
- 2.22.4.2 Offset taxiway centerline and edge pavement markings (do not use glass beads) may be used as a temporary measure to provide the required taxiway object free area. Where offset taxiway pavement markings are provided, centerline lighting, centerline reflectors, or taxiway edge reflectors are required. Existing lighting that does not coincide with the temporary markings must be taken out of service.
- 2.22.4.3 Construction activity, including open excavations, may be accomplished without adjusting the width of the taxiway object free area, subject to the following restrictions:
 - 2.22.4.3.1 Taxiing speed is limited to 10 mph.
 - 2.22.4.3.2 NOTAMs issued advising taxiing pilots of hazard and recommending reduced taxiing speeds on the taxiway.
 - 2.22.4.3.3 Marking and lighting meeting the provisions of paragraphs 2.18 and 2.20 are implemented.
 - 2.22.4.3.4 If desired, appropriate orange construction signs are installed. See paragraph 2.18.4.2 and Appendix F.
 - 2.22.4.3.5 Five-foot clearance is maintained between equipment and materials and any part of an aircraft (includes wingtip overhang). If such clearance can only be maintained if an aircraft does not have full use of the entire taxiway width (with its main landing gear at the edge of the usable pavement), then it will be necessary to move personnel and equipment for the passage of that aircraft.
 - 2.22.4.3.6 Flaggers furnished by the contractor must be used to direct and control construction equipment and personnel to a pre-established setback distance for safe passage of aircraft, and airline and/or airport personnel. Flaggers must also be used to direct taxiing aircraft. Due to liability issues, the airport operator should require airlines to provide flaggers for directing taxiing aircraft.

2.22.5 Obstacle Free Zone (OFZ).

In general, personnel, material, and/or equipment may not penetrate the OFZ while the runway is open for aircraft operations. If a penetration to the OFZ is necessary, it may be possible to continue aircraft operations through operational restrictions. Coordinate with the FAA through the appropriate FAA Airports Regional or District Office.

2.22.6 Runway Approach/Departure Areas and Clearways.

All personnel, materials, and/or equipment must remain clear of the applicable threshold siting surfaces, as defined in AC 150/5300-13. Objects that do not penetrate these surfaces may still be obstructions to air navigation and may affect standard instrument approach procedures. Coordinate with the FAA through the appropriate FAA Airports Regional or District Office.

2.22.6.1 Construction activity in a runway approach/departure area may result in the need to partially close a runway or displace the existing runway threshold. Partial runway closure, displacement of the runway threshold, as well as closure of the complete runway and other portions of the movement area also require coordination through the airport operator with the appropriate FAA air traffic manager (FSS if non-towered) and ATO/Technical Operations (for affected NAVAIDS) and airport users.

2.22.6.2 **Caution About Partial Runway Closures.**

When filing a NOTAM for a partial runway closure, clearly state that the portion of pavement located prior to the threshold is not available for landing and departing traffic. In this case, the threshold has been moved for both landing and takeoff purposes (this is different than a displaced threshold). There may be situations where the portion of closed runway is available for taxiing only. If so, the NOTAM must reflect this condition).

2.22.6.3 **Caution About Displaced Thresholds.**

Implementation of a displaced threshold affects runway length available for aircraft landing over the displacement. Depending on the reason for the displacement (to provide obstruction clearance or RSA), such a displacement may also require an adjustment in the landing distance available and accelerate-stop distance available in the opposite direction. If project scope includes personnel, equipment, excavation, or other work within the existing RSA of any usable runway end, do not implement a displaced threshold unless arrivals and departures toward the construction activity are prohibited. Instead, implement a partial closure.

2.23 **Other Limitations on Construction.**

The CSPP must specify any other limitations on construction, including but not limited to:

2.23.1 Prohibitions.

- 2.23.1.1 No use of tall equipment (cranes, concrete pumps, and so on) unless a 7460-1 determination letter is issued for such equipment.
- 2.23.1.2 No use of open flame welding or torches unless fire safety precautions are provided and the airport operator has approved their use.
- 2.23.1.3 No use of electrical blasting caps on or within 1,000 feet (300 meters) of the airport property. See AC 150/5370-10.

2.23.2 Restrictions.

- 2.23.2.1 Construction suspension required during specific airport operations.
- 2.23.2.2 Areas that cannot be worked on simultaneously.
- 2.23.2.3 Day or night construction restrictions.
- 2.23.2.4 Seasonal construction restrictions.
- 2.23.2.5 Temporary signs not approved by the airport operator.
- 2.23.2.6 Grades changes that could result in unplanned effects on NAVAIDs.

CHAPTER 3. GUIDELINES FOR WRITING A CSPP

3.1 **General Requirements.**

The CSPP is a standalone document written to correspond with the subjects outlined in paragraph 2.4. The CSPP is organized by numbered sections corresponding to each subject listed in paragraph 2.4, and described in detail in paragraphs 2.5 - 2.23. Each section number and title in the CSPP matches the corresponding subject outlined in paragraph 2.4 (for example, 1. Coordination, 2. Phasing, 3. Areas and Operations Affected by the Construction Activity, and so on). With the exception of the project scope of work outlined in Section 2. Phasing, only subjects specific to operational safety during construction should be addressed.

3.2 **Applicability of Subjects.**

Each section should, to the extent practical, focus on the specific subject. Where an overlapping requirement spans several sections, the requirement should be explained in detail in the most applicable section. A reference to that section should be included in all other sections where the requirement may apply. For example, the requirement to protect existing underground FAA ILS cables during trenching operations could be considered FAA ATO coordination (Coordination, paragraph 2.5.3), an area and operation affected by the construction activity (Areas and Operations Affected by the Construction Activity, paragraph 2.7.1.4), a protection of a NAVAID (Protection of Navigational Aids (NAVAIDs), paragraph 2.8), or a notification to the FAA of construction activities (Notification of Construction Activities, paragraph 2.13.5.3.2). However, it is more specifically an underground utility requirement (Underground Utilities, paragraph 2.15). The procedure for protecting underground ILS cables during trenching operations should therefore be described in 2.4.2.11: “The contractor must coordinate with the local FAA System Support Center (SSC) to mark existing ILS cable routes along Runway 17-35. The ILS cables will be located by hand digging whenever the trenching operation moves within 10 feet of the cable markings.” All other applicable sections should include a reference to 2.4.2.11: “ILS cables shall be identified and protected as described in 2.4.2.11” or “See 2.4.2.11 for ILS cable identification and protection requirements.” Thus, the CSPP should be considered as a whole, with no need to duplicate responses to related issues.

3.3 **Graphical Representations.**

Construction safety drawings should be included in the CSPP as attachments. When other graphical representations will aid in supporting written statements, the drawings, diagrams, and/or photographs should also be attached to the CSPP. References should be made in the CSPP to each graphical attachment and may be made in multiple sections.

3.4 **Reference Documents.**

The CSPP must not incorporate a document by reference unless reproduction of the material in that document is prohibited. In that case, either copies of or a source for the referenced document must be provided to the contractor. Where this AC recommends references (e.g. as in paragraph 3.9) the intent is to include a reference to the corresponding section in the CSPP, not to this Advisory Circular.

3.5 **Restrictions.**

The CSPP should not be considered as a project design review document. The CSPP should also avoid mention of permanent (“as-built”) features such as pavements, markings, signs, and lighting, except when such features are intended to aid in maintaining operational safety during the construction.

3.6 **Coordination.**

Include in this section a detailed description of conferences and meetings to be held both before and during the project. Include appropriate information from AC 150/5370-12. Discuss coordination procedures and schedules for each required FAA ATO Technical Operations shutdown and restart and all required flight inspections.

3.7 **Phasing.**

Include in this section a detailed scope of work description for the project as a whole and each phase of work covered by the CSPP. This includes all locations and durations of the work proposed. Attach drawings to graphically support the written scope of work. Detail in this section the sequenced phases of the proposed construction. Include a reference to paragraph 3.8, as appropriate.

3.8 **Areas and Operations Affected by Construction.**

Focus in this section on identifying the areas and operations affected by the construction. Describe corresponding mitigation that is not covered in detail elsewhere in the CSPP. Include references to paragraphs below as appropriate. Attach drawings as necessary to graphically describe affected areas and mechanisms proposed. See Appendix F for sample operational effects tables and figures.

3.9 **NAVAID Protection.**

List in this section all NAVAID facilities that will be affected by the construction. Identify NAVAID facilities that will be placed out of service at any time prior to or during construction activities. Identify individuals responsible for coordinating each shutdown and when each facility will be out of service. Include a reference to paragraph 3.6 for FAA ATO NAVAID shutdown, restart, and flight inspection coordination. Outline in detail procedures to protect each NAVAID facility remaining in service from interference by construction activities. Include a reference to paragraph 3.14 for the

issuance of NOTAMs as required. Include a reference to paragraph 3.16 for the protection of underground cables and piping serving NAVAIDs. If temporary visual aids are proposed to replace or supplement existing facilities, include a reference to paragraph 3.19. Attach drawings to graphically indicate the affected NAVAIDs and the corresponding critical areas.

3.10 **Contractor Access.**

This will necessarily be the most extensive section of the CSPP. Provide sufficient detail so that a contractor not experienced in working on airports will understand the unique restrictions such work will require. Due to this extent, it should be broken down into subsections as described below:

3.10.1 Location of Stockpiled Construction Materials.

Describe in this section specific locations for stockpiling material. Note any height restrictions on stockpiles. Include a reference to paragraph 3.21 for hazard marking and lighting devices used to identify stockpiles. Include a reference to paragraph 3.11 for provisions to prevent stockpile material from becoming wildlife attractants. Include a reference to paragraph 3.12 for provisions to prevent stockpile material from becoming FOD. Attach drawings to graphically indicate the stockpile locations.

3.10.2 Vehicle and Pedestrian Operations.

While there are many items to be addressed in this major subsection of the CSPP, all are concerned with one main issue: keeping people and vehicles from areas of the airport where they don't belong. This includes preventing unauthorized entry to the AOA and preventing the improper movement of pedestrians or vehicles on the airport. In this section, focus on mechanisms to prevent construction vehicles and workers traveling to and from the worksite from unauthorized entry into movement areas. Specify locations of parking for both employee vehicles and construction equipment, and routes for access and haul roads. In most cases, this will best be accomplished by attaching a drawing. Quote from AC 150/5210-5 specific requirements for contractor vehicles rather than referring to the AC as a whole, and include special requirements for identifying HAZMAT vehicles. Quote from, rather than incorporate by reference, AC 150/5210-20 as appropriate to address the airport's rules for ground vehicle operations, including its training program. Discuss the airport's recordkeeping system listing authorized vehicle operators.

3.10.3 Two-Way Radio Communications.

Include a special section to identify all individuals who are required to maintain communications with Air Traffic (AT) at airports with active towers, or monitor CTAF at airports without or with closed ATCT. Include training requirements for all individuals required to communicate with AT. Individuals required to monitor AT frequencies should also be identified. If construction employees are also required to communicate by radio with Airport Operations, this procedure should be described in detail. Usage of vehicle mounted radios and/or portable radios should be addressed. Communication procedures for the event of disabled radio communication (that is, light

signals, telephone numbers, others) must be included. All radio frequencies should be identified (Tower, Ground Control, CTAF, UNICOM, ATIS, and so on).

3.10.4 Airport Security.

Address security as it applies to vehicle and pedestrian operations. Discuss TSA requirements, security badging requirements, perimeter fence integrity, gate security, and other needs. Attach drawings to graphically indicate secured and/or Security Identification Display Areas (SIDA), perimeter fencing, and available access points.

3.11 **Wildlife Management.**

Discuss in this section wildlife management procedures. Describe the maintenance of existing wildlife mitigation devices, such as perimeter fences, and procedures to limit wildlife attractants. Include procedures to notify Airport Operations of wildlife encounters. Include a reference to paragraph 3.10 for security (wildlife) fence integrity maintenance as required.

3.12 **FOD Management.**

In this section, discuss methods to control and monitor FOD: worksite housekeeping, ground vehicle tire inspections, runway sweeps, and so on. Include a reference to paragraph 3.15 for inspection requirements as required.

3.13 **HAZMAT Management.**

Describe in this section HAZMAT management procedures: fuel deliveries, spill recovery procedures, Safety Data Sheet (SDS), Material Safety Data Sheet (MSDS) or Product Safety Data Sheet (PSDS) availability, and other considerations. Any specific airport HAZMAT restrictions should also be identified. Include a reference to paragraph 3.10 for HAZMAT vehicle identification requirements. Quote from, rather than incorporate by reference, AC 150/5320-15.

3.14 **Notification of Construction Activities.**

List in this section the names and telephone numbers of points of contact for all parties affected by the construction project. We recommend a single list that includes all telephone numbers required under this section. Include emergency notification procedures for all representatives of all parties potentially impacted by the construction. Identify individual representatives – and at least one alternate – for each party. List both on-duty and off-duty contact information for each individual, including individuals responsible for emergency maintenance of airport construction hazard lighting and barricades. Describe procedures to coordinate immediate response to events that might adversely affect the operational safety of the airport (such as interrupted NAVAID service). Explain requirements for and the procedures for the issuance of Notices to Airmen (NOTAMs), notification to FAA required by 14 CFR Part 77 and Part 157 and in the event of affected NAVAIDs. For NOTAMs, identify an individual, and at least one alternate, responsible for issuing and cancelling each specific type of Notice to

Airmen (NOTAM) required. Detail notification methods for police, fire fighting, and medical emergencies. This may include 911, but should also include direct phone numbers of local police departments and nearby hospitals. Identify the E911 address of the airport and the emergency access route via haul roads to the construction site. Require the contractor to have this information available to all workers. The local Poison Control number should be listed. Procedures regarding notification of Airport Operations and/or the ARFF Department of such emergencies should be identified, as applicable. If airport radio communications are identified as a means of emergency notification, include a reference to paragraph 3.10. Differentiate between emergency and nonemergency notification of ARFF personnel, the latter including activities that affect ARFF water supplies and access roads. Identify the primary ARFF contact person and at least one alternate. If notification is to be made through Airport Operations, then detail this procedure. Include a method of confirmation from the ARFF department.

3.15 Inspection Requirements.

Describe in this section inspection requirements to ensure airfield safety compliance. Include a requirement for routine inspections by the resident engineer (RE) or other airport operator's representative and the construction contractors. If the engineering consultants and/or contractors have a Safety Officer who will conduct such inspections, identify this individual. Describe procedures for special inspections, such as those required to reopen areas for aircraft operations. Part 139 requires daily airfield inspections at certificated airports, but these may need to be more frequent when construction is in progress. Discuss the role of such inspections on areas under construction. Include a requirement to immediately remedy any deficiencies, whether caused by negligence, oversight, or project scope change.

3.16 Underground Utilities.

Explain how existing underground utilities will be located and protected. Identify each utility owner and include contact information for each company/agency in the master list. Address emergency response procedures for damaged or disrupted utilities. Include a reference to paragraph 3.14 for notification of utility owners of accidental utility disruption as required.

3.17 Penalties.

Describe in this section specific penalties imposed for noncompliance with airport rules and regulations, including the CSPP: SIDA violations, VPD, and others.

3.18 Special Conditions.

Identify any special conditions that may trigger specific safety mitigation actions outlined in this CSPP: low visibility operations, snow removal, aircraft in distress, aircraft accident, security breach, VPD, and other activities requiring construction suspension/resumption. Include a reference to paragraph 3.10 for compliance with airport safety and security measures and for radio communications as required. Include

a reference to paragraph 3.14 for emergency notification of all involved parties, including police/security, ARFF, and medical services.

3.19 Runway and Taxiway Visual Aids.

Include marking, lighting, signs, and visual NAVAIDS. Detail temporary runway and taxiway marking, lighting, signs, and visual NAVAIDS required for the construction. Discuss existing marking, lighting, signs, and visual NAVAIDS that are temporarily, altered, obliterated, or shut down. Consider non-federal facilities and address requirements for reimbursable agreements necessary for alteration of FAA facilities and for necessary flight checks. Identify temporary TORA signs or runway distance remaining signs if appropriate. Identify required temporary visual NAVAIDS such as REIL or PAPI. Quote from, rather than incorporate by reference, AC 150/5340-1, Standards for Airport Markings; AC 150/5340-18, Standards for Airport Sign Systems; and AC 150/5340-30, as required. Attach drawings to graphically indicate proposed marking, lighting, signs, and visual NAVAIDS.

3.20 Marking and Signs for Access Routes.

Detail plans for marking and signs for vehicle access routes. To the extent possible, signs should be in conformance with the Federal Highway Administration MUTCD and/or State highway specifications, not hand lettered. Detail any modifications to the guidance in the MUTCD necessary to meet frangibility/height requirements.

3.21 Hazard Marking and Lighting.

Specify all marking and lighting equipment, including when and where each type of device is to be used. Specify maximum gaps between barricades and the maximum spacing of hazard lighting. Identify one individual and at least one alternate responsible for maintenance of hazard marking and lighting equipment in the master telephone list. Include a reference to paragraph 3.14. Attach drawings to graphically indicate the placement of hazard marking and lighting equipment.

3.22 Work Zone Lighting for Nighttime Construction.

If work is to be conducted at night, specify all lighting equipment, including when and where each type of device is to be used. Indicate the direction lights are to be aimed and any directions that aiming of lights is prohibited. Specify any shielding necessary in instances where aiming is not sufficient to prevent interference with air traffic control and aircraft operations. Attach drawings to graphically indicate the placement and aiming of lighting equipment. Where the plan only indicates directions that aiming of lights is prohibited, the placement and positioning of portable lights must be proposed by the Contractor and approved by the airport operator's representative each time lights are relocated or repositioned.

3.23 Protection of Runway and Taxiway Safety Areas.

This section should focus exclusively on procedures for protecting all safety areas, including those altered by the construction: methods of demarcation, limit of access, movement within safety areas, stockpiling and trenching restrictions, and so on. Reference AC 150/5300-13, as required. Include a reference to paragraph 3.10 for procedures regarding vehicle and personnel movement within safety areas. Include a reference to paragraph 3.10 for material stockpile restrictions as required. Detail requirements for trenching, excavations, and backfill. Include a reference to paragraph 3.21 for hazard marking and lighting devices used to identify open excavations as required. If runway and taxiway closures are proposed to protect safety areas, or if temporary displaced thresholds and/or revised declared distances are used to provide the required Runway Safety Area, include a reference to paragraphs 3.14 and 3.19. Detail procedures for protecting the runway OFZ, runway OFA, taxiway OFA and runway approach surfaces including those altered by the construction: methods of demarcation, limit of cranes, storage of equipment, and so on. Quote from, rather than incorporate by reference, AC 150/5300-13, as required. Include a reference to paragraph 3.24 for height (i.e., crane) restrictions as required. One way to address the height of equipment that will move during the project is to establish a three-dimensional “box” within which equipment will be confined that can be studied as a single object. Attach drawings to graphically indicate the safety area, OFZ, and OFA boundaries.

3.24 Other Limitations on Construction.

This section should describe what limitations must be applied to each area of work and when each limitation will be applied: limitations due to airport operations, height (i.e., crane) restrictions, areas which cannot be worked at simultaneously, day/night work restrictions, winter construction, and other limitations. Include a reference to paragraph 3.7 for project phasing requirements based on construction limitations as required.

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APPENDIX A. RELATED READING MATERIAL

Obtain the latest version of the following free publications from the FAA on its Web site at <http://www.faa.gov/airports/>.

Table A-1. FAA Publications

Number	Title and Description
<u>AC 150/5200-28</u>	<i>Notices to Airmen (NOTAMs) for Airport Operators</i> Guidance for using the NOTAM System in airport reporting.
<u>AC 150/5200-30</u>	<i>Airport Field Condition Assessments and Winter Operations Safety</i> Guidance for airport owners/operators on the development of an acceptable airport snow and ice control program and on appropriate field condition reporting procedures.
<u>AC 150/5200-33</u>	<i>Hazardous Wildlife Attractants On or Near Airports</i> Guidance on locating certain land uses that might attract hazardous wildlife to public-use airports.
<u>AC 150/5210-5</u>	<i>Painting, Marking, and Lighting of Vehicles Used on an Airport</i> Guidance, specifications, and standards for painting, marking, and lighting vehicles operating in the airport air operations areas.
<u>AC 150/5210-20</u>	<i>Ground Vehicle Operations to include Taxiing or Towing an Aircraft on Airports</i> Guidance to airport operators on developing ground vehicle operation training programs.
<u>AC 150/5300-13</u>	<i>Airport Design</i> FAA standards and recommendations for airport design. Establishes approach visibility minimums as an airport design parameter, and contains the Object Free area and the obstacle free-zone criteria.
<u>AC 150/5210-24</u>	<i>Airport Foreign Object Debris (FOD) Management</i> Guidance for developing and managing an airport foreign object debris (FOD) program

Number	Title and Description
<u>AC 150/5320-15</u>	<i>Management of Airport Industrial Waste</i> Basic information on the characteristics, management, and regulations of industrial wastes generated at airports. Guidance for developing a Storm Water Pollution Prevention Plan (SWPPP) that applies best management practices to eliminate, prevent, or reduce pollutants in storm water runoff with particular airport industrial activities.
<u>AC 150/5340-1</u>	<i>Standards for Airport Markings</i> FAA standards for the siting and installation of signs on airport runways and taxiways.
<u>AC 150/5340-18</u>	<i>Standards for Airport Sign Systems</i> FAA standards for the siting and installation of signs on airport runways and taxiways.
<u>AC 150/5345-28</u>	<i>Precision Approach Path Indicator (PAPI) Systems</i> FAA standards for PAPI systems, which provide pilots with visual glide slope guidance during approach for landing.
<u>AC 150/5340-30</u>	<i>Design and Installation Details for Airport Visual Aids</i> Guidance and recommendations on the installation of airport visual aids.
<u>AC 150/5345-39</u>	<i>Specification for L-853, Runway and Taxiway Retroreflective Markers</i>
<u>AC 150/5345-44</u>	<i>Specification for Runway and Taxiway Signs</i> FAA specifications for unlighted and lighted signs for taxiways and runways.
<u>AC 150/5345-53</u>	<i>Airport Lighting Equipment Certification Program</i> Details on the Airport Lighting Equipment Certification Program (ALECP).
<u>AC 150/5345-50</u>	<i>Specification for Portable Runway and Taxiway Lights</i> FAA standards for portable runway and taxiway lights and runway end identifier lights for temporary use to permit continued aircraft operations while all or part of a runway lighting system is inoperative.
<u>AC 150/5345-55</u>	<i>Specification for L-893, Lighted Visual Aid to Indicate Temporary Runway Closure</i>

Number	Title and Description
<u>AC 150/5370-10</u>	<i>Standards for Specifying Construction of Airports</i> Standards for construction of airports, including earthwork, drainage, paving, turfing, lighting, and incidental construction.
<u>AC 150/5370-12</u>	<i>Quality Management for Federally Funded Airport Construction Projects</i>
EB 93	<i>Guidance for the Assembly and Installation of Temporary Orange Construction Signs</i>
FAA Order 5200.11	<u>FAA Airports (ARP) Safety Management System (SMS)</u> Basics for implementing SMS within ARP. Includes roles and responsibilities of ARP management and staff as well as other FAA lines of business that contribute to the ARP SMS.
FAA Certalert 98-05	<i>Grasses Attractive to Hazardous Wildlife</i> Guidance on grass management and seed selection.
FAA Form 7460-1	<u>Notice of Proposed Construction or Alteration</u>
FAA Form 7480-1	<u>Notice of Landing Area Proposal</u>
FAA Form 6000.26	National NAS Strategic Interruption Service Level Agreement, Strategic Events Coordination, Airport Sponsor Form

Obtain the latest version of the following free publications from the Electronic Code of Federal Regulations at <http://www.ecfr.gov/>.

Table A-2. Code of Federal Regulation

Number	Title
Title 14 CFR Part 77	Safe, Efficient Use and Preservation of the Navigable Airspace
Title 14 CFR Part 139	Certification of Airports
Title 49 CFR Part 1542	Airport Security

Obtain the latest version of the Manual on Uniform Traffic Control Devices from the Federal Highway Administration at <http://mutcd.fhwa.dot.gov/>.

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APPENDIX B. TERMS AND ACRONYMS**Table B-1. Terms and Acronyms**

Term	Definition
Form 7460-1	Notice of Proposed Construction or Alteration. For on-airport projects, the form submitted to the FAA regional or airports division office as formal written notification of any kind of construction or alteration of objects that affect navigable airspace, as defined in 14 CFR Part 77, <i>Safe, Efficient Use, and Preservation of the Navigable Airspace</i> . (See guidance available on the FAA web site at https://oeaaa.faa.gov .) The form may be downloaded at http://www.faa.gov/airports/resources/forms/ , or filed electronically at: https://oeaaa.faa.gov .
Form 7480-1	Notice of Landing Area Proposal. Form submitted to the FAA Airports Regional Division Office or Airports District Office as formal written notification whenever a project without an airport layout plan on file with the FAA involves the construction of a new airport; the construction, realigning, altering, activating, or abandoning of a runway, landing strip, or associated taxiway; or the deactivation or abandoning of an entire airport The form may be downloaded at http://www.faa.gov/airports/resources/forms/ .
Form 6000-26	Airport Sponsor Strategic Event Submission Form
AC	Advisory Circular
ACSI	Airport Certification Safety Inspector
ADG	Airplane Design Group
AIP	Airport Improvement Program
ALECP	Airport Lighting Equipment Certification Program
ANG	Air National Guard
AOA	Air Operations Area, as defined in 14 CFR Part 107. Means a portion of an airport, specified in the airport security program, in which security measures are carried out. This area includes aircraft movement areas, aircraft parking areas, loading ramps, and safety areas, and any adjacent areas (such as general aviation areas) that are not separated by adequate security systems, measures, or procedures. This area does not include the secured area of the airport terminal building.
ARFF	Aircraft Rescue and Fire Fighting
ARP	FAA Office of Airports
ASDA	Accelerate-Stop Distance Available
AT	Air Traffic
ATCT	Airport Traffic Control Tower
ATIS	Automatic Terminal Information Service
ATO	Air Traffic Organization
Certificated Airport	An airport that has been issued an Airport Operating Certificate by the FAA under

Term	Definition
	the authority of 14 CFR Part 139, <i>Certification of Airports</i> .
CFR	Code of Federal Regulations
Construction	The presence of construction-related personnel, equipment, and materials in any location that could infringe upon the movement of aircraft.
CSPP	Construction Safety and Phasing Plan. The overall plan for safety and phasing of a construction project developed by the airport operator, or developed by the airport operator's consultant and approved by the airport operator. It is included in the invitation for bids and becomes part of the project specifications.
CTAF	Common Traffic Advisory Frequency
Displaced Threshold	A threshold that is located at a point on the runway other than the designated beginning of the runway. The portion of pavement behind a displaced threshold is available for takeoffs in either direction or landing from the opposite direction.
DOT	Department of Transportation
EPA	Environmental Protection Agency
FAA	Federal Aviation Administration
FOD	Foreign Object Debris/Damage
FSS	Flight Service Station
GA	General Aviation
HAZMAT	Hazardous Materials
HMA	Hot Mix Asphalt
IAP	Instrument Approach Procedures
IFR	Instrument Flight Rules
ILS	Instrument Landing System
LDA	Landing Distance Available
LOC	Localizer antenna array
Movement Area	The runways, taxiways, and other areas of an airport that are used for taxiing or hover taxiing, air taxiing, takeoff, and landing of aircraft, exclusive of loading aprons and aircraft parking areas (reference 14 CFR Part 139).
MSDS	Material Safety Data Sheet
MUTCD	Manual on Uniform Traffic Control Devices
NAVAID	Navigation Aid
NAVAID Critical Area	An area of defined shape and size associated with a NAVAID that must remain clear and graded to avoid interference with the electronic signal.
Non-Movement Area	The area inside the airport security fence exclusive of the Movement Area. It is important to note that the non-movement area includes pavement traversed by aircraft.

Term	Definition
NOTAM	Notices to Airmen
Obstruction	Any object/obstacle exceeding the obstruction standards specified by 14 CFR Part 77, subpart C.
OCC	Operations Control Center
OE / AAA	Obstruction Evaluation / Airport Airspace Analysis
OFA	Object Free Area. An area on the ground centered on the runway, taxiway, or taxi lane centerline provided to enhance safety of aircraft operations by having the area free of objects except for those objects that need to be located in the OFA for air navigation or aircraft ground maneuvering purposes. (See AC 150/5300-13 for additional guidance on OFA standards and wingtip clearance criteria.)
OFZ	Obstacle Free Zone. The airspace below 150 ft (45 m) above the established airport elevation and along the runway and extended runway centerline that is required to be clear of all objects, except for frangible visual NAVAIDs that need to be located in the OFZ because of their function, in order to provide clearance protection for aircraft landing or taking off from the runway and for missed approaches. The OFZ is subdivided as follows: Runway OFZ, Inner Approach OFZ, Inner Transitional OFZ, and Precision OFZ. Refer to AC 150/5300-13 for guidance on OFZ.
OSHA	Occupational Safety and Health Administration
OTS	Out of Service
P&R	Planning and Requirements Group
NPI	NAS Planning & Integration
PAPI	Precision Approach Path Indicator
PFC	Passenger Facility Charge
PLASI	Pulse Light Approach Slope Indicator
Project Proposal Summary	A clear and concise description of the proposed project or change that is the object of Safety Risk Management.
RA	Reimbursable Agreement
RE	Resident Engineer
REIL	Runway End Identifier Lights
RNAV	Area Navigation
ROFA	Runway Object Free Area
RSA	Runway Safety Area. A defined surface surrounding the runway prepared or suitable for reducing the risk of damage to airplanes in the event of an undershoot, overshoot, or excursion from the runway, in accordance with AC 150/5300-13 .
SDS	Safety Data Sheet
SIDA	Security Identification Display Area
SMS	Safety Management System

Term	Definition
SPCD	Safety Plan Compliance Document. Details developed and submitted by a contractor to the airport operator for approval providing details on how the performance of a construction project will comply with the CSPP.
SRM	Safety Risk Management
SSC	System Support Center
Taxiway Safety Area	A defined surface alongside the taxiway prepared or suitable for reducing the risk of damage to an airplane unintentionally departing the taxiway, in accordance with AC 150/5300-13 .
TDG	Taxiway Design Group
Temporary	Any condition that is not intended to be permanent.
Temporary Runway End	The beginning of that portion of the runway available for landing and taking off in one direction, and for landing in the other direction. Note the difference from a displaced threshold.
Threshold	The beginning of that portion of the runway available for landing. In some instances, the landing threshold may be displaced.
TODA	Takeoff Distance Available
TOFA	Taxiway Object Free Area
TORA	Takeoff Run Available. The length of the runway less any length of runway unavailable and/or unsuitable for takeoff run computations. See AC 150/5300-13 for guidance on declared distances.
TSA	Taxiway Safety Area, or Transportation Security Administration
UNICOM	A radio communications system of a type used at small airports.
VASI	Visual Approach Slope Indicator
VGSI	Visual Glide Slope Indicator. A device that provides a visual glide slope indicator to landing pilots. These systems include precision approach path indicator (PAPI), visual approach slope indicator (VASI), and pulse light approach slope indicator (PLASI).
VFR	Visual Flight Rules
VOR	Very High Frequency Omnidirectional Radio Range
VPD	Vehicle / Pedestrian Deviation

APPENDIX C. SAFETY AND PHASING PLAN CHECKLIST

This appendix is keyed to Chapter 2. In the electronic version of this AC, clicking on the paragraph designation in the Reference column will access the applicable paragraph. There may be instances where the CSPP requires provisions that are not covered by the list in this appendix.

This checklist is intended as an aid, not a required submittal.

Table C-1. CSPP Checklist

Coordination	Reference	Addressed?			Remarks
		Yes	No	NA	
General Considerations					
Requirements for predesign, prebid, and preconstruction conferences to introduce the subject of airport operational safety during construction are specified.	<u>2.5</u>				
Operational safety is a standing agenda item for construction progress meetings.	<u>2.5</u>				
Scheduling of the construction phases is properly addressed.	<u>2.6</u>				
Any formal agreements are established.	<u>2.5.3</u>				
Areas and Operations Affected by Construction Activity					
Drawings showing affected areas are included.	<u>2.7.1</u>				
Closed or partially closed runways, taxiways, and aprons are depicted on drawings.	<u>2.7.1.1</u>				
Access routes used by ARFF vehicles affected by the project are addressed.	<u>2.7.1.2</u>				
Access routes used by airport and airline support vehicles affected by the project are addressed.	<u>2.7.1.3</u>				
Underground utilities, including water supplies for firefighting and drainage.	<u>2.7.1.4</u>				

Coordination	Reference	Addressed?			Remarks
		Yes	No	NA	
Approach/departure surfaces affected by heights of temporary objects are addressed.	<u>2.7.1.5</u>				
Construction areas, storage areas, and access routes near runways, taxiways, aprons, or helipads are properly depicted on drawings.	<u>2.7.1</u>				
Temporary changes to taxi operations are addressed.	<u>2.7.2.1</u>				
Detours for ARFF and other airport vehicles are identified.	<u>2.7.2.2</u>				
Maintenance of essential utilities and underground infrastructure is addressed.	<u>2.7.2.3</u>				
Temporary changes to air traffic control procedures are addressed.	<u>2.7.2.4</u>				
NAVAIDs					
Critical areas for NAVAIDs are depicted on drawings.	<u>2.8</u>				
Effects of construction activity on the performance of NAVAIDs, including unanticipated power outages, are addressed.	<u>2.8</u>				
Protection of NAVAID facilities is addressed.	<u>2.8</u>				
The required distance and direction from each NAVAID to any construction activity is depicted on drawings.	<u>2.8</u>				
Procedures for coordination with FAA ATO/Technical Operations, including identification of points of contact, are included.	<u>2.8, 2.13.1, 2.13.5.3.1, 2.18.1</u>				
Contractor Access					
The CSPP addresses areas to which contractor will have access and how	<u>2.9</u>				

Coordination	Reference	Addressed?			Remarks
		Yes	No	NA	
the areas will be accessed.					
The application of 49 CFR Part 1542 Airport Security, where appropriate, is addressed.	<u>2.9</u>				
The location of stockpiled construction materials is depicted on drawings.	<u>2.9.1</u>				
The requirement for stockpiles in the ROFA to be approved by FAA is included.	<u>2.9.1</u>				
Requirements for proper stockpiling of materials are included.	<u>2.9.1</u>				
Construction site parking is addressed.	<u>2.9.2.1</u>				
Construction equipment parking is addressed.	<u>2.9.2.2</u>				
Access and haul roads are addressed.	<u>2.9.2.3</u>				
A requirement for marking and lighting of vehicles to comply with <i>AC 150/5210-5, Painting, Marking and Lighting of Vehicles Used on an Airport</i> , is included.	<u>2.9.2.4</u>				
Proper vehicle operations, including requirements for escorts, are described.	<u>2.9.2.5, 2.9.2.6</u>				
Training requirements for vehicle drivers are addressed.	<u>2.9.2.7</u>				
Two-way radio communications procedures are described.	<u>2.9.2.9</u>				
Maintenance of the secured area of the airport is addressed.	<u>2.9.2.10</u>				
Wildlife Management					
The airport operator's wildlife management procedures are addressed.	<u>2.10</u>				

Coordination	Reference	Addressed?			Remarks
		Yes	No	NA	
Foreign Object Debris Management					
The airport operator's FOD management procedures are addressed.	<u>2.11</u>				
Hazardous Materials Management					
The airport operator's hazardous materials management procedures are addressed.	<u>2.12</u>				
Notification of Construction Activities					
Procedures for the immediate notification of airport user and local FAA of any conditions adversely affecting the operational safety of the airport are detailed.	<u>2.13</u>				
Maintenance of a list by the airport operator of the responsible representatives/points of contact for all involved parties and procedures for contacting them 24 hours a day, seven days a week is specified.	<u>2.13.1</u>				
A list of local ATO/Technical Operations personnel is included.	<u>2.13.1</u>				
A list of ATCT managers on duty is included.	<u>2.13.1</u>				
A list of authorized representatives to the OCC is included.	<u>2.13.2</u>				
Procedures for coordinating, issuing, maintaining and cancelling by the airport operator of NOTAMS about airport conditions resulting from construction are included.	<u>2.8, 2.13.2, 2.18.3.3.9</u>				
Provision of information on closed or hazardous conditions on airport movement areas by the airport operator to the OCC is specified.	<u>2.13.2</u>				
Emergency notification procedures for medical, fire fighting, and police	<u>2.13.3</u>				

Coordination	Reference	Addressed?			Remarks
		Yes	No	NA	
response are addressed.					
Coordination with ARFF personnel for non-emergency issues is addressed.	<u>2.13.4</u>				
Notification to the FAA under 14 CFR parts 77 and 157 is addressed.	<u>2.13.5</u>				
Reimbursable agreements for flight checks and/or design and construction for FAA owned NAVAIDs are addressed.	<u>2.13.5.3.2</u>				
Inspection Requirements					
Daily and interim inspections by both the airport operator and contractor are specified.	<u>2.14.1, 2.14.2</u>				
Final inspections at certificated airports are specified when required.	<u>2.14.3</u>				
Underground Utilities					
Procedures for protecting existing underground facilities in excavation areas are described.	<u>2.15</u>				
Penalties					
Penalty provisions for noncompliance with airport rules and regulations and the safety plans are detailed.	<u>2.16</u>				
Special Conditions					
Any special conditions that affect the operation of the airport or require the activation of any special procedures are addressed.	<u>2.17</u>				
Runway and Taxiway Visual Aids - Marking, Lighting, Signs, and Visual NAVAIDs					
The proper securing of temporary airport markings, lighting, signs, and visual NAVAIDs is addressed.	<u>2.18.1</u>				
Frangibility of airport markings, lighting, signs, and visual NAVAIDs is specified.	<u>2.18.1, 2.18.3, 2.18.4.2, 2.20.2.4</u>				

Coordination	Reference	Addressed?			Remarks
		Yes	No	NA	
The requirement for markings to be in compliance with <u>AC 150/5340-1</u> , <i>Standards for Airport Markings</i> , is specified.	<u>2.18.2</u>				
Detailed specifications for materials and methods for temporary markings are provided.	<u>2.18.2</u>				
The requirement for lighting to conform to <u>AC 150/5340-30</u> , <i>Design and Installation Details for Airport Visual Aids</i> ; <u>AC 150/5345-50</u> , <i>Specification for Portable Runway and Taxiway Lights</i> ; and <u>AC 150/5345-53</u> , <i>Airport Lighting Certification Program</i> , is specified.	<u>2.18.3</u>				
The use of a lighted X is specified where appropriate.	<u>2.18.2.1.2</u> , <u>2.18.3.2</u>				
The requirement for signs to conform to <u>AC 150/5345-44</u> , <i>Specification for Runway and Taxiway Signs</i> ; <u>AC 150/5340-18</u> , <i>Standards for Airport Sign Systems</i> ; and <u>AC 150/5345-53</u> , <i>Airport Lighting Certification Program</i> , is specified.	<u>2.18.4</u>				
Marking and Signs For Access Routes					
The CSPP specifies that pavement markings and signs intended for construction personnel should conform to <u>AC 150/5340-18</u> and, to the extent practicable, with the MUTCD and/or State highway specifications.	<u>2.18.4.2</u>				
Hazard Marking and Lighting					
Prominent, comprehensible warning indicators for any area affected by construction that is normally accessible to aircraft, personnel, or vehicles are specified.	<u>2.20.1</u>				

Coordination	Reference	Addressed?			Remarks
		Yes	No	NA	
Hazard marking and lighting are specified to identify open manholes, small areas under repair, stockpiled material, and waste areas.	<u>2.20.1</u>				
The CSPP considers less obvious construction-related hazards.	<u>2.20.1</u>				
Equipment that poses the least danger to aircraft but is sturdy enough to remain in place when subjected to typical winds, prop wash and jet blast is specified.	<u>2.20.2.1</u>				
The spacing of barricades is specified such that a breach is physically prevented barring a deliberate act.	<u>2.20.2.1</u>				
Red lights meeting the luminance requirements of the State Highway Department are specified.	<u>2.20.2.2</u>				
Barricades, temporary markers, and other objects placed and left in areas adjacent to any open runway, taxiway, taxi lane, or apron are specified to be as low as possible to the ground, and no more than 18 inch high.	<u>2.20.2.3</u>				
Barricades are specified to indicate construction locations in which no part of an aircraft may enter.	<u>2.20.2.3</u>				
Highly reflective barriers with lights are specified to barricade taxiways leading to closed runways.	<u>2.20.2.5</u>				
Markings for temporary closures are specified.	<u>2.20.2.5</u>				
The provision of a contractor's representative on call 24 hours a day for emergency maintenance of airport hazard lighting and barricades is specified.	<u>2.20.2.7</u>				

Coordination	Reference	Addressed?			Remarks
		Yes	No	NA	
Work Zone Lighting for Nighttime Construction					
If work is to be conducted at night, the CSPP identifies construction lighting units and their general locations and aiming in relationship to the ATCT and active runways and taxiways.	<u>2.21</u>				
Protection of Runway and Taxiway Safety Areas					
The CSPP clearly states that no construction may occur within a safety area while the associated runway or taxiway is open for aircraft operations.	<u>2.22.1.1,</u> <u>2.22.3.1</u>				
The CSPP specifies that the airport operator coordinates the adjustment of RSA or TSA dimensions with the ATCT and the appropriate FAA Airports Regional or District Office and issues a local NOTAM.	<u>2.22.1.2,</u> <u>2.22.3.2</u>				
Procedures for ensuring adequate distance for protection from blasting operations, if required by operational considerations, are detailed.	<u>2.22.3.3</u>				
The CSPP specifies that open trenches or excavations are not permitted within a safety area while the associated runway or taxiway is open, subject to approved exceptions.	<u>2.22.1.4</u>				
Appropriate covering of excavations in the RSA or TSA that cannot be backfilled before the associated runway or taxiway is open is detailed.	<u>2.22.1.4</u>				
The CSPP includes provisions for prominent marking of open trenches and excavations at the construction site.	<u>2.22.1.4</u>				
Grading and soil erosion control to maintain RSA/TSA standards are	<u>2.22.3.5</u>				

Coordination	Reference	Addressed?			Remarks
		Yes	No	NA	
addressed.					
The CSPP specifies that equipment is to be removed from the ROFA when not in use.	<u>2.22.2</u>				
The CSPP clearly states that no construction may occur within a taxiway safety area while the taxiway is open for aircraft operations.	<u>2.22.3</u>				
Appropriate details are specified for any construction work to be accomplished in a taxiway object free area.	<u>2.22.4</u>				
Measures to ensure that personnel, material, and/or equipment do not penetrate the OFZ or threshold siting surfaces while the runway is open for aircraft operations are included.	<u>2.22.4.3.6</u>				
Provisions for protection of runway approach/departure areas and clearways are included.	<u>2.22.6</u>				
Other Limitations on Construction					
The CSPP prohibits the use of open flame welding or torches unless adequate fire safety precautions are provided and the airport operator has approved their use.	<u>2.23.1.2</u>				
The CSPP prohibits the use of electrical blasting caps on or within 1,000 ft (300 m) of the airport property.	<u>2.23.1.3</u>				

APPENDIX D. CONSTRUCTION PROJECT DAILY SAFETY INSPECTION CHECKLIST

The situations identified below are potentially hazardous conditions that may occur during airport construction projects. Safety area encroachments, unauthorized and improper ground vehicle operations, and unmarked or uncovered holes and trenches near aircraft operating surfaces pose the most prevalent threats to airport operational safety during airport construction projects. The list below is one tool that the airport operator or contractor may use to aid in identifying and correcting potentially hazardous conditions. It should be customized as appropriate for each project including information such as the date, time and name of the person conducting the inspection.

Table D-1. Potentially Hazardous Conditions

Item	Action Required (Describe)	No Action Required (Check)
Excavation adjacent to runways, taxiways, and aprons improperly backfilled.		
Mounds of earth, construction materials, temporary structures, and other obstacles near any open runway, taxiway, or taxi lane; in the related Object Free area and aircraft approach or departure areas/zones; or obstructing any sign or marking.		
Runway resurfacing projects resulting in lips exceeding 3 inch (7.6 cm) from pavement edges and ends.		
Heavy equipment (stationary or mobile) operating or idle near AOA, in runway approaches and departures areas, or in OFZ.		
Equipment or material near NAVAIDs that may degrade or impair radiated signals and/or the monitoring of navigation and visual aids. Unauthorized or improper vehicle operations in localizer or glide slope critical areas, resulting in electronic interference and/or facility shutdown.		
Tall and especially relatively low visibility units (that is, equipment with slim profiles) — cranes, drills, and similar objects — located in critical areas, such as OFZ and		

Item	Action Required (Describe)	No Action Required (Check)
approach zones.		
Improperly positioned or malfunctioning lights or unlighted airport hazards, such as holes or excavations, on any apron, open taxiway, or open taxi lane or in a related safety, approach, or departure area.		
Obstacles, loose pavement, trash, and other debris on or near AOA. Construction debris (gravel, sand, mud, paving materials) on airport pavements may result in aircraft propeller, turbine engine, or tire damage. Also, loose materials may blow about, potentially causing personal injury or equipment damage.		
Inappropriate or poorly maintained fencing during construction intended to deter human and animal intrusions into the AOA. Fencing and other markings that are inadequate to separate construction areas from open AOA create aviation hazards.		
Improper or inadequate marking or lighting of runways (especially thresholds that have been displaced or runways that have been closed) and taxiways that could cause pilot confusion and provide a potential for a runway incursion. Inadequate or improper methods of marking, barricading, and lighting of temporarily closed portions of AOA create aviation hazards.		
Wildlife attractants — such as trash (food scraps not collected from construction personnel activity), grass seeds, tall grass, or standing water — on or near airports.		
Obliterated or faded temporary markings on active operational areas.		
Misleading or malfunctioning obstruction lights. Unlighted or unmarked obstructions in the approach to any open runway pose aviation hazards.		

Item	Action Required (Describe)	No Action Required (Check)
Failure to issue, update, or cancel NOTAMs about airport or runway closures or other construction related airport conditions.		
Failure to mark and identify utilities or power cables. Damage to utilities and power cables during construction activity can result in the loss of runway / taxiway lighting; loss of navigation, visual, or approach aids; disruption of weather reporting services; and/or loss of communications.		
Restrictions on ARFF access from fire stations to the runway / taxiway system or airport buildings.		
Lack of radio communications with construction vehicles in airport movement areas.		
Objects, regardless of whether they are marked or flagged, or activities anywhere on or near an airport that could be distracting, confusing, or alarming to pilots during aircraft operations.		
Water, snow, dirt, debris, or other contaminants that temporarily obscure or derogate the visibility of runway/taxiway marking, lighting, and pavement edges. Any condition or factor that obscures or diminishes the visibility of areas under construction.		
Spillage from vehicles (gasoline, diesel fuel, oil) on active pavement areas, such as runways, taxiways, aprons, and airport roadways.		
Failure to maintain drainage system integrity during construction (for example, no temporary drainage provided when working on a drainage system).		

Item	Action Required (Describe)	No Action Required (Check)
Failure to provide for proper electrical lockout and tagging procedures. At larger airports with multiple maintenance shifts/workers, construction contractors should make provisions for coordinating work on circuits.		
Failure to control dust. Consider limiting the amount of area from which the contractor is allowed to strip turf.		
Exposed wiring that creates an electrocution or fire ignition hazard. Identify and secure wiring, and place it in conduit or bury it.		
Site burning, which can cause possible obscuration.		
Construction work taking place outside of designated work areas and out of phase.		

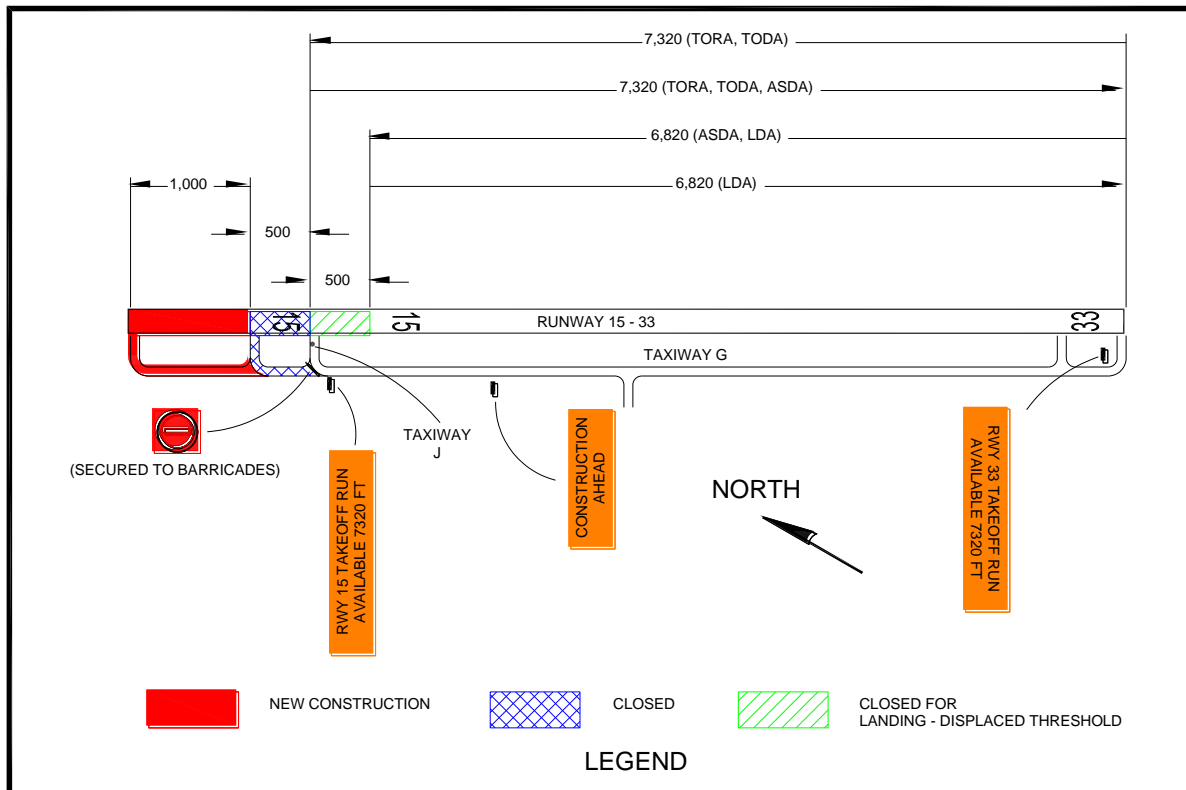
APPENDIX E. SAMPLE OPERATIONAL EFFECTS TABLE

E.1 Project Description.

Runway 15-33 is currently 7820 feet long, with a 500 foot stopway on the north end. This project will remove the stopway and extend the runway 1000 feet to the north and 500 feet to the south. Finally, the existing portion of the runway will be repaved. The runway 33 glide slope will be relocated. The new runway 33 localizer has already been installed by FAA Technical Operations and only needs to be switched on. Runway 15 is currently served only by a localizer, which will remain in operation as it will be beyond the future RSA. Appropriate NOTAMS will be issued throughout the project.

E.1.1 During Phase I, the runway 15 threshold will be displaced 1000 feet to keep construction equipment below the approach surface. The start of runway 15 takeoff and the departure end of runway 33 will also be moved 500 feet to protect workers from jet blast. Declared distances for runway 33 will be adjusted to provide the required RSA and applicable departure surface. Excavation near Taxiway G will require its ADG to be reduced from IV to III. See Figure E-1.

Figure E-1. Phase I Example

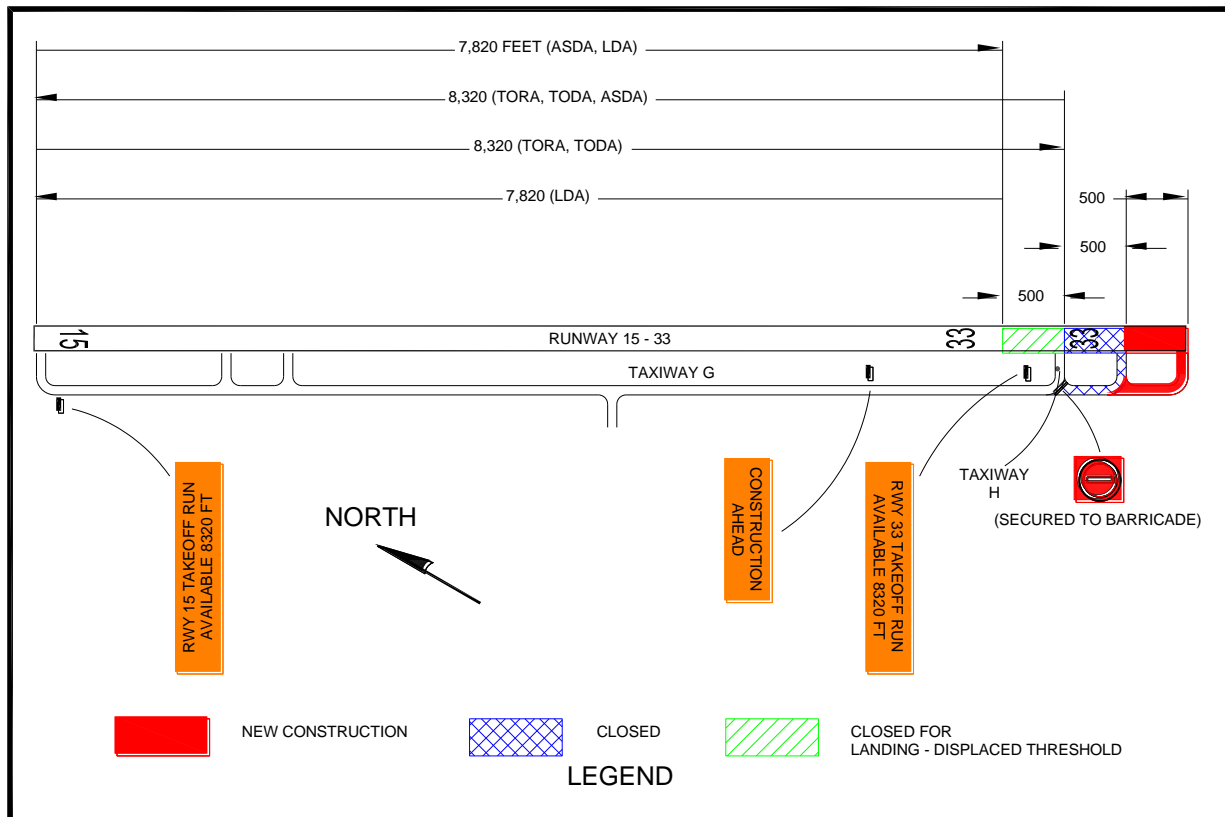


Note 1: Where hold signs are installed on both sides of a taxiway, install the TORA sign on the left side of the taxiway before the final turn to the runway intersection.

Note 2: Based on the declared distances for Runway 33 departures, the maximum equipment height in the construction area is 12.5 feet (500/40 = 12.5).

E.2 During Phase II, the runway 33 threshold will be displaced 1000 feet to keep construction equipment below the approach surface. The start of runway 33 takeoff and the departure end of runway 15 will also be moved 500 feet to protect workers from jet blast. Declared distances for runway 15 will be adjusted to provide the required RSA and applicable departure surface. See Figure E-2.

Figure E-2. Phase II Example



Note 1: Where hold signs are installed on both sides of a taxiway, install the TORA sign on the left side of the taxiway before the final turn to the runway intersection.

Note 2: Based on the declared distances for Runway 15 departures, the maximum equipment height in the construction area is 12.5 feet ($500/40 = 12.5$).

- E.3 During Phase III, the existing portion of the runway will be repaved with Hot Mix Asphalt (HMA) and the runway 33 glide slope will be relocated. Construction will be accomplished between the hours of 8:00 pm and 5:00 am, during which the runway will be closed to operations.

Figure E-3. Phase III Example

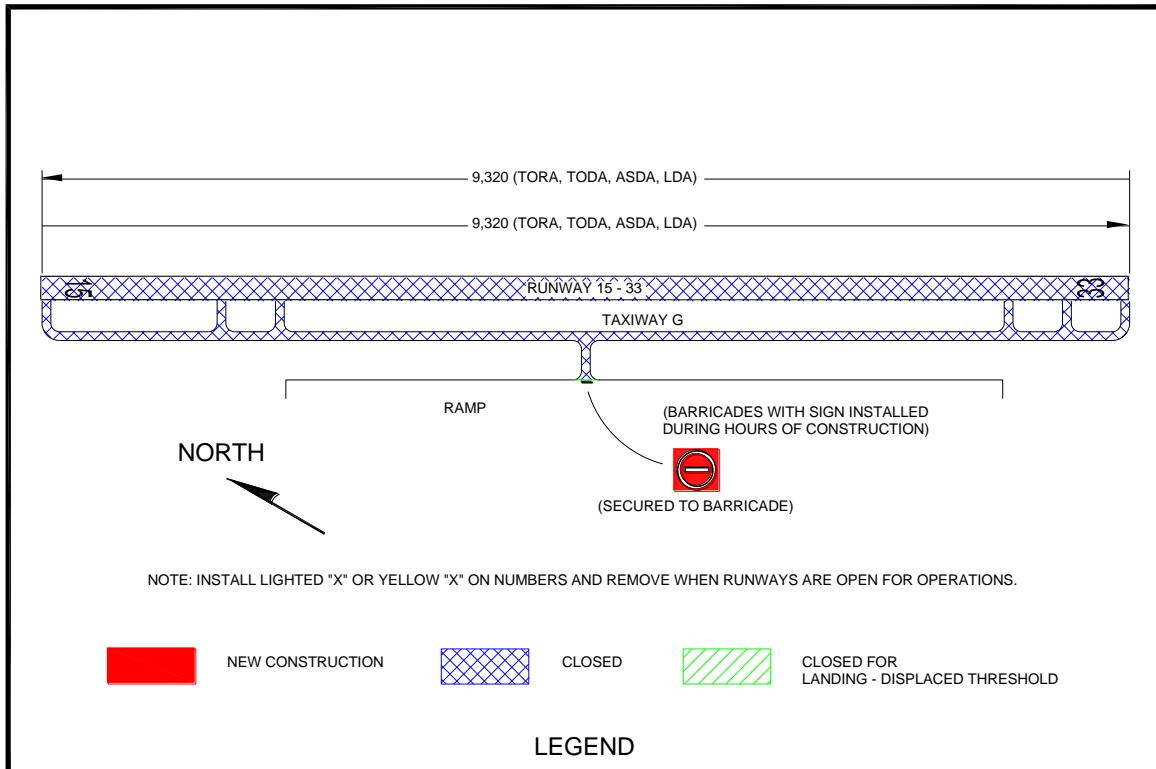


Table E-1. Operational Effects Table

Project	Runway 15-33 Extension and Repaving			
Phase	Normal (Existing)	Phase I: Extend Runway 15 End	Phase II: Extend Runway 33 End	Phase III: Repave Runway
Scope of Work	N/A	Extend Runway 15-33 1,000 ft on north end with Hot Mix Asphaltic Concrete (HMA).	Extend Runway 15-33 500 ft on south end with Hot Mix Asphaltic Concrete (HMA).	Repave existing runway with HMA Relocate Runway 33 Glide Slope
Effects of Construction Operations	N/A	Existing North 500 ft closed	Existing South 500 ft closed	Runway closed between 8:00 pm and 5:00 am Edge lighting out of service
Construction Phase	N/A	Phase I (Anticipated)	Phase II (Anticipated)	Phase III (Anticipated)
Runway 15 Average Aircraft Operations	Carrier: 52 /day GA: 26 /day Military: 11 /day	Carrier: 40 /day GA: 26 /day Military: 0 /day	Carrier: 45 /day GA: 26 /day Military: 5 /day	Carrier: 45 / day GA: 20 / day Military: 0 /day
Runway 33 Average Aircraft Operations	Carrier: 40 /day GA: 18 /day Military: 10 /day	Carrier: 30 /day GA: 18 /day Military: 0 /day	Carrier: 25 /day GA: 18 /day Military: 5 /day	Carrier: 20 /day GA: 5 /day Military: 0 /day
Runway 15-33 Aircraft Category	C-IV	C-IV	C-IV	C-IV
Runway 15 Approach Visibility Minimums	1 mile	1 mile	1 mile	1 mile
Runway 33 Approach Visibility Minimums	¾ mile	¾ mile	¾ mile	1 mile

Note: Proper coordination with Flight Procedures group is necessary to maintain instrument approach procedures during construction.

Project		Runway 15-33 Extension and Repaving			
Phase		Normal (Existing)	Phase I: Extend Runway 15 End	Phase II: Extend Runway 33 End	Phase III: Repave Runway
Runway 15 Declared Distances	TORA	7,820	7,320	8,320	9,320
	TODA	7,820	7,320	8,320	9,320
	ASDA	7,820	7,320	7,820	9,320
	LDA	7,820	6,820	7,820	9,320
Runway 33 Declared Distances	TORA	7,820	7,320	8,320	9,320
	TODA	7,820	7,320	8,320	9,320
	ASDA	8,320	6,820	8,320	9,320
	LDA	7,820	6,820	7,820	9,320
Runway 15 Approach Procedures		LOC only	LOC only	LOC only	LOC only
		RNAV	RNAV	RNAV	RNAV
		VOR	VOR	VOR	VOR
Runway 33 Approach Procedures		ILS	ILS	ILS	LOC only
		RNAV	RNAV	RNAV	RNAV
		VOR	VOR	VOR	VOR
Runway 15 NAVAIDs		LOC	LOC	LOC	LOC
Runway 33 NAVAIDs		ILS, MALSR	ILS, MALSR	ILS, MALSR	LOC, MALSR
Taxiway G ADG		IV	III	IV	IV
Taxiway G TDG		4	4	4	4
ATCT (hours open)		24 hours	24 hours	24 hours	0500 - 2000
ARFF Index		D	D	D	D

Project	Runway 15-33 Extension and Repaving			
Phase	Normal (Existing)	Phase I: Extend Runway 15 End	Phase II: Extend Runway 33 End	Phase III: Repave Runway
Special Conditions	Air National Guard (ANG) military operations	All military aircraft relocated to alternate ANG Base	Some large military aircraft relocated to alternate ANG Base	All military aircraft relocated to alternate ANG Base
Information for NOTAMs		Refer above for applicable declared distances. Taxiway G limited to 118 ft wingspan	Refer above for applicable declared distances.	Refer above for applicable declared distances. Airport closed 2000 – 0500. Runway 15 glide slope OTS.

Note: This table is one example. It may be advantageous to develop a separate table for each project phase and/or to address the operational status of the associated NAVAIDs per construction phase.

Complete the following chart for each phase to determine the area that must be protected along the runway and taxiway edges:

Table E-2. Runway and Taxiway Edge Protection

Runway/Taxiway	Aircraft Approach Category* A, B, C, or D	Airplane Design Group* I, II, III, or IV	Safety Area Width in Feet Divided by 2*

*See AC 150/5300-13 to complete the chart for a specific runway/taxiway.

Complete the following chart for each phase to determine the area that must be protected before the runway threshold:

Table E-3. Protection Prior to Runway Threshold

Runway End Number	Airplane Design Group* I, II, III, or IV	Aircraft Approach Category* A, B, C, or D	Minimum Safety Area Prior to the Threshold*	Minimum Distance to Threshold Based on Required Approach Slope*	
				ft	: 1
			ft	ft	: 1
			ft	ft	: 1
			ft	ft	: 1
			ft	ft	: 1

*See AC 150/5300-13 to complete the chart for a specific runway.

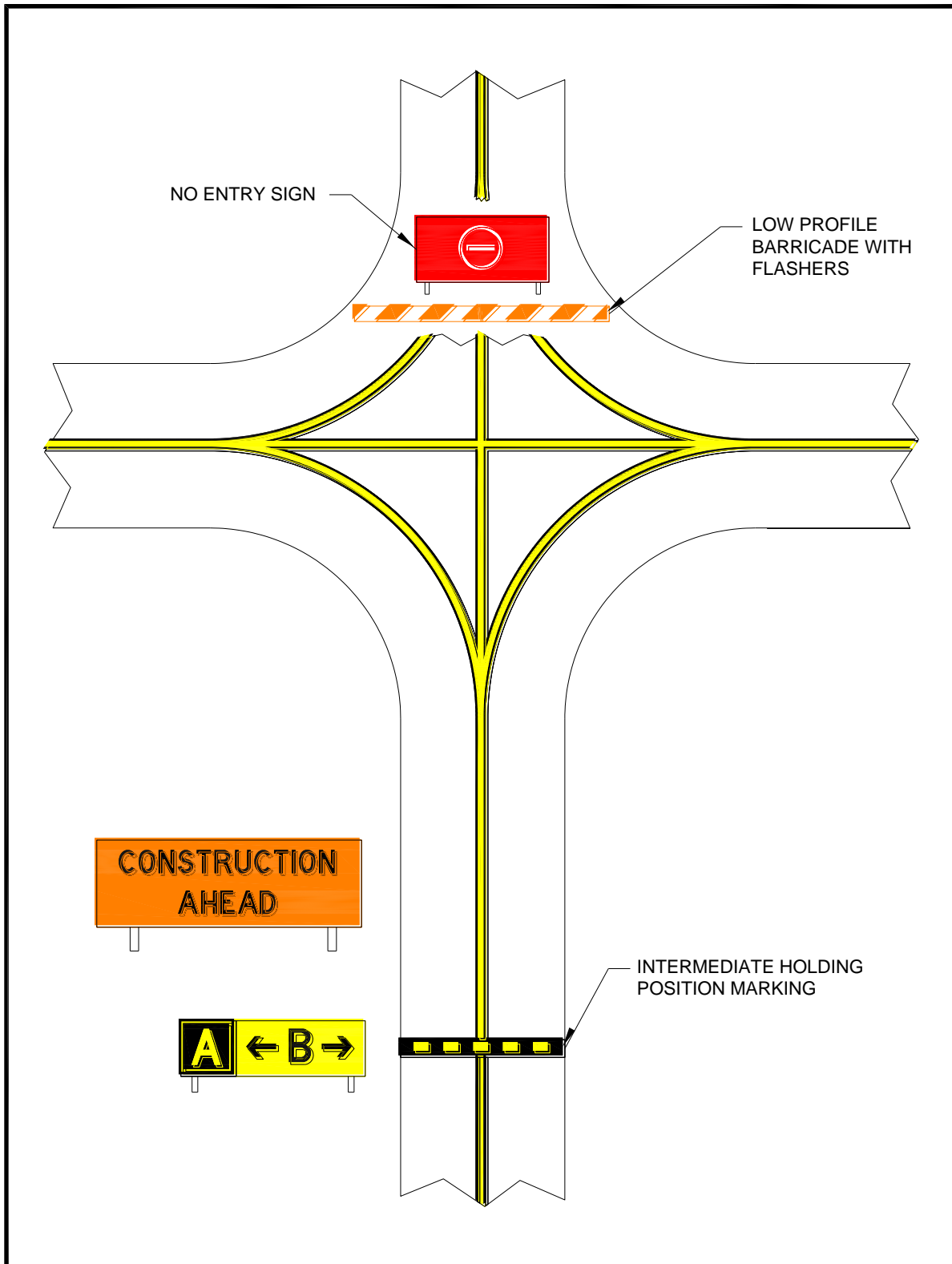
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APPENDIX F. ORANGE CONSTRUCTION SIGNS

Figure F-1. Approved Sign Legends

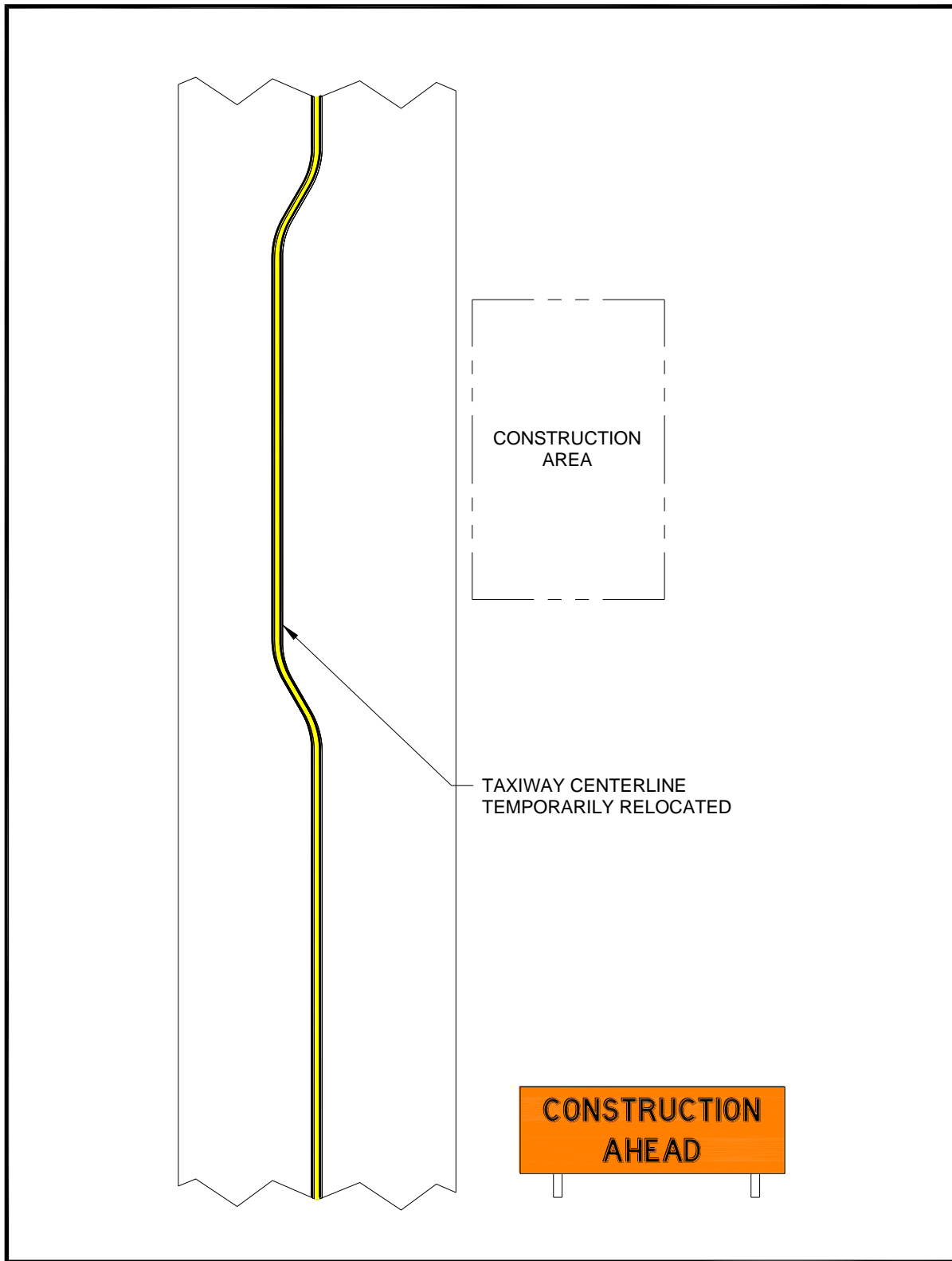


Figure F-2. Orange Construction Sign Example 1



Note: For proper placement of signs, refer to EB 93.

Figure F-3. Orange Construction Sign Example 2



Note: For proper placement of signs, refer to EB 93.

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Advisory Circular Feedback

If you find an error in this AC, have recommendations for improving it, or have suggestions for new items/subjects to be added, you may let us know by (1) mailing this form to Manager, Airport Engineering Division, Federal Aviation Administration ATTN: AAS-100, 800 Independence Avenue SW, Washington DC 20591 or (2) faxing it to the attention of the Office of Airport Safety and Standards at (202) 267-5383.

Subject: AC 150/5370-2G

Date: _____

Please check all appropriate line items:

An error (procedural or typographical) has been noted in paragraph _____ on page _____.

Recommend paragraph _____ on page _____ be changed as follows:

In a future change to this AC, please cover the following subject:
(Briefly describe what you want added.)

Other comments:

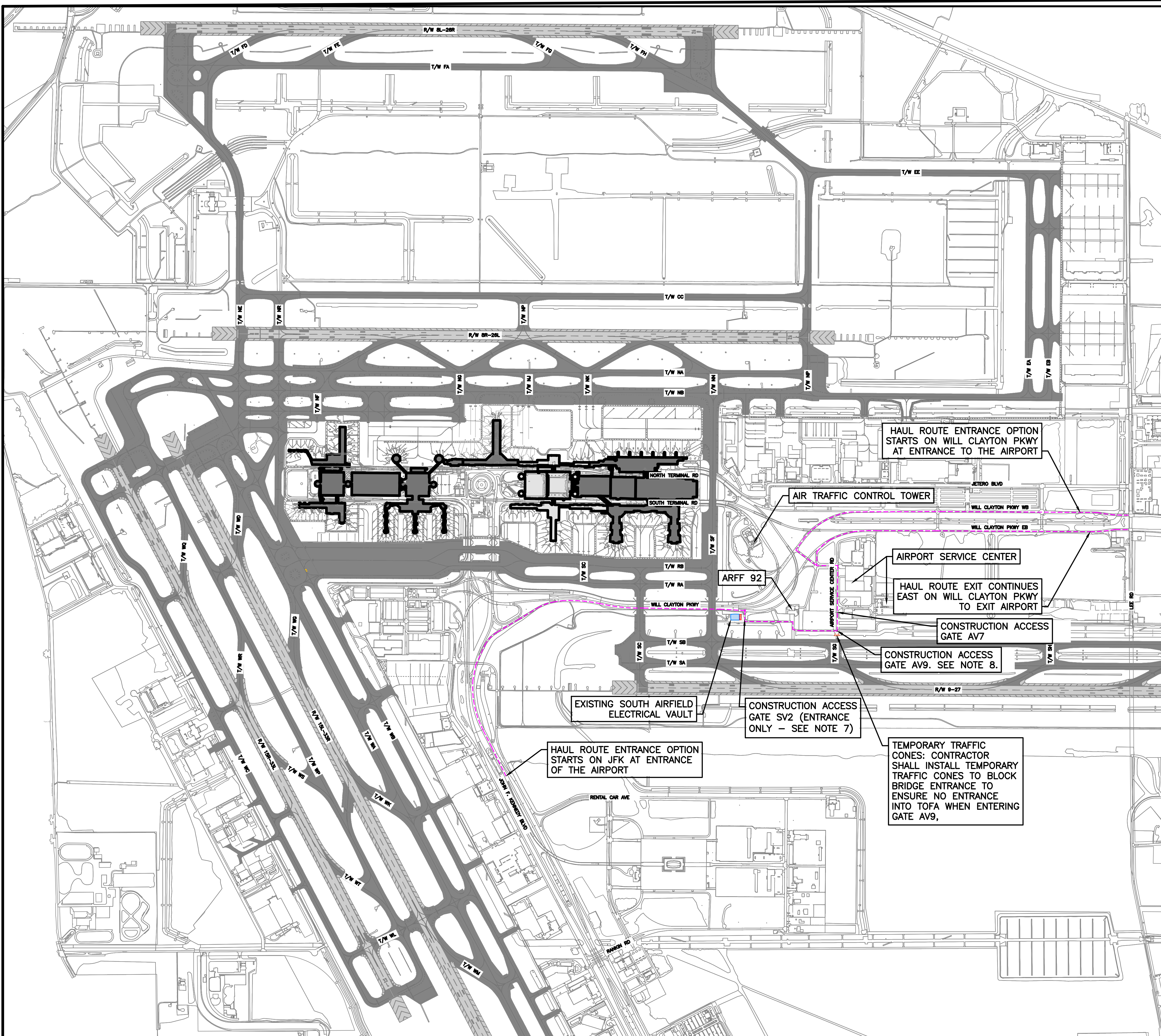
I would like to discuss the above. Please contact me at (phone number, email address).

Submitted by: _____

Date: _____

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Appendix D. Phasing Plans



LEGEND

	PHASE 1 WORK AREA
	CONSTRUCTION STAGING AREA
	HAUL ROUTE
▲▲▲	TEMPORARY TRAFFIC CONES

CONSTRUCTION PHASE DURATION AND WORK HOUR RESTRICTIONS

PHASE	DURATION (CAL. DAYS)	WORK HOURS
1	900 DAYS	24/7

NOTES:

1. CONSTRUCTION PHASING DURATION IS BASED ON ARCHITECTURE AND MECHANICAL PHASING DURATIONS.
2. SEE ARCHITECTURE AND MECHANICAL SERIES DRAWINGS FOR ADDITIONAL PHASING OF INTERIOR CONSTRUCTION.
3. ALL AOA PAVEMENT SURFACES SHALL BE OPEN FOR AIRPORT OPERATIONS AT ALL TIMES UNLESS OTHERWISE DESIGNATED AND APPROVED BY THE RPR FOR SPECIFIC CONSTRUCTION WORK PERIODS. THE ACCESS ROAD SHALL REMAIN OPERATIONAL AT ALL TIMES. THE AOA CONSISTS OF THE AREA INSIDE THE PERIMETER SECURITY FENCE.
4. PRIOR TO THE START OF EACH WORK DAY, THE CONTRACTOR SHALL COORDINATE EACH WORK PERIOD'S ACTIVITIES WITH AIRPORT OPERATIONS WHICH SHALL INCLUDE A CLOSURE AND BARRICADE PLAN FOR REVIEW AND APPROVAL. NO WORK WILL BE ALLOWED WITHIN THE AOA UNLESS IT HAS BEEN COORDINATED WITH AIRPORT OPERATIONS.
5. CONTRACTOR SHALL COORDINATE STAGING AREA SITE AND ACCESS WITH THE RPR.
6. MAIN ACCESS TO THE PROJECT SITE SHALL BE THROUGH ACCESS GATE SV2, ACCESS GATE AV9, AND ACCESS GATES AV7. CONTRACTOR TO SUPPLY GATE GUARDS, IF REQUIRED BY HAS.
7. CONTRACTOR SHALL UTILIZE ACCESS GATE AV2 AS AN ENTRANCE ONLY GATE TO AVOID TRAVEL THROUGH THE TERMINAL ROADWAYS. CONTRACTOR SHALL UTILIZE GATES AV9 AND AV7 WHEN LEAVING THE CONSTRUCTION SITE.
8. CONTRACTOR SHALL NOT EXTEND PASS THE TAXIWAY OBJECT FREE AREA (TOFA) WHEN ENTERING GATE AV9. CONTRACTOR SHALL CONTACTOR AIRSIDE OPERATIONS FOR ANY REASONS TO MOVE INTO THE TOFA.
9. TRAFFIC FLOW MUST NOT IMPEDE ARFF 92 ACCESS AT ANYTIME, I.E. BACKUPS, BREAKDOWNS, ETC.
10. PRIOR TO LEAVING THE WORK SITE EACH DAY, CONTRACTOR SHALL COORDINATE WITH OPERATIONS TO VERIFY BARRICADES ARE SET UP PROPERLY AND OPERATIONAL. REFER TO BARRICADE NOTES ON GC-901 FOR ADDITIONAL REQUIREMENTS.
11. EQUIPMENT HEIGHT AND LIGHTING RESTRICTIONS ARE LIMITED TO A MAXIMUM OF 15 FEET WITHIN THE CONTRACTOR STORAGE AREA LOCATION.
12. REFER TO HAUL ROUTE NOTES ON GC-001.
13. THE LOCATION OF CONTRACTOR PARKING WILL BE DETERMINED AT PRE-CONSTRUCTION MEETING.
14. DUE TO LIMITED SPACE ON THE AIRFIELD, THE CONTRACTOR MAY BE PROVIDED WITH AN ALTERNATE LOCATION ON AIRPORT PROPERTY.
15. CONTRACTOR SHALL NOT IMPEDE AND/ OR BLOCK TRAFFIC ON THE VSR WITHIN THE PROJECT WORK AREA AT ANY TIME.



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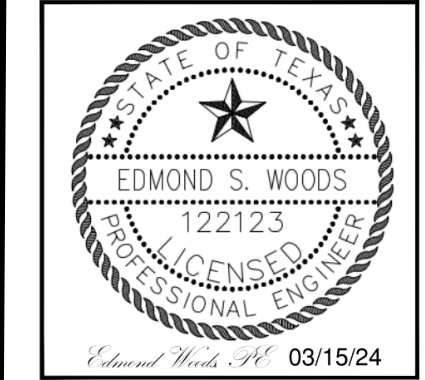
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 GEORGE BUSH INTERCONTINENTAL AIRPORT / HOUSTON
 4104 WILL CLAYTON PARKWAY, HOUSTON, TX 77032
 CONSTRUCTION SAFETY PHASING PLAN
 OVERALL CONSTRUCTION STAGING, STORAGE,
 HAUL ROUTE AND GATE ACCESS PLAN

PROJECT MGR: AEO
 DESIGNER: JM
 DRAWN BY: JW
 CHECK BY: EW

DATE: 03/15/24



APPROVED BY:

DIRECTOR
 HOUSTON AIRPORT SYSTEM

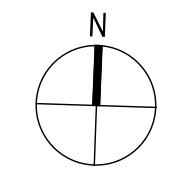
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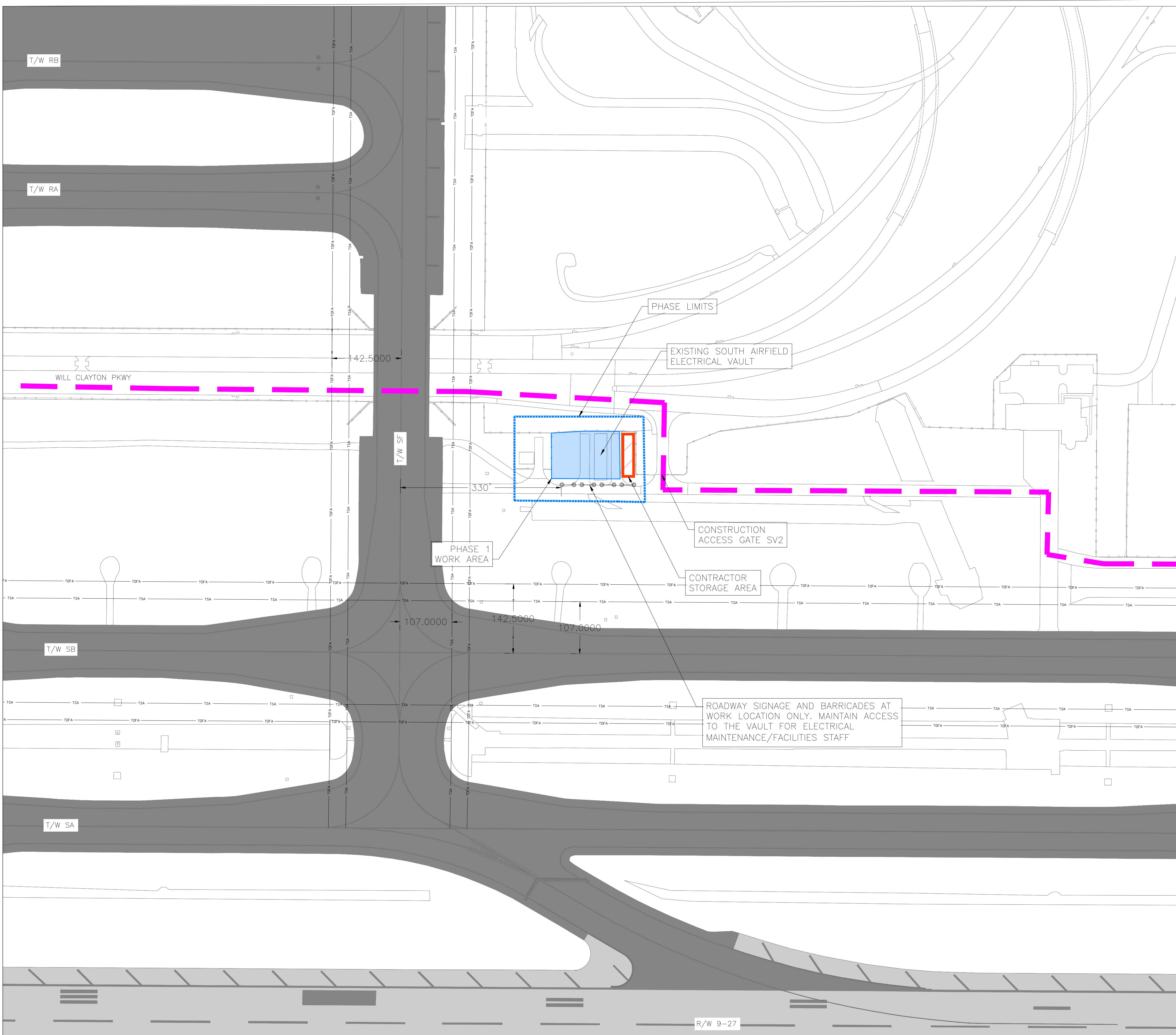
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GRAPHIC SCALE: 1" = 800'-0"



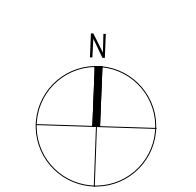
LEGEND

- PHASE 1 WORK AREA
- PHASE LIMITS
- CONSTRUCTION STAGING AREA
- HAUL ROUTE
- TYPE III BARRICADES

CONSTRUCTION PHASE DURATION AND WORK HOUR RESTRICTIONS		
PHASE	DURATION (CAL. DAYS)	WORK HOURS
1	900 DAYS	24/7

NOTES:

1. CONSTRUCTION PHASING DURATION IS BASED ON ARCHITECTURE AND MECHANICAL PHASING DURATIONS.
2. SEE ARCHITECTURE AND MECHANICAL SERIES DRAWINGS FOR ADDITIONAL PHASING OF INTERIOR CONSTRUCTION.
3. ALL AOA PAVEMENT SURFACES SHALL BE OPEN FOR AIRPORT OPERATIONS AT ALL TIMES UNLESS OTHERWISE DESIGNATED AND APPROVED BY THE RPR FOR SPECIFIC CONSTRUCTION WORK PERIODS. THE ACCESS ROAD SHALL REMAIN OPERATIONAL AT ALL TIMES. THE AOA CONSISTS OF THE AREA INSIDE THE PERIMETER SECURITY FENCE.
4. PRIOR TO THE START OF EACH WORK DAY, THE CONTRACTOR SHALL COORDINATE EACH WORK PERIOD'S ACTIVITIES WITH AIRPORT OPERATIONS WHICH SHALL INCLUDE A CLOSURE AND BARRICADE PLAN FOR REVIEW AND APPROVAL. NO WORK WILL BE ALLOWED WITHIN THE AOA UNLESS IT HAS BEEN COORDINATED WITH AIRPORT OPERATIONS.
5. CONTRACTOR SHALL COORDINATE STAGING AREA SITE AND ACCESS WITH THE RPR.
6. MAIN ACCESS TO THE PROJECT SITE SHALL BE THROUGH ACCESS GATE SV2, ACCESS GATE AV9, AND ACCESS GATES AV7. CONTRACTOR TO SUPPLY GATE GUARDS, IF REQUIRED BY HAS.
7. CONTRACTOR SHALL UTILIZE ACCESS GATE AV2 AS AN ENTRANCE ONLY GATE TO AVOID TRAVEL THROUGH THE TERMINAL ROADWAYS. CONTRACTOR SHALL UTILIZE GATES AV9 AND AV7 WHEN LEAVING THE CONSTRUCTION SITE.
8. CONTRACTOR SHALL NOT EXTEND PASS THE TAXIWAY OBJECT FREE AREA (TOFA) WHEN ENTERING GATE AV9. CONTRACTOR SHALL CONTACTOR AIRSIDE OPERATIONS FOR ANY REASONS TO MOVE INTO THE TOFA.
9. TRAFFIC FLOW MUST NOT IMPEDE ARFF 92 ACCESS AT ANYTIME, I.E. BACKUPS, BREAKDOWNS, ETC.
10. PRIOR TO LEAVING THE WORK SITE EACH DAY, CONTRACTOR SHALL COORDINATE WITH OPERATIONS TO VERIFY BARRICADES ARE SET UP PROPERLY AND OPERATIONAL. REFER TO BARRICADE NOTES ON GC-901 FOR ADDITIONAL REQUIREMENTS.
11. EQUIPMENT HEIGHT AND LIGHTING RESTRICTIONS ARE LIMITED TO A MAXIMUM OF 15 FEET WITHIN THE CONTRACTOR STORAGE AREA LOCATION.
12. REFER TO HAUL ROUTE NOTES ON GC-001.
13. THE LOCATION OF CONTRACTOR PARKING WILL BE DETERMINED AT PRE-CONSTRUCTION MEETING.
14. DUE TO LIMITED SPACE ON THE AIRFIELD, THE CONTRACTOR MAY BE PROVIDED WITH AN ALTERNATE LOCATION ON AIRPORT PROPERTY.
15. CONTRACTOR SHALL NOT IMPEDE AND/ OR BLOCK TRAFFIC ON THE VSR WITHIN THE PROJECT WORK AREA AT ANY TIME.



GRAPHIC SCALE: 1" = 100'-0"

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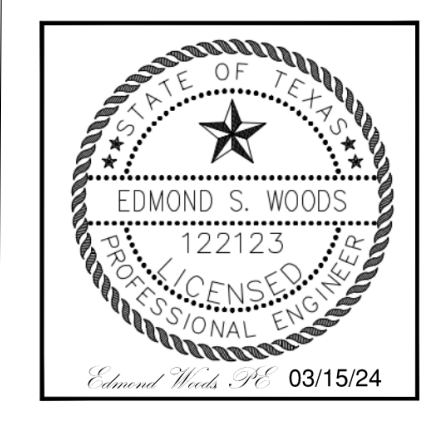
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4104 WILL CLAYTON PARKWAY, HOUSTON, TX 77032
CONSTRUCTION SAFETY PHASING PLAN
PHASING PLAN - PHASE 1

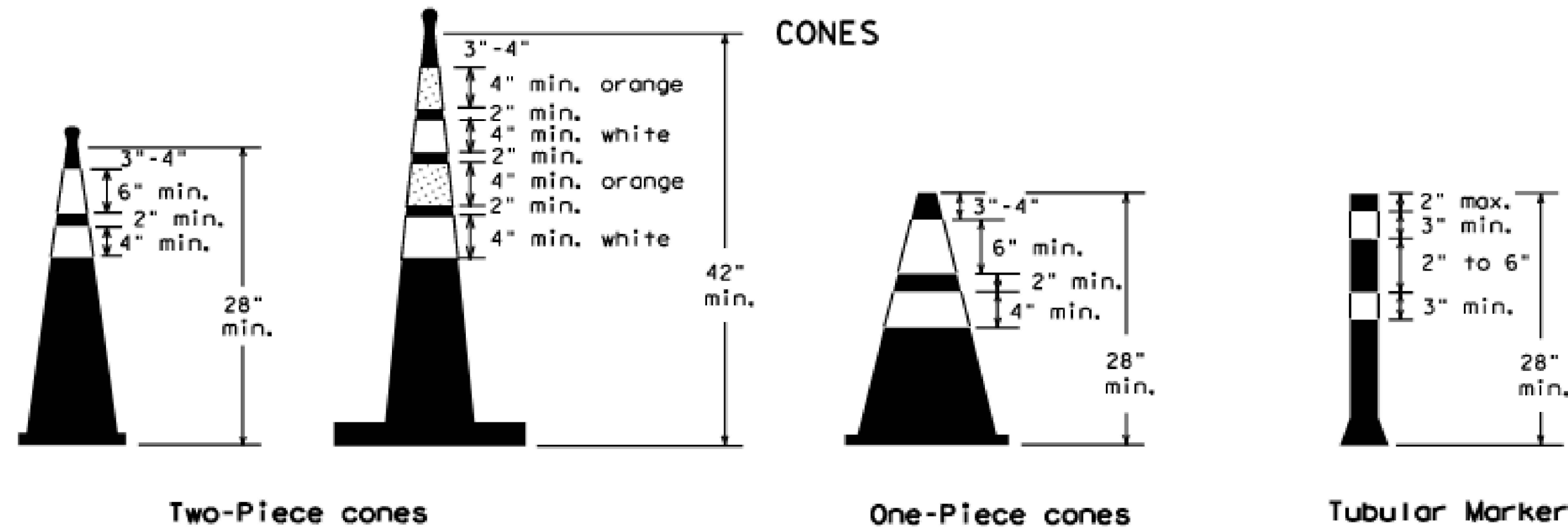
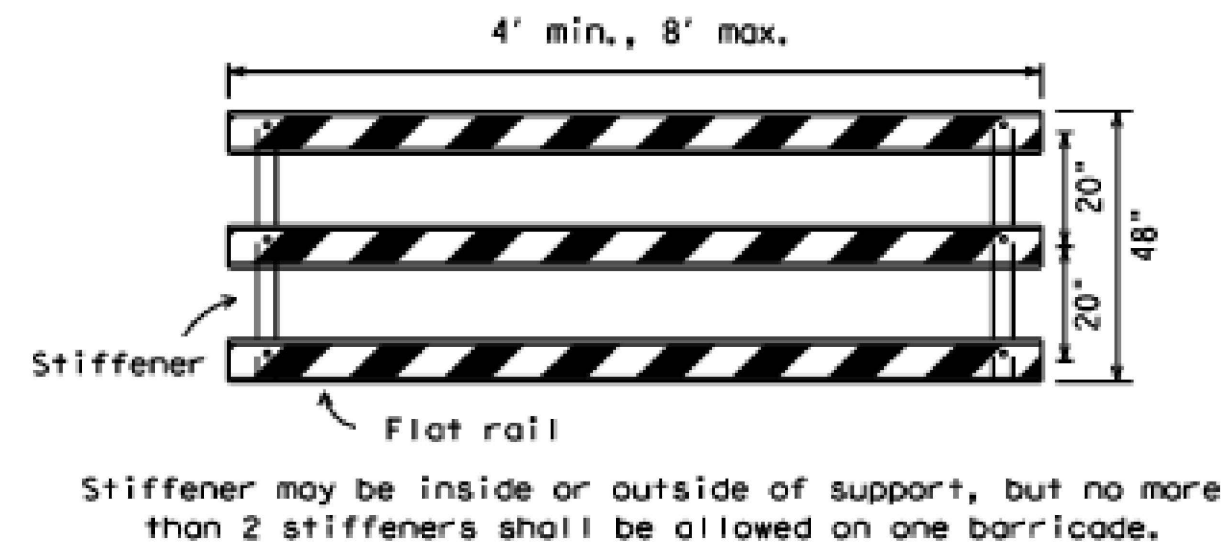
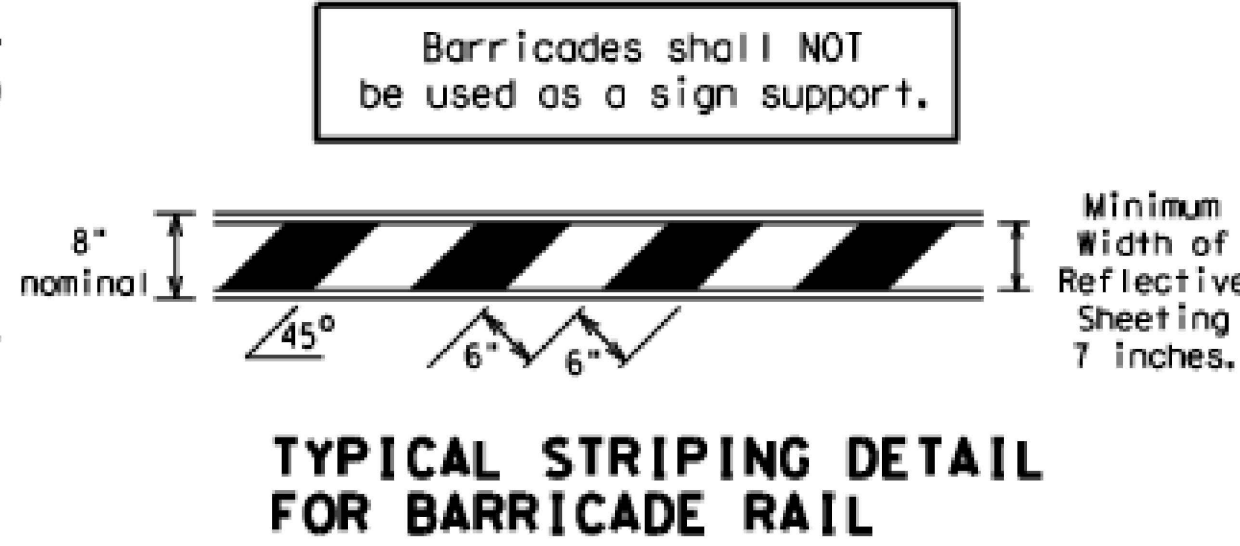
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DESIGNER: JM
DRAWN BY: JW
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TYPE 3 BARRICADES

1. Refer to the Compliant Work Zone Traffic Control Devices List (CWZTCD) for details of the Type 3 Barricades and a list of all materials used in the construction of Type 3 Barricades.
2. Type 3 Barricades shall be used at each end of construction projects closed to all traffic.
3. Barricades extending across a roadway should have stripes that slope downward in the direction toward which traffic must turn in detouring. When both right and left turns are provided, the chevron striping may slope downward in both directions from the center of the barricade. Where no turns are provided at a closed road, striping should slope downward in both directions toward the center of roadway.
4. Striping of rails, for the right side of the roadway, should slope downward to the left. For the left side of the roadway, striping should slope downward to the right.
5. Identification markings may be shown only on the back of the barricade rails. The maximum height of letters and/or company logos used for identification shall be 1".
6. Barricades shall not be placed parallel to traffic unless an adequate clear zone is provided.
7. Warning lights shall NOT be installed on barricades.
8. Where barricades require the use of weights to keep from turning over, the use of sandbags with dry, cohesionless sand is recommended. The sandbags will be tied shut to keep the sand from spilling and to maintain a constant weight. Sand bags shall not be stacked in a manner that covers any portion of a barricade rails reflective sheeting. Rock, concrete, iron, steel or other solid objects will NOT be permitted. Sandbags should weigh a minimum of 35 lbs and a maximum of 50 lbs. Sandbags shall be made of a durable material that tears upon vehicular impact. Rubber (such as tire inner tubes) shall not be used for sandbags. Sandbags shall only be placed along or upon the base supports of the device and shall not be suspended above ground level or hung with rope, wire, chains or other fasteners.
9. Sheeting for barricades shall be retroreflective Type A or Type B conforming to Departmental Material Specification DMS-8300 unless otherwise noted.



28" Cones shall have a minimum weight of 9 1/2 lbs.
 42" 2-piece cones shall have a minimum weight of 30 lbs, including base.

1. Traffic cones and tubular markers shall be predominantly orange, and meet the height and weight requirements shown above.
2. One-piece cones have the body and base of the cone molded in one consolidated unit. Two-piece cones have a cone shaped body and a separate rubber base, or ballast, that is added to keep the device upright and in place.
3. Two-piece cones may have a handle or loop extending up to 8" above the minimum height shown, in order to aid in retrieving the device.
4. Cones or tubular markers shall have white or white and orange reflective bands as shown above. The reflective bands shall have a smooth, sealed outer surface and meet the requirements of Departmental Material Specification DMS-8300 Type A or Type B.
5. 28" cones and tubular markers are generally suitable for short duration and short-term stationary work as defined on BC(4). These should not be used for intermediate-term or long-term stationary work unless personnel is on-site to maintain them in their proper upright position.
6. 42" two-piece cones, vertical panels or drums are suitable for all work zone durations.
7. Cones or tubular markers used on each project should be of the same size and shape.

SHEET 22 OF 29

Texas Department of Transportation *Traffic Safety Division Standard*

BARRICADE AND CONSTRUCTION CHANNELIZING DEVICES

BC (10) - 21

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FILE: OF THIS STANDARD TO OTHER FORMATS OR FOR INCORRECT RESULTS OR DAMAGES RESULTING FROM ITS USE.

BARRICADE AND CONSTRUCTION (BC) STANDARD SHEETS GENERAL NOTES:

1. The Barricade and Construction Standard Sheets (BC sheets) are intended to show typical examples for placement of temporary traffic control devices, construction pavement markings, and typical work zone signs. The information contained in these sheets meet or exceed the requirements shown in the "Texas Manual on Uniform Traffic Control Devices" (TMUTCD).
2. The development and design of the Traffic Control Plan (TCP) is the responsibility of the Engineer.
3. The Contractor may propose changes to the TCP that are signed and sealed by a licensed professional engineer for approval. The Engineer may develop, sign and seal Contractor proposed changes.
4. The Contractor is responsible for installing and maintaining the traffic control devices as shown in the plans. The Contractor may not move or change the approximate location of any device without the approval of the Engineer.
5. Geometric design of lane shifts and detours should, when possible, meet the applicable design criteria contained in manuals such as the American Association of State Highway and Transportation Officials (AASHTO), "A Policy on Geometric Design of Highways and Streets," the TxDOT "Roadway Design Manual" or engineering judgment.
6. When projects abut, the Engineer(s) may omit the END ROAD WORK, TRAFFIC FINES DOUBLE, and other advance warning signs if the signing would be redundant and the work areas appear continuous to the motorists. If the adjacent project is completed first, the Contractor shall erect the necessary warning signs as shown on these sheets, the TCP sheets or as directed by the Engineer. The BEGIN ROAD WORK NEXT X MILES sign shall be revised to show appropriate work zone distance.
7. The Engineer may require duplicate warning signs on the median side of divided highways where median width will permit and traffic volumes justify the signing.
8. All signs shall be constructed in accordance with the details found in the "Standard Highway Sign Designs for Texas," latest edition. Sign details not shown in this manual shall be shown in the plans or the Engineer shall provide a detail to the Contractor before the sign is manufactured.
9. The temporary traffic control devices shown in the illustrations of the BC sheets are examples. As necessary, the Engineer will determine the most appropriate traffic control devices to be used.
10. Where highway construction or maintenance work is being undertaken, other than mobile operations as defined by the Texas Manual on Uniform Traffic Control Devices, CSJ limit signs are required. CSJ limit signs are shown on BC(2). The OBEY WARNING SIGNS STATE LAW sign, STAY ALERT TALK OR TEXT LATER and the WORK ZONE TRAFFIC FINES DOUBLE sign with plaque shall be erected in advance of the CSJ limits. The BEGIN ROAD WORK NEXT X MILES, CONTRACTOR and END ROAD WORK signs shall be erected at or near the CSJ limits. For mobile operations, CSJ limit signs are not required.
11. Traffic control devices should be in place only while work is actually in progress or a definite need exists.
12. The Engineer has the final decision on the location of all traffic control devices.
13. Inactive equipment and work vehicles, including workers' private vehicles must be parked away from travel lanes. They should be as close to the right-of-way line as possible, or located behind a barrier or guardrail, or as approved by the Engineer.

SHEET 1 OF 29

Texas Department of Transportation *Traffic Safety Division Standard*

BARRICADE AND CONSTRUCTION GENERAL NOTES AND REQUIREMENTS

BC (1) - 21

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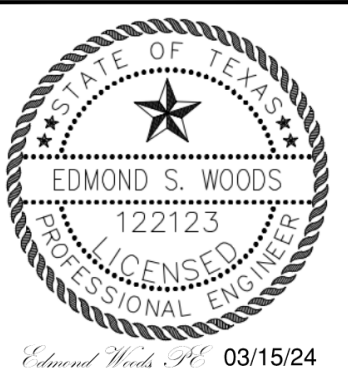
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 CONSTRUCTION SAFETY PHASING PLAN
 DETAILS

PROJECT MGR: AEO
 DESIGNER: JM
 DRAWN BY: JW
 CHECK BY: EW
 DATE: 03/15/24



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 C.I.P. NO. _____
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Appendix E. Safety and Security Notes

GENERAL PHASING NOTES

- NO WORK SHALL BE PERFORMED UNTIL ALL BARRICADES, CONSTRUCTION FENCE, AND ROADWAY CLOSURE MARKERS HAVE BEEN ERECTED AND ACCEPTED BY IAH AIRPORT OPERATIONS.
- NO EARTH DISTURBANCE SHALL OCCUR PRIOR TO THE INSTALLATION AND ACCEPTANCE OF THE EROSION AND SEDIMENTATION CONTROLS.
- PRIOR TO COMMENCING WORK IN ANY AREA OF THE AOA, THE CONTRACTOR SHALL SUBMIT A WORK AUTHORIZATION NOTICE (WAN) TO IAH AIRPORT OPERATIONS FOR APPROVAL AT LEAST 72 HOURS IN ADVANCE. NO WORK IN A NEW AREA SHALL BE PERMITTED WITHOUT AN APPROVED WAN. IF PROPOSED WORK INCLUDES EXTENDED MOVEMENT AREA CLOSURES, ADDITIONAL ADVANCE NOTIFICATION TIME MAY BE REQUIRED. WANS WILL BE PRESENTED TO STAKEHOLDERS BY THE HAS PROJECT MANAGER ON TUESDAYS.
- FLAGGERS MUST STAY OUTSIDE OF TOFA'S (160 FT FROM TAXIWAY CENTERLINE) OF ACTIVE TAXIWAYS.
- THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR COORDINATION OF CONSTRUCTION ACTIVITIES AND MAINTAINING CONSTANT COORDINATION BETWEEN THE SUBCONTRACTORS AND OTHER STAKEHOLDERS. ALL CONSTRUCTION ACTIVITIES PLANNED BY THE CONTRACTOR SHALL BE REVIEWED AND APPROVED BY IAH AIRPORT OPERATIONS.
- THE CONTRACTOR SHALL PLAN WORK ACTIVITIES SO AS TO MINIMIZE THE EXTENT AND TIME OF AIRFIELD PAVEMENT CLOSURES. AREAS REQUIRING MINIMAL WORK SHALL BE COMPLETED AND RESTORED TO OPERATING STATUS AS SOON AS PRACTICAL WITHIN EACH PHASE OF WORK.
- EACH PHASE OF WORK SHALL NOT BE CONSIDERED COMPLETE UNTIL THE CONCRETE PAVEMENT HAS REACHED REQUIRED STRENGTH, JOINT SEALANT HAS CURED, TEMPORARY PAVEMENT MARKINGS ARE ESTABLISHED, AND SIGNAGE AND LIGHTING CONFORM TO FAA STANDARDS. THESE ELEMENTS OF WORK, AND THEIR REQUIRED DURATIONS, MUST BE ACCOUNTED FOR IN THE CONTRACTOR'S SCHEDULE, AND THEY DO NOT ALLEVIATE THE CONTRACTOR FROM COMPLETING THE WORK AREAS WITHIN THE TIME REQUIREMENTS STATED IN THE CONTRACT. EACH PHASE OF WORK MUST BE COMPLETE AND ACCEPTED BY THE REPRESENTATIVE PRIOR TO RE-OPENING THE AIRFIELD PAVEMENT AREAS TO AIRCRAFT TRAFFIC.
- ALL WORK MUST BE ACCOMPLISHED DURING THE WORK HOURS LISTED IN THE CONSTRUCTION SAFETY AND PHASING PLAN (CSPP).
- PERMITS: IT IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR TO OBTAIN AND PAY FOR ALL APPLICABLE PERMITS FOR CONSTRUCTION AND EQUIPMENT.
- TEMPORARY DRAINAGE: THROUGHOUT THE COURSE OF CONSTRUCTION, THE CONTRACTOR SHALL PROVIDE AND INSTALL ANY DRAINAGE PROVISIONS NECESSARY TO MAINTAIN POSITIVE (CONTINUOUS AND FLOWING) DRAINAGE AND NOT RESTRICT THE EXISTING DRAINAGE FLOW PATTERN. AT THE END OF PROJECT THE CONTRACTOR SHALL RESTORE ALL GRADES, PER DESIGN PLANS, AND REMOVE ALL TEMPORARY DRAINAGE PIPES AND FACILITIES AT NO ADDITIONAL COST TO AUTHORITY.
- ARFF AND EMERGENCY RESPONSE VEHICLE ACCESS TO THE ACTIVE AIRFIELD SHALL BE MAINTAINED AT ALL TIMES.
- NO CONTRACTOR EMPLOYEE VEHICLES WILL BE ALLOWED WITHIN THE AOA. CONTRACTOR EMPLOYEE PARKING SHALL BE IN THE AREAS DESIGNATED ON THE PLANS OR IN ANOTHER AREA DESIGNATED BY AIRPORT PERSONNEL.
- ALL CONTRACTOR VEHICLES AND TRAFFIC SHALL REMAIN WITHIN THE DESIGNATED CONSTRUCTION LIMITS OR HAUL ROUTES. ABSOLUTELY NO CONTRACTOR VEHICLES WILL BE ALLOWED ON OTHER ACTIVE AIRFIELD OPERATIONS AREAS.
- WORK AREA LIMITS INDICATE THE PRIMARY LIMITS OF THE PERMANENT WORK TO BE CONSTRUCTED IN EACH PHASE. ADDITIONAL PERMANENT AND TEMPORARY WORK OUTSIDE OF THE WORK AREA LIMITS IS REQUIRED IN SOME CASES/PHASES. WORK OUTSIDE THE WORK AREA LIMITS SHOWN SHALL BE COORDINATED WITH IAH AIRPORT OPERATIONS A MINIMUM OF 7 BUSINESS DAYS IN ADVANCE. ANY ADDITIONAL BARRICADES REQUIRED TO PERFORM THIS WORK SHALL BE CONSIDERED INCIDENTAL TO OTHER PROJECT WORK.
- TEMPORARY WORK REQUIRED FOR EACH PHASE IS SHOWN IN THE PHASE IN WHICH IT WILL BE UTILIZED OR IS NECESSARY. IT SHALL BE COMPLETED BEFORE WORK MAY BEGIN WITHIN THE WORK AREA LIMITS OF EACH PHASE AND MAY REQUIRE WORK OUTSIDE THE WORK AREAS SHOWN. WHEN IN DOUBT, THE CONTRACTOR SHALL REQUEST FOR DIRECTION FROM THE ENGINEER AND IAH AIRPORT OPERATIONS ON THE WORK REQUIRED AT THE BEGINNING OF EACH PHASE AND IN NO CASE SHALL THE CONTRACTOR BE ALLOWED TO PROCEED WITH CLOSURE OF AREAS BEYOND THE SUBSEQUENT PHASE WITHOUT WRITTEN APPROVAL FROM IAH AIRPORT OPERATIONS. IT MAY ALSO BE POSSIBLE TO PERFORM THIS WORK EARLIER THAN THE PHASE IT IS SHOWN. TEMPORARY WORK MAY INCLUDE REMOVAL AND CONSTRUCTION OF TEMPORARY AOA FENCE AND/OR GATES, PLACEMENT OF CLOSURE BARRICADES, REMOVAL OF EXISTING PAVEMENT MARKINGS, APPLICATION OF TEMPORARY PAVEMENT MARKINGS, CONSTRUCTION OF TEMPORARY PAVEMENT, DRAINAGE STRUCTURES, AND TEMPORARY AIRFIELD LIGHTING, SIGNAGE, AND ELECTRICAL JUMPERS.
- INTERRUPTION TO AIRFIELD LIGHTING SYSTEMS SHALL BE LIMITED TO THOSE AREAS THAT ARE CLOSED IN SUPPORT OF CONSTRUCTION ACTIVITY OR AS COORDINATED IN ADVANCE WITH IAH AIRPORT OPERATIONS.
- PRIOR TO STARTING WORK WITHIN ANY PHASE, THE CONTRACTOR SHALL HOLD A PRE-PHASE COORDINATION MEETING. THE CONTRACTOR SHALL OBTAIN THE IAH AIRPORT OPERATIONS' APPROVAL TO CLOSE THE WORK AREA FOR THAT PHASE. THIS INCLUDES INSTALLATION OF LIGHTED BARRICADES, RUNWAY CLOSURE MARKERS, AND CONFIRMING THAT A NOTAM HAS BEEN ISSUED FOR THE CLOSURE AREA.
- CONSTRUCTION DURING THE PROJECT MAY BE HALTED AT ANY TIME BY THE OWNER, AND/OR IAH AIRPORT OPERATIONS IF IT IS DETERMINED TO BE IN THE BEST INTEREST OF AIRPORT OPERATIONS SAFETY OR SECURITY. THE CONTRACTOR MAY BE DIRECTED TO REMOVE EQUIPMENT AND/OR EVACUATE THE SITE IN ORDER TO ENABLE AIRCRAFT OPERATIONS FOR COMMERCIAL OR GENERAL AVIATION. NECESSARY EXTENSIONS IN CONTRACT TIME WILL BE GRANTED OR A STOP WORK ORDER WILL BE ISSUED DUE TO THESE DELAYS. HOWEVER, THERE WILL BE NO ADJUSTMENTS IN CONTRACT PRICE DUE TO THESE DELAYS. IN ADDITION TO THE ABOVE, THE FOLLOWING SPECIAL REQUIREMENTS WILL APPLY FOR ALL CONSTRUCTION PHASES:
 - A PRE-ACTIVITY MEETING SHALL BE HELD PRIOR TO THE START OF ANY PHASE, OR WHEN DEVIATIONS OR CHANGES FROM THE CONTRACT DOCUMENTS OCCUR, AND A DAILY SAFETY MEETING SHALL BE HELD PRIOR TO AND AT THE COMPLETION OF EACH WORK PERIOD, BETWEEN IAH AIRPORT OPERATIONS AND THE CONTRACTOR'S SUPERINTENDENT TO DISCUSS REQUIREMENTS FOR THE NEXT PHASE, WORK SHIFT OR WORK AREA. ANY WORK OR OPERATION, EITHER NEW OR OUT OF THE ORDINARY, SHALL REQUIRE A SAFETY/PRE-ACTIVITY MEETING.
 - THE CONTRACTOR SHALL INCORPORATE A SAFETY PLAN SPECIFIC TO DAYTIME AND NIGHTTIME CONSTRUCTION OPERATIONS IN THE SAFETY PLAN COMPLIANCE DOCUMENT (SPCD), AS WELL AS A CONTINGENCY PLAN TO ADDRESS CASES OF ABNORMAL FAILURES OR UNEXPECTED SITUATIONS USING APPENDIX D OF AC 150/5370-2G AS A GUIDE.
 - TRUCK HAUL ROUTES ON THE AIRFIELD SHALL BE DELINEATED WITH LIGHTED BARRICADES, SIGNAGE, OR OTHER MEANS APPROVED BY THE ENGINEER, HAS PROJECT MANAGER, AND IAH AIRPORT OPERATIONS TO CLEARLY MARK THE ROUTES TO THE WORK SITE. THE CONTRACTOR SHALL PARTICIPATE IN AN ENHANCED MOVEMENT AREA DRIVER TRAINING COURSE ADMINISTERED BY IAH AIRPORT OPERATIONS AND SHALL CONTINUOUSLY BRIEF ALL CONTRACTOR EMPLOYEES, SUPPLIERS, AND SUBCONTRACTORS ON THE HAUL ROUTES TO BE UTILIZED, BOTH PRIMARY AND ALTERNATE ROUTES, THROUGHOUT THE DURATION OF THIS CONTRACT.
 - FINES: PAYMENT OF ALL FINES ASSESSED TO GEORGE BUSH INTERCONTINENTAL AIRPORT DUE TO VIOLATIONS BY THE CONTRACTOR OF FAA/TSA SECURITY OR SAFETY REQUIREMENTS SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR AND SHALL BE DEDUCTED FROM MONIES DUE THE CONTRACTOR.
 - IF THE RESTRICTED AREA GATE IS FOUND TO BE OPEN OR UNLOCKED AND UNATTENDED, LAW ENFORCEMENT AND/OR TRANSPORTATION SECURITY ADMINISTRATION MAY ISSUE THE CONTRACTOR A CITATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL COURT COSTS AND IMPOSED FINES. IN ADDITION, A CHARGE OF UP TO \$10,000.00 MAY BE LEVIED BY THE HOUSTON AIRPORT SYSTEM AND/OR TRANSPORTATION SECURITY ADMINISTRATION FOR EACH VIOLATION SO DOCUMENTED AND UPON THE REQUEST FOR FINAL PAYMENT THE TOTAL OF ANY SUCH CHARGES WILL BE DEDUCTED FROM MONIES DUE THE CONTRACTOR.
 - IN THE EVENT THE CONTRACTOR DEVIATES FROM THE IDENTIFIED CONSTRUCTION LIMITS AND/OR DESIGNATED HAUL ROUTES ONTO AN ACTIVE RUNWAY, TAXIWAY OR TAXILANE, THE CONTRACTOR WILL BE FINED \$1,000 PER OCCURRENCE WHICH WILL BE DEDUCTED FROM THE FINAL CONTRACT AMOUNT DUE THE CONTRACTOR. IN ADDITION TO FINES, A NOTICE OF VIOLATION (NOV) MAY BE ISSUED, WHICH MAY INCLUDE SUSPENSION OF WORK OR TERMINATION, DEPENDING ON THE LEVEL OF VIOLATION COMMITTED.
- IDENTIFICATION - PERSONNEL: ALL CONTRACTOR EMPLOYEES, SUBCONTRACTORS, AGENTS, VENDORS, INVITEES, ETC., REQUIRING ACCESS TO THE CONSTRUCTION SITE SHALL, IN ACCORDANCE WITH THE AIRPORT SECURITY PLAN, CSPP AND SPCD, BE REQUIRED TO DISPLAY AIRPORT ISSUED IDENTIFICATION OR BE UNDER AIRPORT-APPROVED AND BADGED ESCORT PERSONNEL. THESE BADGES WILL BE IDENTIFIED NUMERICALLY AND ISSUED TO INDIVIDUAL EMPLOYEES WITH A PERMANENT RECORD MAINTAINED ON EACH INDIVIDUAL TO WHOM A BADGE IS ISSUED. IN ADDITION, A \$55 NON-REFUNDABLE PROCESSING FEE WILL BE REQUIRED FOR EACH BADGE. THIS FEE MUST BE PAID BEFORE A BADGE IS ISSUED. NO BADGE WILL BE ISSUED TO ANY PERSON UNTIL A REVIEW OF THE REQUIRED PAPERWORK BY AIRPORT SECURITY AND ALL REQUIREMENTS ARE MET. PAPERWORK SHALL BE SUBMITTED A

MINIMUM OF 24 HOURS BEFORE ISSUANCE OF A BADGE. THE CONTRACTOR IS RESPONSIBLE FOR PERSONNEL ATTENDING TRAINING AND COMPLETING SECURITY BADGE APPLICATIONS, WHICH WILL INCLUDE AIR/GROUND RADIO, TAXIWAY/TAXILANE, AND AIRPORT FAMILIARIZATION. ESTIMATED TIME FOR COMPLETION IS TWO (2) HOURS. FLAGGERS MUST BE BADGED AND MUST HAVE SUCCESSFULLY COMPLETED ENHANCED AIRPORT MOVEMENT/NON-MOVEMENT AREA TRAINING INSTRUCTED BY IAH AIRPORT OPERATIONS. IN ADDITION TO THE REGULAR BADGE AND MOVEMENT TRAINING, PRIOR TO PERFORMING IN THAT CAPACITY ON AIRPORT PROPERTY, AT THE COMPLETION OF THE CONTRACT ALL BADGES SHALL BE RETURNED TO THE AIRPORT. A CHARGE OF \$50 PER BADGE WILL BE ASSESSED FOR ALL UNRETURNED BADGES. GATE GUARDS AND ESCORTS SHALL BE CONSIDERED UNDER THE FLAGGER CLASSIFICATION AND SHALL BE SUBJECT TO THE SAME REQUIREMENTS AS FLAGGERS.

- AS REQUIRED DURING THE DAY AND AT THE END OF EACH DAY, ANY AIRFIELD PAVEMENT WHICH IS NOT CLOSED TO AIRCRAFT AND WHICH HAS BEEN USED BY THE CONTRACTOR, SHALL BE CLEANED BY VACUUM SWEEPER TRUCK OR OTHER ACCEPTABLE METHODS APPROVED BY THE OWNER. ALL EQUIPMENT SHALL BE STORED OR MOVED TO THE CONTRACTOR'S STAGING AREAS. CONTRACTOR SHALL MAINTAIN A MINIMUM OF TWO FULLY FUNCTIONING VACUUM SWEEPER TRUCKS ON SITE AT ALL TIMES. SWEEPERS NOT MAINTAINING A FOD FREE AREA SHALL BE REMOVED FROM THE PROJECT SITE AND REPLACED AT THE SOLE EXPENSE OF THE CONTRACTOR AT THE DISCRETION OF IAH AIRPORT OPERATIONS. A BACKUP SWEEPER SHALL BE AVAILABLE WITHIN 10 MINUTES OF THE SITE AT ALL TIMES.

AIRPORT SAFETY REQUIREMENTS

- THE CONTRACTOR SHALL DEVELOP A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) DETAILING HOW THE CONTRACTOR WILL COMPLY WITH THE CONSTRUCTION SAFETY AND PHASING PLAN (CSPP). THE SPCD SHALL DETAIL ALL ELEMENTS OF THE CONSTRUCTION DOCUMENTED IN THE CSPP, INCLUDING SPECIFIC HEIGHTS AND HAZARDS OF EQUIPMENT TO BE USED AND CONTRACTORS POINT OF CONTACT. THE SPCD SHALL BE SUBMITTED TO THE ENGINEER AND IAH AIRPORT OPERATIONS FOR REVIEW AND APPROVAL. THE SPCD SHALL BE PREPARED PER FAA AC 150/5370-2 AS INCLUDED IN APPENDIX A OF THE CSPP.
- AIRCRAFT TRAFFIC SHALL HAVE PRECEDENCE AT ALL TIMES ON ACTIVE PAVEMENT AREAS NOT SPECIFICALLY CLOSED FOR EACH PHASE WORK.
- THE CONTRACTOR SHALL ACQUAINT HIS/HER SUPERVISORS AND EMPLOYEES WITH THE AIRPORT ACTIVITY AND OPERATIONS THAT ARE INHERENT TO THIS ACTIVE AIR CARRIER AIRPORT AND SHALL CONDUCT THE CONSTRUCTION ACTIVITIES TO CONFORM TO ALL ROUTINE AND EMERGENCY AIR TRAFFIC REQUIREMENTS AND GUIDELINES ON SAFETY AS SPECIFIED HEREIN.
- WORK WITHIN ANY TAXIWAY OBJECT FREE AREA (TOFA) AND/OR RUNWAY SAFETY AREA (RSA) REQUIRE THE TAXIWAY AND/OR RUNWAY TO BE CLOSED. CONTRACTOR SHALL REQUEST TAXIWAY, TAXILANE, RUNWAY, APRON, OR VSR CLOSURES IN WRITING, FROM IAH AIRPORT OPERATIONS THROUGH THE HOUSTON AIRPORT SYSTEM (HAS) PROJECT MANAGER A MINIMUM OF 72 HOURS PRIOR TO ANY PLANNED WORK. ONLY UPON APPROVAL OF THE CLOSURE REQUEST MAY THE CONTRACTOR PROCEED WITH SAID WORK. FOR ANY REQUIRED RUNWAY CLOSURE, THE CONTRACTOR SHALL INSTALL RUNWAY CLOSURE X'S.
- ANY CONSTRUCTION ACTIVITY WITHIN THE RUNWAY SAFETY AREA OR TAXIWAY OBJECT FREE AREA OR OPEN EXCAVATIONS IN EXCESS OF THREE INCHES DEEP AND SLOPES GREATER THAN 3% WITHIN THE ABOVE AREAS, WILL REQUIRE CLOSURE OF THE AFFECTED RUNWAY OR TAXIWAY, UNLESS OTHERWISE APPROVED BY IAH AIRPORT OPERATIONS THROUGH THE HAS PROJECT MANAGER. CONSTRUCTION ACTIVITIES ARE NOT PERMITTED WITHIN 25 FEET OF PARKED AIRCRAFT WITHOUT PRIOR WRITTEN APPROVAL FROM IAH OPERATIONS.
- FOREIGN OBJECT DEBRIS (FOD) IS A MAJOR CONCERN ON THIS PROJECT. FOD CAN BE FROM BUT IS NOT LIMITED TO TRASH LEFT ON THE AIRFIELD, EXCESSIVE DUST, CONSTRUCTION MATERIALS SPILLAGE, AND CONSTRUCTION ACTIVITIES. THE CONTRACTOR SHALL MAINTAIN A CLEAN AND NEAT WORK AREA AT ALL TIMES. WATER TRUCKS SHALL BE UTILIZED TO CONTROL ALL BLOWING DUST AT ALL TIMES AND AS REQUESTED BY THE REPRESENTATIVE. SWEEPERS SHALL BE USED BEHIND ALL HAUL TRUCKS ON HAUL ROUTES, VSR, AND ON ADJACENT TAXIWAYS, AND APRON AREAS TO MAINTAIN A FOD FREE ENVIRONMENT.
- THE CONTRACTOR IS DIRECTED TO COMPLY WITH, AND ACQUAINT HIS/HER EMPLOYEES WITH, THE FOLLOWING FAA ADVISORY CIRCULARS. THESE DOCUMENTS AND RELATED REQUIREMENTS ARE REFERENCED IN DETAIL IN THE PROJECT SPECIFICATIONS:
 - 150/5200-18C, "AIRPORT SAFETY - SELF INSPECTION"
 - 150/5210-5D, "PAINTING, MARKING & LIGHTING OF VEHICLES USED ON AN AIRPORT"
 - 150/5370-2G, "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION"
- THE FOLLOWING SPECIAL REQUIREMENTS APPLY FOR NIGHT CONSTRUCTION:
 - A DAILY SAFETY AND PROGRESS MEETING SHALL BE HELD BETWEEN IAH AIRPORT OPERATIONS AND THE CONTRACTOR'S SUPERINTENDENT TO DISCUSS REQUIREMENTS FOR THE NEXT WORK PERIOD. THE CONTRACTOR MUST COORDINATE EACH NIGHT'S ACTIVITIES WITH IAH AIRPORT OPERATIONS BY 1700 HOURS EACH DAY, WHICH WILL INCLUDE A CLOSURE AND BARRICADE PLAN FOR REVIEW AND APPROVAL. NO WORK WILL BE ALLOWED WITHIN THE AOA UNLESS IT HAS BEEN COORDINATED WITH AIRPORT OPERATIONS FOR THAT NIGHT.
 - NO LOCKOUT TAGOUT IS ALLOWED AT NIGHT.
 - THE CONTRACTOR SHALL INCORPORATE A SAFETY PLAN SPECIFIC TO NIGHTTIME CONSTRUCTION OPERATIONS IN THE SAFETY PLAN COMPLIANCE DOCUMENT (SPCD), AS WELL AS A CONTINGENCY PLAN TO ADDRESS CASES OF ABNORMAL FAILURES OR UNEXPECTED DISASTERS USING APPENDIX D OF AC 150/5370-2G AS A GUIDE.
- THE CONTRACTOR SHALL ENSURE ALL SAFETY ITEMS, INCLUDING BARRICADES, LIGHTS, SIGNAGE, AND STRIPING ARE IN OPERABLE CONDITION AND VISIBLE AT ALL TIMES. THE WORK AREA(S) SHALL BE VISUALLY INSPECTED AND MAINTAINED DAILY TO THE SATISFACTION OF THE IAH AIRPORT OPERATIONS.
- THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS NECESSARY TO ENSURE THE SAFETY OF OPERATING AIRCRAFT AS WELL AS HIS/HER OWN EQUIPMENT AND PERSONNEL. SPECIAL CONSIDERATIONS SHOULD BE GIVEN TO FLIGHT SCHEDULES AND MISCELLANEOUS AIRCRAFT OPERATIONS. THE CONTRACTOR SHALL OBEY ALL INSTRUCTIONS AS TO ROUTES TO BE TAKEN BY EQUIPMENT TRAVELING WITHIN THE AIRPORT OPERATIONS AREA AND KEEP SUCH VEHICLES AND EQUIPMENT MARKED WITH THE SPECIFIED AIRPORT SAFETY FLAGS OR FLASHING YELLOW BEACONS. THE CONTRACTOR SHALL MAKE HIS OWN ESTIMATE OF ALL DIFFICULTIES TO BE ENCOUNTERED. EQUIPMENT NOT ACTUALLY IN OPERATION SHALL BE KEPT CLEAR OF OBJECT FREE AREAS. PERSONNEL SHALL NOT ENTER AREAS OF THE AIRPORT WHERE AIRCRAFT ARE OPERATING WITHOUT SPECIFIC PERMISSION. ALL EQUIPMENT SHALL REMAIN CLEAR OF ALL ACTIVE SAFETY AREAS AND OBJECT FREE AREAS.
- THE CONTRACTOR SHALL ERECT AND MAINTAIN ALL NECESSARY BARRICADES, SIGNS, DANGER SIGNALS AND LIGHTS FOR THE PROTECTION OF THE WORK AND THE SAFETY OF THE PUBLIC FOR BOTH LAND AND AIR TRAFFIC IN ACCORDANCE WITH THE SPECIFICATIONS (AC 150/5370-2G). ALL CONSTRUCTION LIGHTS USED FOR NIGHTTIME WORK MUST BE EQUIPPED WITH SHIELDS TO DIRECT LIGHT AWAY FROM THE RUNWAY AND ATCT.
- CLOSED RUNWAYS OR TAXIWAYS SHALL BE BARRICADED OFF AT ALL INTERSECTIONS WITH ACTIVE RUNWAYS OR TAXIWAYS. THE CONTRACTOR SHALL HAVE PERSONNEL ON CALL 24 HOURS PER DAY FOR EMERGENCY MAINTENANCE OF AIRPORT HAZARD LIGHTING AND BARRICADE.
- MEASURES SHALL BE ADOPTED TO PREVENT POTENTIAL POLLUTANTS FROM ENTERING ANY DRAINAGE SYSTEM OR WATERWAY. MATERIALS AND DEBRIS SHALL NOT BE STORED IN THE WORK AREA IN A MANNER THAT WOULD ALLOW THEM TO ENTER THE DRAINAGE SYSTEM AS A RESULT OF SPILLAGE, NATURAL RUNOFF OR FLOODING. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO IMMEDIATELY NOTIFY THE OWNER SHOULD THERE BE A SPILLAGE OF MATERIAL WHICH MIGHT CONTAMINATE THE DRAINAGE SYSTEM. IT SHALL ALSO BE THE CONTRACTOR'S RESPONSIBILITY TO REMOVE AND CLEAR UP SUCH SPILLAGE IN A MANNER ACCEPTABLE TO THE SPONSOR. MATERIAL SHALL BE SECURED SO THAT IT WILL NOT BE BLOWN BY THE WIND ONTO THE AIRFIELD SURFACES.
- CONSTRUCTION EQUIPMENT SHALL HAVE A MAXIMUM HEIGHT OF 33 FEET UNLESS A 7460 HAS BEEN FILED AND APPROVED BY FAA.
- ALL CONSTRUCTION PERSONNEL SHALL ATTEND A DAILY SAFETY BRIEFING PRIOR TO COMMENCING WORK FOR THE DAY. THESE MEETINGS SHALL BE MADE OPEN TO THE ENGINEER, OWNER, REPRESENTATIVE, IAH AIRPORT OPERATIONS, AND ANY OTHER GOVERNING AUTHORITY THAT WOULD LIKE TO ATTEND. THERE WILL ALSO BE A MANDATORY WEEKLY CONSTRUCTION MEETING WITH THE HAS PROJECT MANAGER, IAH OPERATIONS, THE ENGINEER, AND OTHER STAKEHOLDERS THAT MUST BE ATTENDED BY THE CONTRACTOR'S SENIOR FIELD STAFF, INCLUDING BUT NOT LIMITED TO SUPERINTENDENT(S) AND TEAM LEADERS. THE DATE AND TIME OF THE WEEKLY CONSTRUCTION MEETING WILL BE ESTABLISHED PRIOR TO THE START OF WORK AT THE PROJECT PRE-CONSTRUCTION CONFERENCE.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO SEE THAT ALL SHEETING, SHORING, AND BRACING IS DONE IN ACCORDANCE WITH CURRENT OSHA REGULATIONS AND REQUIREMENTS. SHEETING, SHORING, AND BRACING IS CONSIDERED TO BE AN INCIDENTAL PART OF THE WORK AND NO SEPARATE PAYMENT WILL BE ALLOW
- RESPONSIBILITY FOR TEMPORARY LIGHTING AND MARKING: AT THE START OF EACH PHASE, THE CONTRACTOR WILL BE RESPONSIBLE FOR FURNISHING AND MAINTAINING THE NECESSARY TEMPORARY MARKING, AIRFIELD LIGHTING, SIGNAGE, BARRICADES, AND HAZARD LIGHTING AS REQUIRED BY THE PLANS AND SPECIFICATIONS TO MARK CONSTRUCTION AREAS, HAZARDS, ETC. REFLECTORIZED ORANGE PLASTIC LOW-PROFILE WATER-FILLED BARRICADES WITH ATTACHED FLASHING RED

LIGHTS AND REFLECTIVE TAPE OR EQUAL. CONTRACTOR SHALL MAINTAIN BARRICADES FULL OF WATER TO PREVENT DISPLACEMENT BY JET BLAST AND IMMEDIATELY REPLACE ANY BARRICADES THAT LEAK.

- APPROVAL IS SUBJECT TO WITHDRAWAL AT ANY TIME BECAUSE OF CHANGE IN THE WEATHER, EMERGENCY CONDITIONS ON THE EXISTING AIRFIELD AREAS, ANTICIPATION OF EMERGENCY CONDITIONS, AND FOR ANY OTHER REASON DETERMINED BY IAH AIRPORT OPERATIONS ACTING UNDER THE ORDERS AND INSTRUCTIONS OF THE OWNER AND THE DESIGNATED FAA REPRESENTATIVE. ANY INSTRUCTIONS TO THE CONTRACTOR TO CLEAR ANY GIVEN AREA, AT ANY TIME, BY IAH AIRPORT OPERATIONS, THE OWNER OR THE FAA CONTROL TOWER (BY RADIO OR OTHER MEANS) SHALL BE IMMEDIATELY EXECUTED. CONSTRUCTION WORK WILL COMMENCE IN THE CLEARED AREA ONLY WHEN ADDITIONAL INSTRUCTIONS ARE ISSUED BY THE OWNER.
 - ALL COMMUNICATION WITH THE AIRPORT TRAFFIC CONTROL TOWER OR OTHER ELEMENTS OF THE AIRPORT SHALL BE THROUGH IAH AIRPORT OPERATIONS.
 - THE CONTRACTOR SHALL SUBMIT A DESTRUCTIVE/INCLEMENT WEATHER PLAN TO SET FORTH GENERAL GUIDANCE AND INFORMATION FOR THE CONTRACTOR TO COORDINATE PREPAREDNESS PLANS WHEN DESTRUCTIVE/INCLEMENT WEATHER THREATENS THE GEORGE BUSH INTERCONTINENTAL AIRPORT ENVIRONMENT.
 - CONSTRUCTION ACTIVITY IN THE VICINITY OF NAVIGATIONAL AIDS: 72 HOURS PRIOR TO THE PRE-CONSTRUCTION CONFERENCE AND/OR CONSTRUCTION START, THE CONTRACTOR SHALL CONTACT THE HAS PROJECT MANAGER TO COORDINATE WITH THE LOCAL AIRWAY FACILITIES MANAGER. THEIR REPRESENTATIVE WILL MEET WITH THE CONTRACTOR TO IDENTIFY FAA FACILITIES AND FAA CABLES.
 - THE CONTRACTOR SHALL INSTALL ALL REQUIRED BARRICADES AT DESIGNATED PLAN LOCATIONS, HAVE ALL ACCESS GATES GUARDED AND LOCKABLE, AND HAVE ALL EQUIPMENT EITHER FLAGGED AND FITTED WITH FLASHING YELLOW DOME-TYPE LIGHTS ON TOP OF THE VEHICLES, AS REQUIRED FOR DAYTIME AND NIGHTTIME CONSTRUCTION ACTIVITIES. ALL OF THESE ITEMS SHALL BE INTEGRATED AS A PART OF THE SPCD. THE CONTRACTOR SHALL INSTALL THE COMPONENTS OF THE SPCD AT THE APPROPRIATE TIMES AS SPECIFIED IN THE CONTRACT DOCUMENTS. THE CONTRACTOR SHALL INSPECT EVERY ASPECT OF THE SPCD ON AT LEAST A DAILY BASIS AND ENSURE ALL COMPONENTS ARE FUNCTIONING PROPERLY. IAH AIRPORT OPERATIONS SHALL ALSO INSPECT THE SYSTEM DAILY AND IF ANY DEFICIENCIES ARE NOTED, THE CONTRACTOR SHALL IMMEDIATELY TAKE STEPS TO CORRECT ANY AND ALL DEFICIENCIES. THE CONTRACTOR SHALL VISUALLY CHECK BARRICADE FLASHING LIGHTS ON A DAILY BASIS, 60 MINUTES BEFORE SUNSET FOR PROPER OPERATIONS. CONTRACTOR SHALL IMMEDIATELY REPLACE LIGHTS, BATTERIES, AND LAMPS AS DEEMED NECESSARY BY THE CONTRACTOR OR IAH AIRPORT OPERATIONS. THE SYSTEM ELEMENTS TO BE INSPECTED AND DEFICIENCIES NOTED ARE AS FOLLOWS:
 - BARRICADES SHALL BE ESTABLISHED IN ACCORDANCE WITH THE CONTRACT DOCUMENTS, SECURED FROM MOVEMENT AND FILLED WITH WATER. ALL FLASHING WARNING LIGHTS SHALL BE OPERATING PROPERLY.
 - ALL CONTRACTOR PERSONNEL AND EQUIPMENT ACCESS GATES STAFFED AND SECURITY PROCEDURES IN PLACE.
 - ALL EQUIPMENT OUTFITTED WITH FLASHING YELLOW DOME-TYPE LIGHTS, MARKINGS AND FLAGGING. CONTRACTOR USE OF AUTHORIZED AIRPORT ACCESS GATES CHECKED.
 - BARRICADES, TRAFFIC CONTROL, AND MANAGEMENT OF WORK SHOWN IN THE PLANS SHOWS GENERAL REQUIREMENTS; HOWEVER, THE CONTRACTOR IS RESPONSIBLE FOR SEQUENCING DETAILS WHICH MAY REQUIRE MORE OR LESS BARRICADES AND MOVEMENT OF BARRICADES TO FIT THE CONTRACTOR'S PROPOSED SEQUENCING AND SCHEDULE.
 - IAH AIRPORT OPERATIONS AND THE HOUSTON AIRPORT SYSTEM (HAS) SHALL, AT ALL TIMES, HAVE COMPLETE JURISDICTION OVER THE SAFETY OF ALL AIRCRAFT OPERATIONS DURING THE WORK. WHEREVER THE SAFETY OF AIR TRAFFIC IS CONCERNED, THE DECISIONS OF THE AIRPORT DIRECTOR, OR DESIGNATED REPRESENTATIVE, SHALL BE FINAL AS TO METHODS, PROCEDURES AND MEASURES USED. THIS IS NOT PAID FOR SEPARATELY AND IS INCIDENTAL TO OTHER PROJECT ITEMS. THE CONTRACTOR IS RESPONSIBLE FOR PROTECTING ALL WORK AND SEGREGATING THEIR WORK AREAS FROM OTHER AIRPORT USE AREAS.
- ## HAUL ROUTES
- LOCATION OF HAUL ROUTES ON THE AIRPORT SITE SHALL BE AS SPECIFIED ON THE PLANS OR AS APPROVED BY IAH AIRPORT OPERATIONS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE OFF-SITE HAUL ROUTES (STATE HIGHWAYS, COUNTY ROADS OR CITY STREETS) WITH THE APPROPRIATE OWNER WHO HAS JURISDICTION OVER THE AFFECTED ROUTE AND OBTAIN HAUL PERMITS NECESSARY AS REQUIRED BY THE LOCAL JURISDICTION. ON-SITE HAUL ROUTES SHALL BE MAINTAINED BY THE CONTRACTOR AND SHALL BE RESTORED TO THEIR ORIGINAL CONDITION UPON COMPLETION OF BEING USED AS A HAUL ROUTE. THE PRE- AND POST-CONSTRUCTION CONDITION OF ON-SITE HAUL ROUTES SHALL BE JOINTLY INSPECTED AND DETERMINED BY THE CONTRACTOR AND THE PROJECT REPRESENTATIVE. FENCING, DRAINAGE, GRADING AND OTHER MISCELLANEOUS CONSTRUCTION REQUIRED TO CONSTRUCT TEMPORARY HAUL ROUTES OR ACCESS POINTS ON THE AIRPORT WILL BE THE CONTRACTOR'S TOTAL RESPONSIBILITY AND SHALL BE APPROVED BY THE OWNER'S REPRESENTATIVE PRIOR TO THE WORK. ALL ON-SITE FAA ACCESS ROADS TO FAA FACILITIES SHALL REMAIN OPEN AND MAINTAINED AT ALL TIMES. PHOTOGRAPHS AND A VIDEO OF THE HAUL ROUTES SPECIFIED IN THE PLANS MUST BE PROVIDED BY THE CONTRACTOR BEFORE AND AFTER CONSTRUCTION TO THE OWNER'S REPRESENTATIVE. THE CONTRACTOR IS RESPONSIBLE FOR REPAIRING ANY DAMAGE TO HAUL ROUTES RESULTING FROM CONSTRUCTION TRAFFIC AS DETERMINED BY THE PROJECT REPRESENTATIVE. ANY SERVICE, ACCESS OR FAA ROADWAY CROSSED BY CONSTRUCTION TRAFFIC SHALL BE PROTECTED AGAINST DAMAGE AND ALL DAMAGE OCCURRING WILL BE REPAIRED AT THE CONTRACTOR'S EXPENSE WITH NO ADDITIONAL COMPENSATION OR CONTRACT TIME. ANY PAVEMENTS DAMAGED BY THE CONSTRUCTION EQUIPMENT SHALL BE REMOVED AND REPLACED TO AT LEAST 10 FEET ON EACH SIDE OF THE MOST EXTREME OUTER TIRE MARKS TO ENSURE ALL PAVEMENT TRAVERSED BY THE CONSTRUCTION EQUIPMENT IS REMOVED AND REPLACED.
 - FENCING, DRAINAGE, GRADING AND OTHER MISCELLANEOUS CONSTRUCTION REQUIRED TO CONSTRUCT TEMPORARY HAUL ROUTES OR ACCESS POINTS ON THE AIRPORT WILL BE THE CONTRACTOR'S TOTAL RESPONSIBILITY AND SHALL BE APPROVED BY IAH AIRPORT OPERATIONS PRIOR TO THE WORK.
 - PRIOR TO THE START OF USING A SPECIFIED HAUL ROUTE FOR EACH PHASE, THE CONTRACTOR AND REPRESENTATIVE SHALL REVIEW THE ROUTE AND VIDEO RECORD AND LOG ALL EXISTING DAMAGE. ALL USED HAUL ROUTES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION PRIOR TO THE COMPLETION OF THE PROJECT.
 - ALL CONTRACTOR VEHICLES DELIVERING MATERIAL TO THE PROJECT SITE SHALL ONLY ACCESS THE AOA THROUGH THE ALLOWABLE CONTRACTOR GATES AS SHOWN ON THE PLANS
 - CONSTRUCTION LIMITS: ALL CONTRACTOR VEHICLES AND TRAFFIC SHALL REMAIN WITHIN THE DESIGNATED CONSTRUCTION LIMITS OR HAUL ROUTES. ABSOLUTELY NO CONTRACTOR VEHICLES WILL BE ALLOWED ON OTHER ACTIVE AIRFIELD OPERATIONS AREAS. FLAGGERS SHALL BE PROVIDED AT ALL TIMES WHENEVER CONSTRUCTION ACCESS IS REQUIRED ACROSS AN ACTIVE TAXIWAY.
 - THE CONTRACTOR WILL ARRANGE WITH IAH AIRPORT OPERATIONS THROUGH THE HAS PROJECT MANAGER FOR INSPECTION PRIOR TO REOPENING FOR AIRCRAFT USE OF ANY RUNWAY, TAXIWAY OR TAXILANE THAT HAS BEEN CLOSED FOR CONSTRUCTION, ON OR ADJACENT THERETO, OR THAT HAS BEEN USED FOR A CROSSING POINT OR HAUL ROUTE BY THE CONTRACTOR. THE CONTRACTOR SHALL PERFORM PRE- AND POST-INSPECTIONS ON A DAILY BASIS AND PROVIDE A WRITTEN CHECKLIST TO THE HAS CONSTRUCTION MANAGER AND IAH AIRPORT OPERATIONS DOCUMENTING FOD INSPECTION AND CLEAN-UP, SECURITY, SAFETY AND OBJECT FREE AREA INSPECTIONS, AND SIGNAGE, LIGHTING, BARRICADE PLACEMENT AND OPERATIONS, AND VERIFY ALL BARRICADES ARE SECURED FROM MOVEMENT, THROUGHOUT THE DURATION OF THIS CONTRACT. ANY UNSAFE CONDITIONS SHALL BE CORRECTED IMMEDIATELY BY THE CONTRACTOR IN ACCORDANCE WITH THE CONTRACT.
 - TRUCK HAUL ROUTES ON THE AIRFIELD SHALL BE DELINEATED BY THE CONTRACTOR IF NOT COINCIDENT WITH EXISTING VEHICLE SERVICE ROADS. OTHER MEANS TO CLEARLY MARK THE ROUTES TO THE WORK SITE MAY BE APPROVED BY IAH AIRPORT OPERATIONS.
 - THE CONTRACTOR SHALL PROVIDE AND APPLY DUST CONTROL AT ALL TIMES, AS REQUIRED, TO ABATE NUISANCE DUST WHICH IS A DIRECT RESULT OF CONSTRUCTION ACTIVITIES ON AND ABOUT THE CONSTRUCTION AREA. MINIMUM OF ONE WATER TRUCK SHALL BE IN OPERATIONAL CONDITION ON SITE, DURING NON-WORKING HOURS. THE CONTRACTOR SHALL MAINTAIN A 25 MINUTE RESPONSE TIME, 24 HOURS A DAY AND 7 DAYS A WEEK THROUGHOUT THE CONTRACT TIME, TO ADDRESS DUST CONTROL AND FOD ISSUES IDENTIFIED BY THE AIRPORT OPERATIONS. HAULING VEHICLES SHALL BE EQUIPPED WITH OPERATIONAL COVERS. LOADS SHALL BE COVERED AT ALL TIMES EXCEPT FOR DURING LOADING AND UNLOADING.
 - HAUL ROADS DESIGNATED ON THE PLANS MAY ALSO BE USED BY AIRPORT OPERATIONS AND OTHER CONTRACTORS. CONTRACTOR SHALL NOT INTERFERE WITH OTHER AIRPORT TRAFFIC AND SHALL YIELD TO AIRCRAFT AND TO EMERGENCY VEHICLES. SPEED LIMIT ON HAUL ROADS IS 15 MILES PER HOUR MAXIMUM AND 5 MILES PER HOUR IN THE IMMEDIATE VICINITY OF AN AIRCRAFT.



HOUSTON AIRPORT SYSTEM

Jacobs

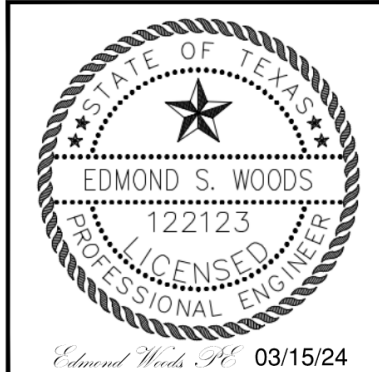
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REVISIONS		
NO.	DESCRIPTION	DATE
0	ISSUE FOR CONSTRUCTION	03/15/24

HOUSTON AIRPORT SYSTEM
PROJECT 952 SOUTH LIGHTING VAULT RENOVATION
GEORGE BUSH INTERCONTINENTAL AIRPORT / HOUSTON
4104 WILL CLAYTON PARKWAY, HOUSTON, TX 77032
CONSTRUCTION SAFETY PHASING PLAN
CONSTRUCTION PHASING NOTES 1

PROJECT MGR: AEO
DESIGNER: JM
DRAWN BY: JW
CHECK BY: EW

DATE: 03/15/24



APPROVED BY:

DIRECTOR
HOUSTON AIRPORT SYSTEM

JACOBS NO.

A.I.P. NO.

C.I.P. NO.

H.A.S. NO.

PN852

SHEET NO.

GC - 001

STAGING AREAS

- STAGING AREAS DO NOT HAVE UTILITIES. ANY UTILITIES REQUIRED BY THE CONTRACTOR SHALL BE COORDINATED WITH THE UTILITY COMPANIES. THE CONTRACTOR IS WHOLLY RESPONSIBLE FOR OBTAINING UTILITIES AND FOR STAGING AND PERMITTING. THIS SHALL BE DONE AT NO ADDITIONAL COST TO THE OWNER.
- MATERIALS DELIVERY TO THE SITE: ALL CONTRACTOR'S MATERIAL ORDERS FOR DELIVERY TO THE WORK SITE WILL USE, AS A DELIVERY ADDRESS, THE STREET NAME ASSIGNED TO THE ACCESS POINT AT THE CONTRACTOR'S STAGING AREA PROVIDED AS SHOWN IN THE PROJECT PLANS. THE NAME "GEORGE BUSH INTERCONTINENTAL AIRPORT" SHALL NOT BE USED IN THE DELIVERY ADDRESS AT ANY TIME. THIS WILL PRECLUDE DELIVERY TRUCKS FROM ENTERING INTO THE TERMINAL COMPLEX, OR TAKING SHORT CUTS THROUGH THE PERIMETER GATES AND INADVERTENTLY ENTERING INTO AIRCRAFT OPERATIONS AREAS.
- CONTRACTOR MAY STORE CONSTRUCTION EQUIPMENT ON AIRPORT PROPERTY AT THE END OF EACH DAY. NO EQUIPMENT OR MATERIAL WILL BE ALLOWED TO REMAIN WITHIN THE AOA (NOT IN OFA WHEN RUNWAY OR TAXIWAY IS OPEN) WHEN WORK IS NOT BEING PERFORMED. EQUIPMENT SHALL BE MOVED TO STAGING AREAS AT THE END OF EACH SHIFT. THE CONTRACTOR SHALL MAKE MODIFICATIONS WITHIN THE OVERNIGHT STAGING AREA WHEN DEEMED NECESSARY BY IAH AIRPORT OPERATIONS. UPON COMPLETION OF THE WORK, THE CONTRACTOR SHALL RESTORE AND RE-VEGETATE ALL OVERNIGHT STAGING AREAS TO ORIGINAL CONDITION. THIS WORK SHALL BE CONSIDERED INCIDENTAL TO OVERALL PERFORMANCE OF THE WORK ASSOCIATED WITH THE PROJECT.

STOCKPILES AND MATERIAL STORAGE

- UNLESS OTHERWISE ALLOWED BY AIRPORT MANAGEMENT, NO MATERIAL STOCKPILES MAY REMAIN AFTER THE PROJECT IS COMPLETE.
- STOCKPILE LOCATIONS SHALL BE SUBMITTED AS PART OF A WORK PLAN FOR EACH CONSTRUCTION PHASE AND APPROVED BY IAH AIRPORT OPERATIONS. OVERNIGHT STORAGE OF STOCKPILED OR EXCAVATED MATERIALS IN THE CONSTRUCTION AREA IS NOT PERMITTED UNLESS AN APPROVED STOCKPILE PLAN IS IN PLACE.
- MATERIALS STORED OR STOCKPILED ON THE AIRPORT SHALL BE SO PLACED AND THE WORK SHALL, AT ALL TIMES, BE SO CONDUCTED AS TO CAUSE NO GREATER OBSTRUCTION TO THE AIR AND GROUND TRAFFIC THAN IS CONSIDERED NECESSARY BY THE ENGINEER. MATERIALS SHALL NOT BE STOCKPILED WITHIN THE RUNWAY SAFETY AREA (RSA), OBSTACLE FREE ZONE (OFZ), OR TAXIWAY OBJECT FREE AREA (TOFA). STOCKPILING MATERIAL WITHIN THE RUNWAY OBJECT FREE AREA (ROFA), OR GREATER THAN 3' ANYWHERE IN THE AIRPORT OPERATIONS AREA (AOA), REQUIRES APPROVAL OF IAH AIRPORT OPERATIONS AND SUBMISSION OF FORM 7460-1 TO THE FAA. ALLOW FOR A MINIMUM 45 BUSINESS DAY REVIEW PERIOD. IF APPROVED, STOCKPILED MATERIAL MUST BE PROPERLY MARKED AND IDENTIFIED.
- STOCKPILED MATERIAL IN CONSTRUCTION AREAS WITHIN THE AOA SHALL REQUIRE SUBMISSION AND APPROVAL OF A 7460. STOCKPILES OUTSIDE THE AOA SHALL BE A MAXIMUM HEIGHT OF 15 FEET UNLESS A 7460 HAS BEEN FILED AND APPROVED BY FAA.
- CONCRETE AND ASPHALT RUBBLE AND EXCAVATION WASTE MATERIAL REMOVED FROM THE CONSTRUCTION AREA SHALL BE LEGALLY DISPOSED OF OFF THE AIRPORT PROPERTY. NO MATERIAL SHALL BE WASTED ON THE AIRPORT SITE UNLESS APPROVED BY THE AIRPORT. ANY ON-AIRPORT APPROVED WASTE AND DISPOSAL AREA SHALL BE SEEDED AND RESTORED IN A SMOOTH GRADED AND DRAINABLE CONDITION AT NO ADDITIONAL COST TO THE OWNER. PRIOR TO DEMOLITION AND REMOVAL WORK BEGINS, THE CONTRACTOR SHALL OBTAIN WRITTEN PERMISSION FROM THE SITE WHICH THEY PLAN TO DUMP WASTE MATERIAL AND PROVIDE IT TO THE AIRPORT.
- CONTRACTOR SHALL BE RESPONSIBLE FOR SAFELY STOCKPILING MATERIAL WITH RESPECT TO ALL ATMOSPHERIC CONDITIONS (WIND, RAIN) SO AS NOT TO PRODUCE A FOD HAZARD

CONTRACTOR VEHICLES AND EQUIPMENT

- ALL CONTRACTOR VEHICLES THAT ARE AUTHORIZED TO OPERATE ON THE AIRPORT IN THE ACTIVE AIRPORT OPERATIONS AREA (AOA) SHALL DISPLAY IN FULL VIEW A FLASHING AMBER (YELLOW) DOME-TYPE LIGHT, OR A 3' X 3" OR LARGER, ORANGE AND WHITE CHECKERBOARD FLAG, EACH CHECKERBOARD COLOR BEING 1-FOOT SQUARE WITH COMPANY LOGOS ON BOTH SIDES OF THE VEHICLE. (SEE FAA AC 150/5210-5D) AND ESCORTED UNDER THE CONTROL OF AN AUTHORIZED CONTRACTOR ESCORT. CONTRACTOR VEHICLES OPERATING ON ACTIVE PAVEMENTS IN THE MOVEMENT AREA SHALL BE UNDER THE CONTROL OF A GEORGE BUSH INTERCONTINENTAL (IAH) AIRPORT OPERATIONS ESCORT. CONTRACTOR SHALL COORDINATE A MINIMUM OF 48 HOURS IN ADVANCE TO REQUEST AN IAH AIRPORT OPERATIONS ESCORT. ANY VEHICLE OPERATING IN THE ACTIVE AOA DURING THE HOURS OF DARKNESS SHALL BE EQUIPPED WITH A FLASHING AMBER (YELLOW) DOME LIGHT, MOUNTED ON TOP OF THE VEHICLE AND OF SUCH INTENSITY TO CONFORM TO LOCAL CODES FOR MAINTENANCE AND EMERGENCY VEHICLES. ALL EQUIPMENT REQUIRED TO REMAIN WITHIN THE AOA DURING THE PERFORMANCE OF THIS CONTRACT SHALL BE EQUIPPED WITH RED BEACONS.
- ALL AOA CONSTRUCTION VEHICLES SHALL BE OPERATED BY A BADGED EMPLOYEE WITH RAMP DRIVING PRIVILEGES OR BE ESCORTED BY A VEHICLE OPERATED BY A BADGED EMPLOYEE WITH RAMP DRIVING AND ESCORT PRIVILEGES WHEN ENTERING THE AOA. ALL CONTRACTOR VEHICLES THAT ARE REQUIRED TO OPERATE ON OR ACROSS ACTIVE RUNWAYS, TAXIWAYS, TAXILANES, APRONS, CRITICAL NAVAIDS AREAS, AND RUNWAY APPROACH/DEPARTURE OR PROTECTION ZONES, SHALL DO SO UNDER THE DIRECT CONTROL OF AN IAH AIRPORT OPERATIONS ESCORT VEHICLE. WHILE IT IS PREFERRED THAT EACH CONSTRUCTION VEHICLE HAVE ITS OWN ESCORT, A MAXIMUM OF 3 VEHICLES PER ESCORT AND 2 18-WHEEL TRUCKS PER ESCORT WILL BE ALLOWED. THE CONTRACTOR SHALL PROVIDE A MINIMUM OF TWO ESCORTS FOR ALL HAULING ACTIVITIES; A LEAD ESCORT NECESSARY TO MEET THE MINIMUM ESCORTING REQUIREMENTS AND A TRAILING ESCORT REQUIRED FOR MONITORING POTENTIAL FOD AT ALL TIMES. ALL VEHICLES AND OPERATORS SHALL BE APPROVED BY IAH AIRPORT. ALL VEHICLES SHALL BE MARKED AND LIGHTED AS DESCRIBED IN THE AIRPORT SAFETY REQUIREMENTS.
- UNDER NO CIRCUMSTANCES WILL THERE BE ANY MOVEMENT OF CONTRACTOR VEHICLES AND/OR EQUIPMENT ACROSS ANY ACTIVE AIRFIELD PAVEMENTS AT ANY TIME UNLESS UNDER ESCORT BY IAH AIRPORT OPERATIONS. FLAGGERS SHALL BE REQUIRED AT ALL ACTIVE TAXIWAY/TAXILANE CROSSINGS UNLESS THE TAXIWAY/TAXILANE IS CLOSED. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR BE PERMITTED TO CROSS ANY RUNWAY WITHOUT PRIOR APPROVAL AND UNDER ESCORT OF IAH AIRPORT OPERATIONS.
- ALL PERSONNEL OPERATING A VEHICLE WITHIN THE AOA SHALL OBTAIN ALL NECESSARY SIDA, RAMP, NON-MOVEMENT AND MOVEMENT AIRFIELD DRIVER TRAINING AS REQUIRED BY IAH AIRPORT OPERATIONS PERSONNEL. CONTRACTOR WILL BE ISSUED SPECIFIC GATE ACCESS AND HAUL ROUTE CARDS IDENTIFYING THE APPROVED ACCESS LOCATIONS AND HAUL ROUTES.
- THE CONTRACTOR SHALL CONFINE HIS PERSONNEL, EQUIPMENT, OPERATIONS AND TRAVEL TO THE AREA WITHIN THE DEFINED WORK LIMITS SHOWN ON THE PLANS.
- THE CONTRACTOR SHALL INFORM ALL CONSTRUCTION PERSONNEL AS TO THE PROPER ROUTES, SPEEDS AND PROCEDURES FOR TRANSPORTING EQUIPMENT AND MATERIALS TO THE CONSTRUCTION SITE, AND ALL RESTRICTIONS TO MOVEMENT OF EQUIPMENT OR PERSONNEL WITHIN THE AIR OPERATIONS AREA. ALL PERSONNEL SHALL BE ADVISED OF ANY CHANGES IN AIRPORT OPERATIONS ON A DAILY BASIS, AND MORE OFTEN IF NECESSARY, THAT MAY FURTHER RESTRICT THEIR MOVEMENT.
- EQUIPMENT SHALL BE ALLOWED WITHIN THE RUNWAY OBJECT FREE AREA (ROFA) BUT MUST BE REMOVED FROM ROFA WHEN NOT IN USE.
- VEHICLES WITHIN THE AOA SHALL BE VISIBLY IDENTIFIABLE AS CONTRACTOR VEHICLES WHICH HAVE BEEN PROPERLY CLEARED FOR ENTRY (LOGO AND FLAGS/LIGHTS ON AUTHORIZED EQUIPMENT AND VEHICLES). VEHICLE LOGOS SHALL BE VISIBLE WITHIN 200' RANGE.
- THERE SHALL BE NO EXCAVATION DURING THE NIGHT WHEN AIRFIELD LIGHTING IS ACTIVE.

ELECTRICAL - GENERAL PHASING NOTES

- INFORMATION PROVIDED ON THE DRAWINGS FOR EXISTING UTILITIES, CABLES, DUCTS, MANHOLES, FIXTURES, ETC. ARE APPROXIMATE AND ARE NOT INTENDED TO PROVIDE EXACT LOCATIONS OR TYPE OF COMPONENT, AND SHOULD NOT BE SCALED FROM DRAWINGS. THE LOCATION OF MANHOLES, PULL BOXES, ETC. ALONG WITH THE ROUTE (AND IDENTIFICATION) FOR CIRCUITS SHOWN ON THE DRAWINGS SHALL BE VERIFIED PRIOR TO CONSTRUCTION FOR - AT MINIMUM - ALL CIRCUITS IDENTIFIED IN THE CIRCUIT TABLE ON SHEET ELO01.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO VERIFY AND SATISFY HIMSELF/HERSELF AS TO THE LOCATION OF ALL UNDERGROUND FACILITIES WITHIN THE AREA OF CONSTRUCTION. ANY EXISTING UTILITIES, CABLES, EQUIPMENT, DEVICES DAMAGED IN THE COURSE OF THIS CONTRACT SHALL BE IMMEDIATELY REPAIRED AT THE EXPENSE OF THE CONTRACTOR TO THE SATISFACTION OF THE OWNER.

- WHERE ANY ITEM IS FOUND TO BE LOCATED DIFFERENTLY THAN IS SHOWN ON THE DRAWINGS, THE ACTUAL LOCATION SHALL BE IMMEDIATELY MEASURED AND RECORDED ON THE RECORD DRAWING, AND THE RPR SHALL BE NOTIFIED IMMEDIATELY.
- EXISTING UTILITY LOCATIONS SHOWN ON THE PLANS ARE APPROXIMATE AND SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO COMMENCING ANY WORK. ANY INTERRUPTION OF UTILITY SERVICE SHALL BE COORDINATED AND APPROVED BY THE RPR PRIOR TO COMMENCING WORK.
- CIRCUIT ROUTING IS SHOWN AS BELIEVED TO EXIST IN THE FIELD BASED ON RESEARCH AND FIELD INVESTIGATION, HOWEVER, MANY DEVIATIONS ARE EXPECTED DUE TO THE LACK OF A COMPREHENSIVE AS-BUILT. AS SUCH, THE CONTRACTOR SHALL REQUEST THE CONSTRUCTION MANAGER IN WRITING FOR ALL RECORD DRAWINGS OF THE AREA IN CONSTRUCTION. THE CONTRACTOR SHALL REVIEW ALL RECORD DRAWINGS AND BECOME FAMILIAR WITH EXISTING CONDITIONS PRIOR TO ANY CONSTRUCTION.
- THE CONTRACTOR SHALL COORDINATE WITH DIG/SAFE TEXAS 811, HAS SERVICE DESK (281-233-1900), HAS ITPM SHAWN SUSKI (713-392-0769), FAA, AND ANY OTHER STAKE HOLDER EXPRESSING INTEREST A MINIMUM OF 72 HOURS PRIOR TO STARTING CONSTRUCTION. ADDITIONAL CONTACTS FOR PERSONNEL AT EACH AGENCY SHOULD BE REQUESTED AT THE PRE-CONSTRUCTION MEETING.
- WORK SHALL CONFORM TO LOCAL HAS DESIGN REQUIREMENTS, FAA CRITERIA, AND ALL LOCAL AND NATIONAL CODES.
- REFER TO CIVIL DEMOLITION, MARKING, LAYOUT AND ALL CIVIL DESIGN ITEMS. CIVIL INFORMATION SHOWN ON ELECTRICAL DRAWINGS IS FOR REFERENCE ONLY.
- ANY UNSCHEDULED INTERRUPTION OF SERVICE TO ACTIVE LIGHTING CIRCUITS SHALL BE IMMEDIATELY REPAIRED BY THE CONTRACTOR. ANY DAMAGE TO EXISTING BASE CANS, CIRCUITS, OR EQUIPMENT CAUSED BY THE CONTRACTOR'S EQUIPMENT OR PERSONNEL SHALL BE PROMPTLY REPAIRED AT THE CONTRACTOR'S EXPENSE. ALL ACTIVE LIGHTING SYSTEMS FOR OPEN AIRCRAFT OPERATIONAL AREAS SHALL REMAIN READY FOR OPERATION DURING THE IFR WEATHER CONDITIONS AND FROM DUSK TO DAWN OR AS DIRECTED BY THE RPR.
- CIRCUITS AND EQUIPMENT SHALL BE TAGGED AND LABELED. FURTHERMORE, THE CONTRACTOR SHALL VERIFY CIRCUIT ID TAGS FOR PRIMARY CIRCUITS THAT ARE BEING ACCESSED PRIOR TO MAKING ANY CHANGES TO THE CIRCUIT. IF THE CIRCUIT ID IS DIFFERENT FROM WHAT IS SHOWN ON THE CONTRACT DOCUMENTS, THE RPR SHALL BE NOTIFIED IMMEDIATELY.
- FOR EQUIPMENT NOTED TO BE REMOVED, THE CONTRACTOR SHALL COORDINATE WITH THE RPR AND EITHER SALVAGE OR DISPOSE OF THE EQUIPMENT AT THE DISCRETION OF HAS MAINTENANCE.
- THE CONTRACTOR SHALL MEGGER EACH EXISTING CIRCUIT PER SPECIFICATION L-108 AND PROVIDE THE READINGS IN WRITING TO THE RPR PRIOR TO COMMENCING WORK. AT THE COMPLETION OF THE PROJECT, ANY MEASUREMENTS NOT MEETING OR EXCEEDING THE PRE-CONSTRUCTION MEASUREMENTS SHALL REQUIRE THE CONTRACTOR TO LOCATE AND REPLACE CABLE/CONNECTORS OR ISOLATION TRANSFORMERS AS NECESSARY AT NO ADDITIONAL COST TO THE OWNER.
- THE CONTRACTOR SHALL NOT PERFORM MEGGER TESTING AT A VOLTAGE HIGHER THAN WHAT IS STATED IN SPECIFICATION L-108.
- THE CONTRACTOR SHALL MANUALLY LOCK-OUT EACH CIRCUIT AT THE FIELD LIGHTING VAULTS WHEN WORK IS BEING PERFORMED ON THE CIRCUIT. THE CIRCUIT SHALL BE TAGGED AND THE CONTRACTOR'S NAME SHALL BE CLEARLY IDENTIFIED ON EACH TAG. THE CONTRACTOR SHALL HAVE A LOCK-OUT KIT ON SITE AT ALL TIMES. THE RPR SHALL BE NOTIFIED EACH TIME A CIRCUIT IS SECURED AND EACH TIME THE CIRCUIT IS RETURNED TO REMOTE CONTROL. THE CONTRACTOR WILL DESIGNATE ONE INDIVIDUAL IN WRITING WHO WILL BE RESPONSIBLE FOR LIGHTING VAULT ACCESS AND FOR LOCK-OUT/TAG-OUT PROCEDURES. THE CONTRACTOR SHALL PROVIDE DAILY COORDINATION WITH THE RPR FOR VAULT ACCESS AND CIRCUIT INTERRUPTION. WEATHER AND OTHER CONSIDERATIONS MAY PRECLUDE CIRCUIT INTERRUPTION.

LOCK-OUT SHALL BE COORDINATED WITH HAS MAINTENANCE STAFF AND MEET HAS REQUIREMENTS.

- THE CONTRACTOR SHALL CLEAN/SWAB OUT EXISTING DUCTS BEING USED. USE SEWER TAPE TO CLEAR OBSTRUCTIONS WITHIN CONCRETE ENCASED DUCTS.
- BASE CANS SHALL BE MADE OF GALVANIZED STEEL AND MEET THE REQUIREMENTS OF FAA BASE CAN TYPES L-867 AND L-868, CLASS 1A AND SPECIFICATIONS L-115 AND L-125.
- THE CONTRACTOR SHALL CEASE PULLING THE MANDREL THROUGH CONDUIT IF IT DOES NOT PULL FREELY. NOTIFY THE RPR WHERE THIS OCCURS.
- 1/4" BOLTS AND LARGER SHALL BE HEX HEAD. BOLTS SMALLER THAN 1/4" SHALL BE HEX SOCKET.
- ANTI-SEIZE COMPOUND SHALL BE APPLIED TO ALL FRANGIBLE COUPLINGS, STAINLESS STEEL OR GALVANIZED THREADED CONNECTIONS AND BOLTS. ANTI-SEIZE COMPOUNDS THAT ARE COMPATIBLE WITH THE MATERIAL WITH WHICH IT COMES IN CONTACT WITH SHALL BE USED.
- THE FIXTURE MOUNTING BOLTS SHALL BE EXTENDED THROUGH THE BASE CAN MOUNTING FLANGE INTO THE BASE CAN A MINIMUM OF 1/2" AND A MAXIMUM OF 1 1/2". THE BOLTS SHALL HAVE ENOUGH THREAD LENGTH SO THEY DO NOT SHOULDER OUT BEFORE THE FIXTURE IS SECURELY TIGHTENED.
- NEW ISOLATION TRANSFORMERS AND PRIMARY CONNECTOR KITS SHALL BE INSTALLED WITH EACH INSTALLED FIXTURE. SEPARATE PAYMENT FOR CONNECTOR KITS WILL NOT BE MADE, THE INSTALLATION OF CONNECTOR KITS SHALL BE INCIDENTAL TO INSTALLATION OF CABLE.
- CONTRACTOR SHALL NOT INSTALL NEW CABLES IN EXISTING CONDUITS WHERE CIRCUITS ARE TO REMAIN. CONTRACTOR SHALL REMOVE EXISTING CABLE AND PULL EXISTING AND NEW CIRCUITS THROUGH THE CONDUIT AT THE SAME TIME. CABLE ROUTING WITHIN A DUCTBANK WAS NOT FIELD VERIFIED AND ON-GOING PROJECTS HAVE MODIFIED CIRCUITS, THEREFORE THE CONTRACTOR SHALL FIELD VERIFY CIRCUIT ROUTING PRIOR TO REMOVAL AND INSTALLATION OF CABLE. THE CONTRACTOR MAY BE REQUIRED TO REPLACE ADDITIONAL CABLE AS DIRECTED BY THE RPR.
- WHERE NEW CABLES ARE SHOWN TO BE IN EXISTING DUCT, CIRCUITS TO BE REPLACED OR TAKEN OUT OF COMMISSION SHALL BE REMOVED AND DISPOSED OF. WHERE MULTIPLE CIRCUITS ARE SHOWN TO BE REPLACED IN A DUCT/DUCTBANK, IT IS PRESUMED ALL CABLES ARE IN A SINGLE DUCT. IF CABLES TO BE REPLACED ARE IN SEPARATE DUCTS WITH OTHER CABLES TO REMAIN, VERIFY REMOVAL OF THE OTHER CIRCUITS WITH THE RPR BEFORE PROGRESSING WITH CABLE REPLACEMENT. IF EMPTY DUCTS EXIST ALONG THE PROPOSED CABLE INSTALLATION ALIGNMENT, THE CONTRACTOR SHALL INSTALL CABLES IN SPARE DUCTS.
- THE CONTRACTOR SHALL MAINTAIN THE EXISTING COUNTERPOISE SYSTEM AND EXTEND IT WHERE NEW LIGHT BASES ARE TO BE INSTALLED WHERE POSSIBLE. THE RPR SHALL HAVE FINAL JUDGEMENT ON WHETHER THE COUNTERPOISE IN ANY LOCATION CAN BE PRESERVED/CONNECTED TO OR IF NEW COUNTERPOISE MUST BE INSTALLED.
- THE CONTRACTOR PERFORMING SPLICING OF 5KV CABLE (AIRFIELD CABLE) SHALL BE EXPERIENCED IN THE FIELD OF AIRFIELD ELECTRICAL CONSTRUCTION AND DEMOLITION, WITH A MINIMUM OF 3 YEARS EXPERIENCE AND TRAINING IN AIRFIELD CABLE SPLICING.
- THE ELECTRICAL CONTRACTOR SHALL COORDINATE WITH THE CIVIL CONTRACTOR.
- SOME AIRFIELD LIGHTING CIRCUITS PASS THROUGH OR NEAR THE CONSTRUCTION AREA AND WILL REMAIN ENERGIZED DURING CONSTRUCTION. THE CONTRACTOR SHALL RETAIN THESE CIRCUITS IN THE VICINITY OF THE CONSTRUCTION AREA AND APPROPRIATELY MARK/IDENTIFY THEM FOR ALL CONSTRUCTION STAFF (E.G. RUNWAY GUARD LIGHT AND SIGNAGE CIRCUITS).
- ALL JUMPERING SHOWN ON THE ELECTRICAL PHASING DRAWINGS IS SCHEMATIC IN NATURE AND INTENDED TO GENERALLY SHOW THE ROUTE BELIEVED TO BE REQUIRED TO MAINTAIN POWER TO LIGHTS IN AREAS NOT AFFECTED BY CONSTRUCTION. THE CONTRACTOR SHALL VERIFY THE EXISTING CIRCUIT ROUTING PRIOR TO MAKING ANY TEMPORARY CONNECTIONS FOR ALL JUMPERING ACTIVITIES AND DOCUMENT CIRCUIT ROUTING FINDINGS WITH THE RPR.
- IT IS POSSIBLE THAT JUMPERING MAY BE REQUIRED OF CIRCUITS NOT SHOWN ON THE ELECTRICAL PHASING DRAWINGS DUE TO UNKNOWN FIELD CONDITIONS. THE RPR SHALL HAVE THE AUTHORITY TO DIRECT THE CONTRACTOR TO INSTALL TEMPORARY CABLE IN ORDER TO MAINTAIN POWER TO ANY AREA DEEMED NECESSARY.
- ALL DEMOLITION AND NEW CONSTRUCTION MUST BE COMPLETED AND FIELD ACCEPTED PRIOR TO CLOSURE OF A PHASE DURATION.
- VARIOUS FAA CABLES PASS THROUGH THE WORK AREA. THESE CABLES SERVE FAA FACILITIES (NAVAIDS) WHICH SHALL REMAIN OPERATIONAL AND ARE CRITICAL FOR SAFE AIRPORT OPERATION. IT IS EXTREMELY IMPORTANT THAT DAMAGE TO THESE CABLES BE PREVENTED. CABLE LOCATIONS SHOWN ON THE DRAWINGS ARE BASED ON THE BEST INFORMATION AVAILABLE, BUT CANNOT BE COUNTED AS PRECISE. THE CONTRACTOR SHALL NOTIFY THE FAA TO REQUEST THAT ANY CABLES OR UTILITIES BE STAKED IN THE FIELD. AFTER FAA STAKES THE CABLES, THE CONTRACTOR SHALL USE HAND EXCAVATION TO LOCATE CABLES AND UTILITIES AS NEEDED.
- AFTER CABLES AND UTILITIES ARE LOCATED, CONTRACTOR SHALL RECORD LOCATIONS AND CABLE PROPERTIES (CONTENTS/SERVICE) AND SHALL ACCURATELY MEASURE AND RECORD THE INFORMATION ON THE AS-BUILT DRAWINGS. CONTRACTOR SHALL USE EXTREME CAUTION TO AVOID DAMAGING FAA CABLES AND UTILITIES, AS ANY DAMAGE MAY NECESSITATE REPLACEMENT OF THE CABLE RUN AS A WHOLE AT FAA'S DISCRETION.

UTILITIES AND NAVAIDS

- THE CONTRACTOR IS HEREBY INFORMED THAT THERE ARE INSTALLED ON THE AIRPORT FAA NAVAIDS, NATIONAL WEATHER SERVICE FACILITIES, AIRFIELD LIGHTING SYSTEMS, ELECTRIC CABLES AND CONTROLS RELATING TO SUCH NAVAID AND FACILITIES. SUCH NAVAIDS, NATIONAL WEATHER SERVICE AND OTHER FACILITIES, AND ELECTRIC CABLES MUST BE FULLY PROTECTED DURING THE ENTIRE CONSTRUCTION TIME. UTILITY LINES SHOWN ON THE PLANS ARE FROM AVAILABLE UTILITY RECORDS. UNDER THIS CONTRACT CAN BE ACCOMPLISHED IN THE VICINITY OF THESE FACILITIES AND CABLES ONLY AT APPROVED PERIODS OF TIME.
 - UTILITIES: IT WILL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR TO LOCATE AND PROTECT ANY PUBLIC UTILITIES THAT ARE IN OR ADJACENT TO THE WORK AREA. THE UTILITIES WILL BE FLAGGED ONE TIME BY THE VARIOUS UTILITY COMPANIES. THESE FLAGS SHALL BE PROTECTED AND MAINTAINED BY THE CONTRACTOR AT ALL TIMES. IF FLAGS ARE LOST OR REMOVED BY THE CONTRACTOR, THEY WILL BE FLAGGED AGAIN AT THE CONTRACTOR'S EXPENSE. ALL UTILITIES SHALL BE PROTECTED AND DAMAGES REPAIRED EXPEDITIOUSLY, AT THE CONTRACTOR'S EXPENSE.
 - PROTECTION AND REPAIR OF DAMAGE TO EXISTING CABLES: ALL UNDERGROUND CABLE SHALL BE PROTECTED AND DAMAGES REPAIRED EXPEDITIOUSLY AT THE CONTRACTOR'S EXPENSE.
 - POWER AND CONTROL CABLES LEADING TO AND FROM ANY FAA NAVAIDS, NATIONAL WEATHER SERVICE AND OTHER FACILITIES WILL BE MARKED IN THE FIELD BY THE LOCAL FAA AIRWAY FACILITIES SECTOR PERSONNEL BEFORE ANY WORK IN THEIR GENERAL VICINITY IS STARTED BY THE CONTRACTOR. THEREAFTER, THROUGH THE ENTIRE TIME OF THIS CONSTRUCTION, THE CONTRACTOR SHALL NOT ALLOW ANY CONSTRUCTION EQUIPMENT TO CROSS THESE CABLES WITHOUT FIRST PROTECTING THE CABLE WITH STEEL PLATE, OR SIMILAR STRUCTURAL DEVICES, ON THREE (3) EITHER SIDE OF THE MARKED CABLE ROUTE. ALL EXCAVATION WITHIN THREE (3) FEET OF EXISTING CABLES SHALL BE ACCOMPLISHED BY NON-INTRUSIVE EXCAVATION ONLY.
 - THE CONTRACTOR SHALL PROTECT FAA NAVAID, NATIONAL WEATHER SERVICE AND OTHER FACILITIES AND CABLES AT ALL TIMES. ANY UNDERGROUND UTILITIES DISCOVERED DURING CONSTRUCTION NOT SHOWN ON THE PLANS SHALL BE REPORTED TO THE HAS PROJECT MANAGER AND IAH OPERATIONS IMMEDIATELY.
 - THE CONTRACTOR SHALL IMMEDIATELY REPAIR AT THEIR OWN EXPENSE, WITH IDENTICAL MATERIAL BY SKILLED EMPLOYEES, ANY UNDERGROUND CABLES SERVING FAA NAVAIDS, NATIONAL WEATHER SERVICE AND OTHER AIRPORT FACILITIES, WHICH ARE DAMAGED BY THEIR EMPLOYEES, EQUIPMENT, OR CONSTRUCTION ACTIVITIES. PRIOR APPROVAL OF THE FAA MUST BE OBTAINED FOR THE MATERIALS, EMPLOYEES, TIME OF DAY OR NIGHT, METHOD OF REPAIRS, AND FOR ANY TEMPORARY OR PERMANENT REPAIRS THE CONTRACTOR PROPOSES TO MAKE TO ANY FAA NAVAID OR FACILITIES DAMAGED BY THE CONTRACTOR. PRIOR APPROVAL OF THE ENGINEER MUST BE OBTAINED FOR THE MATERIALS, EMPLOYEES, TIME OF DAY OR NIGHT, AND FOR THE METHOD OF REPAIRS FOR ANY TEMPORARY OR PERMANENT REPAIRS THE CONTRACTOR PROPOSES TO MAKE TO ANY OTHER AIRPORT FACILITIES AND CABLES DAMAGED BY THE CONTRACTOR. SHOULD THE REPAIR REQUIRE SPLICING, IT SHALL BE SPLICED AT THE DISCRETION OF THE LOCAL FAA AIRWAY FACILITIES SECTOR MANAGER AS TO WHO SHALL PERFORM THE WORK. WHERE THE FAA PERFORMS THE WORK, IT SHALL BE AT THE CONTRACTOR'S EXPENSE. NO WORK SHALL BE BACKFILLED OR COVERED PRIOR TO APPROVAL BY THE AIRWAY FACILITIES SECTOR MANAGER AND/OR HAS PROJECT MANAGER FOR IAH OWNED FACILITIES.
 - THE CONTRACTOR SHALL TAKE ALL STEPS TO PROTECT THE EXISTING RUNWAY AND TAXIWAY LIGHTS, UNDERGROUND CABLES AS WELL AS ALL COMMERCIAL AND AIRPORT UTILITIES DURING CONSTRUCTION IN ORDER TO ENSURE CONTINUOUS OPERATION OF LIGHTS AND NAVIGATIONAL AIDS WHEN NEEDED.
 - A MINIMUM OF 48 HOURS IN ADVANCE OF ANY EXCAVATION OR BORINGS, THE CONTRACTOR SHALL CONTACT TEXAS ONE CALL (811) AND HAS IT (281-233-1900) TO VERIFY UNDERGROUND CABLE LOCATIONS IN THE VICINITY OF THE PROPOSED WORK.
- | CABLE OWNER | CONTACT PERSON | PHONE NUMBER |
|--|-------------------------------|----------------------------------|
| FEDERAL AVIATION ADMINISTRATION | ALAN NUNEZ | (281) 784-7641 |
| HOUSTON AIRPORT SYSTEM | BILL ZRIOKA | (281) 233-1364 |
| HAS ELECTRICAL AND MAINTENANCE TECHNOLOGY INFRASTRUCTURE | JASON STEEN
BILL MATULONIS | (281) 233-1900
(281) 233-1798 |
- THE CONTRACTOR SHALL NOTIFY AIRPORT OPERATIONS SO THAT THEY MAY ADVISE AIRPORT RESCUE AND FIRE FIGHTING (ARFF) PERSONNEL SEVENTY-TWO (72) HOURS IN ADVANCE OF WATERLINES OR FIRE HYDRANTS THAT MUST BE DEACTIVATED AND/OR IF EMERGENCY ACCESS ROUTES MUST BE TEMPORARILY REROUTED OR BLOCKED.
 - THE CONTRACTOR SHALL CONTACT THE FAA TECHNICAL OPERATIONS, TO PROVIDE FIELD LOCATIONS OF EXISTING FACILITY CABLES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR NON-INTRUSIVE EXCAVATION TO LOCATE FAA CABLING, AND PROTECTION OF THOSE CABLES THROUGHOUT THE PROJECT. WHEN PERFORMING WORK AROUND FAA UTILITIES, DUCTBANKS, ETC., THE CONTRACTOR SHALL PROVIDE THE FAA A DETAILED WORKPLAN OUTLINING THE PROPOSED PROTECTION MEASURES. THE WORK PLAN MUST BE SENT A MINIMUM OF 45 DAYS PRIOR TO THE WORK AND SHALL BE APPROVED BY THE FAA.
 - INSPECTIONS BY IAH AIRPORT OPERATIONS - PRIOR TO REOPENING FOR AIRCRAFT USE AND THE DEPARTURE OF THE CONTRACTOR'S WORK CREWS, THE OWNER'S AUTHORIZED REPRESENTATIVE WILL ARRANGE FOR A DAILY INSPECTION BY IAH AIRPORT OPERATIONS OF ANY RUNWAY/TAXIWAY/TAXILANE SAFETY OR OBJECT FREE AREA, OR APRON THAT HAS BEEN CLOSED FOR WORK, OR THAT HAS BEEN USED FOR A CROSSING POINT OR HAUL ROUTE BY THE CONTRACTOR. THESE AREAS MUST COMPLY WITH THE SAFETY REQUIREMENTS, DEFINED BY FEDERAL AVIATION REGULATIONS PART 139, AND INTERPRETED BY THE DESIGNATED OPERATION'S INSPECTOR. BEFORE PERMISSION FOR THE CONTRACTOR'S WORK CREWS TO DEPART WILL BE GRANTED. CONTRACTOR MUST REMAIN ON SITE UNTIL IAH AIRPORT OPERATIONS COMPLETES AN AIRFIELD LIGHTING INSPECTION. ELECTRICAL VAULTS MUST BE RETURNED TO SERVICE NO LESS THAN 1.5 HOURS BEFORE SUNSET. CONTRACTOR CANNOT LEAVE SITE UNTIL AN AIRFIELD SAFETY AND LIGHT CHECK IS COMPLETED WITH IAH AIRPORT OPERATIONS.
 - THE CONTRACTOR SHALL PROTECT EXISTING IT INFRASTRUCTURE, INCLUDING HAS FIBERS, DUCTBANKS, FRAME AND COVERS, AND HANDHOLES AT ALL TIMES. IN THE EVENT OF ANY DAMAGES AND/OR REQUIRED RELOCATION OF THE EXISTING IT INFRASTRUCTURE, THE CONTRACTOR SHALL COMPLY WITH HAS IT STANDARDS AND SPECIFICATIONS ON THE FLY2HOUSTON WEBSITE.



HOUSTON AIRPORT SYSTEM

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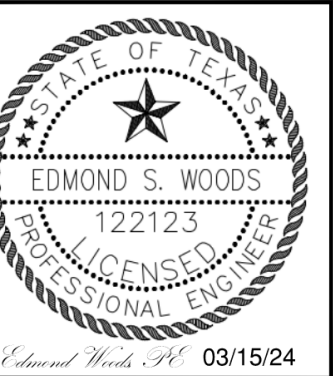
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Houston, TX 77024
(281) 721-3400
www.jacobs.com 1BPE Firm #2968

REVISIONS		
NO.	DESCRIPTION	DATE
0	ISSUE FOR CONSTRUCTION	03/15/24

HOUSTON AIRPORT SYSTEM
PROJECT 952 SOUTH LIGHTING VAULT RENOVATION
GEORGE BUSH INTERCONTINENTAL AIRPORT / HOUSTON
4104 WILL CLAYTON PARKWAY, HOUSTON, TX 77032
CONSTRUCTION SAFETY PHASING PLAN
CONSTRUCTION PHASING NOTES II

PROJECT MGR: AEO
DESIGNER: JM
DRAWN BY: JW
CHECK BY: EW

DATE: 03/15/24



APPROVED BY:

DIRECTOR
HOUSTON AIRPORT SYSTEM

JACOBS NO.

A.I.P. NO.

C.I.P. NO.


H.A.S. NO.
PN952

SHEET NO.

GC - 002

Appendix F. Houston Airport System, Tenant Violations – Offenses, Charging Instrument, Due Process Provisions

Operating Instruction

<p>TITLE</p> <p>Tenant Violations - Offenses, Charging Instrument, Due Process Provisions</p>	<p>OI Category No. 05-03</p>
<p>Authorized Signature:  Mario C. Diaz, Director of Aviation</p>	<p>Date Original Approved: August 01, 2005</p> <p>Revised Date: February 1, 2016</p>

I. PURPOSE

- A. To explain the rationale of the system of enforcing compliance with various laws, ordinances, policies, procedures, rules and regulations of or pertaining to the Houston Airport System (HAS), including, but not limited to, the Airport Security Plan (ASP), Organization Policy & Procedures (OPPS), Airport Operating Procedures (AOP) and Airport Security Manual and/or Operating Instructions (O.I).
- B. To identify the persons authorized to issue Notice(s) of Violation(s), which is the charging instrument in the Houston Airport System (HAS) for O.I. and other administrative violations.
- C. To identify violations, including, but not limited to the specific violations enumerated herein.
- D. To assign consequences to such violations.
- E. To provide a means of providing for due process to those charged.

II. POLICY BASIS

Title 49 Code of Federal Regulations Parts 1540 & 1542, Airport Security Plan, Title 14 Code of Federal Regulations Part 139, Chapter 9 of the City of Houston Code of Ordinances, Houston Airport System Operating Instruction 05-02

III. BACKGROUND

- A. HAS, along with various federal, state and local governmental bodies, including administrative bodies, has developed, and continues to develop, various laws, policies, procedures, rules and regulations that are deemed reasonable and necessary for the safe, efficient and secure operation of the Houston Airport System (HAS).
 - 1. At the Houston Airport System there are three enforcers of these laws, ordinances, policies, procedures, rules and regulations:
 - a. The Airport Operations and Airport Security Sections, as designated by the Airport General Manager, are the primary Sections responsible for enforcement of security, non-security, safety, and operations related laws, ordinances, policies, procedures, rules and regulations; and
 - b. Law Enforcement Officers-Law Enforcement Officers only have the authority to enforce criminal laws and not administrative or civil law nor the administrative or civil counterpart of any criminal law.
 - c. The Section Directors may designate those within their Sections who are authorized to enforce non-criminal and administrative violations, and/or the administrative or civil counterpart of any criminal law, i.e. issue Notices of Violation, by either name or by their operating title class, and any designated by operating title class shall include any and all of those becoming employed in

said operating title class after the date of designation, as well as, those in any operating title class that is named as a successor to a prior operating title class.

- d. The Airport General Manager, as he/she deems necessary, may designate any other section with the responsibility of the enforcement of security, non-security, safety, and operations related laws, ordinances, policies, procedures, rules and regulations.
2. The means of notifying an individual or an organization that they have violated an administrative or civil law, policy, procedure, rule and/or regulation is by means of issuing a charging instrument called an NOV or Notice of Violation. This form may be used for either issuing a warning or for formal charging. (See Attachment #1)
3. This O.I. provides periods of time and a procedure in which to file a contest and a procedure for a fair and impartial hearing.
4. Besides a general offense of violating a law, ordinance, policy, procedure, rule or regulation, specific offenses are listed herein so as to provide the HAS Community a clearer view of some of the areas that the Community needs to concentrate in resolving for the safety, security and efficiency of the HAS airports.
5. This O.I provides a procedure to follow the NOV from issuance through completion of the consequences and to allow for better tracking of the NOV history of each individual and sponsoring organization's sponsored individuals.
6. The title to any HAS position stated in this O.I. or any other O.I. related to ID Badging may be changed by posting the change in the public area at the main ID Badging Office at any HAS Airport affected thereby.

IV. POLICY APPLICABILITY

- A. This (O.I) applies to any person or entity who uses, desires to use, or should be using, the HAS Airport Security Section ID Badging system and/or Access Control System at any one or more of the HAS airports or facilities, employees of HAS who are governed by other HAS and City Of Houston ordinances, rules and regulations.
- B. This (O.I), by this paragraph, does hereby extend and apply O.I. 05-02 (Security Access Control System & ID Badging) to any person or entity who uses, desires to use, or should be using, the HAS Airport Security Section ID Badging system and/or Access Control System at any one or more of the HAS airports or facilities, employees of HAS who are governed by other HAS and City Of Houston ordinances, rules and regulations.
- C. This O.I. and O.I. 05.02 as applicable per airport, also applies to airline crews, airline mechanics and others who are allowed access to restricted or controlled areas based upon identification or access media issued by their employers (included in the definition of "HAS authorized ID Badges") and who are not otherwise required to possess an HAS issued ID Badge shall be required to submit to and undergo the same sanctions as if they had an HAS issued ID Badge and failure to submit to and undergo such sanctions for violations shall make them a trespasser after warning and subject to being so charged under the Penal Code of the State of Texas.
- D. This O.I. and O.I. 05.02 as applicable per airport, also applies to any individual who's employed at HAS owned, leased or controlled property for the purpose of providing any work, goods or services to HAS or any of its contractors, subcontractors, lessees, concessionaires, etc. and in so providing said work, goods

or services enters into any controlled or restricted area of such HAS owned, leased or controlled is required to obtain an HAS issued ID Badge. Individuals employed at all locations requiring a HAS ID badge shall be required to submit a completed HAS badge application to a HAS Badging Office prior to being authorized to work in a controlled or restricted area. These individuals may only be escorted after submitting a completed badge application to HAS Badging Office.

Individuals denied an HAS ID badge due to disqualifying criminal crimes and conditions prohibiting such issuance (O. I. 05-02 Attachment A) or have their HAS ID badge privileges suspended may not be employed or provide goods or services at any HAS owned, leased or controlled property including public areas. The individuals described in this subparagraph shall make them a trespasser after warning and subject to being so charged under the Penal Code of the State of Texas.

V. RESPONSIBILITY

- A. It is the responsibility of each individual and the tenant or other sponsoring entity and/or the party to whom they are providing work, goods or services, jointly and severally, to ensure that they and/or those sponsored or utilized by them are fully aware of the regulations, violations, penalties, and enforcement procedures contained in this (O.I), as well as, all other applicable laws, ordinances, rules and regulations, **before starting work and/or providing work, goods or services at an HAS airport - lack of knowledge or understanding is not an excuse nor a defense.** It shall be the responsibility of all above mentioned are kept advised of all changes and revisions to the regulations, violations, penalties, and enforcement procedures contained in this (O.I) and other applicable laws, ordinances, rules and regulations.
- B. It is the responsibility, duty and obligation of each and every individual to whom an ID Badge is issued, to know the limitations of their access, the laws, ordinances, policies, procedures, rules and regulations governing access, safety and security at the HAS airports and to timely and faithfully carryout their duties and obligations to the same, including, but not limited to, obeying each and every law, policy, procedure, rule and regulation and to strictly avoid any violation(s) of the same and by acceptance and/or retention of an ID Badge they represent that they have complied with these responsibilities, duties and obligations.

VI. DEFINITIONS

- A. Wherever an “Attachment”, “Form” or “Map” is referred to anywhere in this O.I., they are provided for your convenience only as a general assistance and do not constitute a legal description. The current “Attachment”, “Form” or “Map” can be obtained from the HAS Airport Security Section.
- B. The use of bolding, italics, underlining or other means of emphasizing a word or words is merely an aid to bring that portion to the reader’s attention and does not denigrate the status or importance of any other word, words, sentences or paragraphs nor reduce their being mandatory in nature where appropriate.
- C. Word and Phrase Definitions:
1. Airport Identification Badge (ID Badge): Any single identification media or combined identification media and access control media which is, and does always remain, the property of the Houston Airport System and is issued by HAS to provide identification, authorization, and access to restricted and/or controlled airport areas and other HAS facilities. Anytime the term “ID Badge” is used, it is referring to an ID Badge issued by HAS, Airport Security ID Badging, unless otherwise specifically noted. The term shall also include any medallions, etc. designated and authorized by HAS to be attached to the ID Badge.
 2. Airport General Manager: Is the person designated as such by the Houston Airport System for each HAS Airport. The term “Airport General Manager” shall include the person designated by the Airport General

Manager or by the Director of Aviation as the Acting or Interim Airport General Manager, when the Airport General Manager is on leave (vacation, city business, sick leave, FMLA, etc.) or unable or unavailable to fulfill their normal duties.

3. Airport Security Coordinator (ASC): The Primary Airport Security Coordinator is the person that occupies the position as defined in 49 Code of Federal Regulations (CFR) 1542.3 and as designated in the Airport Security Plan. The “Primary Airport Security Coordinator”, as used herein, is the Airport Security Manager. The Airport Security Coordinator for ID Badging, if any, is the person who is the Head Supervisor for the ID Badging Section or any successor title thereto, i.e., it is the person that is the highest ranking individual for an ID Badging Office. The Airport Security Coordinator for the Security Section, if any, is the person holding that operational title.
4. Airport Security Manager (ASM): Is the person so designated in that position by the General Manager of the airport. The term “Airport Security Manager” shall include the person designated by the Airport Security Manager as the Acting or Interim Airport Security Manager, when the Airport Security Manager is on leave (vacation, city business, sick leave, FMLA, etc.).
5. Air Operations Area (AOA): A portion of an airport, specified in the airport security program, in which security measures specified in Part 1540 are carried out. This area includes aircraft movement areas, aircraft parking areas, loading ramps, and safety areas, for use by aircraft regulated under 49 Code of Federal Regulations (CFR) Part 1544 or 1546, and any adjacent areas (such as general aviation areas) that are not separated by adequate security systems, measures, or procedures. Portions of the AOA may concurrently have more restrictive and controlled designations. The AOA is more specifically delineated in attachments to the Airport’s ASP.
6. Airport Security Plan (ASP): An airport’s security program developed for and approved by the Transportation Security Administration (TSA) under the provisions of 49 CFR Chapter XII Part 1542.101. The Airport Security Plan is sometimes referred to as the Airport’s Security Program.
7. Airport Tenant Security Program (ATSP): The agreement, if applicable, between the airport operator and an airport tenant that specifies the measures by which the tenant will perform security functions, and approved by TSA, under Part 1542.113 of 49 CFR Chapter XII.
8. Apron Areas/Ramps: Any area at an HAS airport where aircraft operate or park without being under direct control of the Air Traffic Control Tower, excluding corporate hanger areas, Fixed Based Operator (FBO) areas, and general aviation areas. Access to apron areas is restricted for security/safety reasons as defined in TSA 49 CFR 1540 & 1542, 1544 and 1546 and/or other applicable laws, rules and regulations.
9. Authorized ID Badge: Includes not only HAS issued ID Badges, but also, the ID Badges and access media of airline crews, airline mechanics or others who are allowed access to restricted or controlled areas based upon identification or access media issued by their employers and who are not otherwise required to possess an HAS ID Badge.
10. Authorized Signatory Authority: Is a person authorized by an employer or sponsoring company, and approved by an HAS ID Badging Office, to sign forms, including but not limited to Security Clearance Requests, Badge Renewal Requests, Key Requests, and PIN Requests, for individuals employed by or being sponsored for an HAS ID Badge.
11. Badging & Access Office: The HAS Airport Security ID Badging & Access Office Section that is primarily responsible for reviewing, approving, issuing, accounting to TSA for, and/or governing authorized ID Badges, Keys, PIN’s and other access media. This Office is also responsible for Criminal History Records Check (CHRC)s, other security background checks, designation, assigning and entry of access rights, programming and deprogramming ID Badges, PIN data, etc. into the HAS access control computer system. ID Badges, PIN’s and other media are issued by the Badging & Access Office located

at each of the HAS airports. In addition to each permanent Badging & Access Office at each HAS airport, the HAS Airport Security Section may, from time to time, specially and/or temporarily designate other locations. Hours of operation may vary depending on staffing and other considerations. Other names for this Office are the ID Badging & Access Office and/or ID Badging Office and/or Badging Office and/or other combinations thereof.

12. Company Offense: An offense that is chargeable against an organization, as opposed to, or in addition to, an individual. In a company offense, the sanction shall be performed by the highest ranking officer, project manager, station manager, superintendent, division manager, substantial owner or other management person of a rank similar to the foregoing that is directly connected with the business, operation, or project of the charged organization for the Airport or HAS facility related to the business, operation or project.
13. Controlled Areas: Those areas controlled by card readers, key lock, PIN pad or other access control device. These areas include, but are not limited to, certain parking areas, certain restricted areas, secured areas, sterile areas, apron areas, SIDA areas, AOA or any other area as defined in this O.I. or other applicable law, rule, regulation, Airport Security Plan, Security Program, lease, contract, agreement or by signage as off-limits to anyone not authorized to be in that area.
14. Current Picture: A picture of the applicant which is clear enough and closes enough in appearance so that a reasonable person can readily conclude that the document with the picture and the person presenting the document with the picture as being their own picture are one and the same person.
15. Disqualifying Condition: A pattern of criminal intent or activity as evidence by arrest records or convictions, irrespective of the type of the criminal offense(s) alleged or time period as determined by the Airport Security Manager. Termination of Badging/Access Rights or similar thereto, any airport within the 10 year period prior to seeking badging at an HAS Airport. (The term "Airport "as used herein shall mean any airport in the world)
16. Escort: To accompany or monitor and physically/visually control the activities of an individual who does not have unescorted access authority into or within a TSA Regulated Area as defined in the ASP.
17. Enforcement: The Airport Security and Airport Operations Sections are responsible for enforcement of safety, security and non-security related offenses described in this Operating Instruction and associated O.I.s.
18. Faithfully: Without variance, completely, devotedly, dependably.
19. HAS Airports: All land and improvements which are owned, leased, controlled and/or operated by the City of Houston under the auspices of the Houston Airport System at, or in conjunction with and/or support of, any one or more of the City owned airports.
20. Houston Airport System (HAS): City of Houston department responsible for operation of the City owned airports.
21. I.D. Badge Application and/or Application: Includes, but is not limited to, the Security Clearance Request form and the Security Clearance Renewal Request form, and any other application, clearance or request forms promulgated and used by HAS I.D. Badging Offices for the purpose of badging or issuance of access media.
22. Immediately or Immediate or Timely: Without delay, right there and then, time is of the essence, something to be accomplished before proceeding with other tasks, either business or personal.

23. Immediate Temporary Suspension: An individual's badging rights are immediately suspended as a result of behavior and/or action(s) that creates an unreasonable risk which may diminish the reputation, or the safety and/or security of the HAS community.
24. Key Set Symbol: The code number stamped on a controlled key used for identification purposes.
25. Media: Includes ID Badges, keys, PIN Numbers and other equipment and devices for identification and/or access.
26. Notice of Violation: Written notice on a form, or in a format, designated by HAS Airport Security, officially charging an individual or entity with a violation of Security, or non-security, laws, ordinances, policies, procedures, rules or regulations or any other laws, ordinances, policies, procedures, rules or regulations, including, but not limited to the Airport Security Plan, Organization Policy & Procedures (OPPS), Airport Operating Procedures (A.O.P.), Airport Security Plan and/or Operating Instructions (O.I.).
27. PIN Code: Personal Identification Number, normally identifiable to just one person, but which may, under some circumstances, be issued to more than one person, but often identifiable to a specific group of similarly engaged persons, needing access through a PIN controlled portal and normally used for access to Controlled Areas not covered by conventional locks or card readers.
28. Portal: Any opening through which an individual or object can enter into a place or exit from a place; a portal may be controlled or uncontrolled; a portal includes, but is not limited to, doors, windows, baggage conveyor doors, doorways, cutouts in walls and floors, gates, openings in fences, docks, driveways into and out of a building or area, conveyor openings, construction openings, etc.
29. Remedial Training: Such training as may be required by HAS of any individual or entity receiving a Notice of Violation.
30. Restricted Area: Areas not otherwise classified and which requires a grant of permission to enter and remain in from either HAS or a person or entity having greater rights of possession and control of the area than the person seeking to enter or remain therein.
31. Secured Area: A portion of an airport, specified in the Airport Security Plan, in which certain security measures specified in Part 1542 of 49 CFR Chapter XII are carried out. This area is where aircraft operators and foreign air carriers that have a security program under Part 1544 or 1546 of 49 CFR Chapter XII enplane and deplane passengers and sort and load baggage and any adjacent areas that are not separated by adequate security measures. The Secured Area is more specifically delineated in attachments to the Airport's ASP.
32. Security Identification Display Area (SIDA): A portion of an airport, specified in the airport security program, in which security measures specified in Part 1542 are carried out. *The Secured Area is always a SIDA but a SIDA is not always a Secured Area; other areas of an airport besides the Secured Area may be defined as SIDA.* The Secured Identification Display Area is more specifically delineated in attachments to the Airport's ASP.
33. SIDA Video: Security training media, including, but not limited to, film, videotapes, web-pages, CDs, and DVDs, required by TSA 49 CFR 1542 to be viewed by all of those who are granted access to the (SIDA), and required by HAS for all badged individuals, whether receiving SIDA access or not, prior to exercising the privileges of their Airport ID Badges, keys, PIN's or other access media or IDs.
34. Sponsor and/or Sponsoring Organization: This term, jointly and severally, includes the badging applicant's employer (including an Aircraft Operator subject to 49 CFR Part 1544 and governmental agencies normally exempt under 49 CFR 1542.209(m)(1)) as well as any other person, including the

entity that such other person is employed by, who is authorized by HAS to and does execute an HAS Badge Application in the signature block designated for the Sponsoring Company information and signature. Commonly referred to as “employer”

35. Station Manager: This term means the person that is the individual that is the top person in charge of the day to day overall operations of a company or organization at an HAS airport on the date of the issuance of an NOV. It is an affirmative defense that an individual receiving an NOV is not the individual meeting the foregoing definition, however, any individual claiming this affirmative defense must provide a sworn statement naming the correct individual in his/her company or organization that meets this definition.
 36. Sterile Area: A portion of an airport defined in the airport security program that provides passengers access to boarding aircraft and to which the access generally is controlled by TSA, or by an aircraft operator under Part 1544 of 49 CFR Chapter XII or a foreign air carrier under Part 1546 of 49 CFR Chapter XII, through the screening of persons and property.
 37. Transportation Security Administration (TSA): Division of the U.S. Department of Homeland Security responsible for administering Airport and other transportation venue Security Programs and/or its successor(s), if any, to one or more of its functions.
 38. Under-Badged: An individual is under-badged for an area if the access rights granted by either the type of HAS I.D. Badge they have been issued or the access rights granted to that individual by HAS I.D. Badging is not of the level to permit the individual to be in or remain in an area without an escort.
 39. Violation: The failure to, in whole or in part, strictly perform in a faithful and timely manner any duty or obligation, whether or not the duty or obligation is to do or not to do a general or specific matter imposed upon an HAS ID Badge holder by any federal, state or local, including Houston Airport System, law, ordinance, policy, procedure, rule or regulation.
 40. Inchoate Offense: A type of crime completed by taking a punishable step towards the commission of another crime. The basic inchoate offenses are attempt, solicitation, and conspiracy.
- D. All other words and phrases, not specifically defined in this section or otherwise, in this O.I., shall be defined as is commonly used and understood at HAS airports by HAS Management -- the interpretation of the HAS Director of Aviation, in his/her sole discretion, shall be final.

VII. VIOLATIONS / OFFENSES / CONSEQUENCES

- A. The violation, a/k/a, offense, whether by act or omission, of any Federal, State or Local, law, ordinance, policy, procedure, rule or regulation or any part thereof, whether such violation is due to intentional, knowingly, reckless or negligent conduct or a combination thereof is an offense and may result in a consequence. All offenses covered by this O.I. are strict liability offenses, meaning that a certain state of mind, *mens reas*, is not an element of the offense, unless otherwise specifically stated. Each I.D. Badge holder is hereby personally charged with the duty and obligation to know all laws, ordinances, policies, procedures, rules and regulations concerning safety, conduct, and/or security at an HAS airport or other HAS controlled facility. Any offense that is not specifically listed below shall be a violation of this OI and shall bear the consequences set forth herein.
- B. Should any offense as committed, whether general or specific, cause or have the reasonable possibility of placing another person in danger of imminent bodily injury or death, or should the offense cause or have the reasonable possibility of placing property in danger of imminent damage in an amount greater than \$ 5,000.00, or should the offense or violation result in a TSA or FAA investigation being opened

and/or sanction imposed against HAS, or similar to a violation that resulted in a TSA or FAA investigation being opened and/or sanction being imposed against HAS within the immediately preceding three hundred sixty-five (365) calendar day period, then the offense may be enhanced by one (1) degree.

- C. An individual committing or attempting to engage in an inchoate offense, including, but not limited to, conspiracy, aiding and abetting (either before or after the substantive offense), misprision (failure to report a violation of which the individual has reasonable knowledge to believe has occurred), shall be considered the same as if they had committed the offense and shall bear the consequences set forth herein.
- D. A violator/offender is subject to the following sanctions, these sanctions are not exclusive, but are cumulative to other sanctions that may be imposed by other laws, ordinances, policies, procedures, rules and regulations -- the sanctions herein are mandatory and not subject to compromise, plea bargain, or reduction by a Hearing Panel/Hearing Officer or court:
- E. Time calculations-Violations remain on an individual's record for seven hundred thirty (730) consecutive calendar days. The days will be calculated from the date of the violation.

1. Warning Notice: Can be given in the sole discretion of the person issuing the NOV. Two warnings in a three hundred sixty-five (365) consecutive calendar day period will result in the issuance of an NOV. For the NOV to be a Warning, the Issuer must, at the time of issuance, precede the Violation Details with "WARNING ONLY".

2. Class I-1st Offense: The violator's employer shall be responsible to retrain and educate the violator of the policies, procedures and regulations to prevent future violations.

Class I-2nd Offense: Sanctions for a second Class I violation sustained or uncontested NOVs excluding Warning NOVs are that the violator and the violator's direct line supervisor must attend the viewings at the same time and must also pass the tests that the Airport Security Manager and/or the Airport Manager (ASM if security related and AM if non-security related) of that airport has determined is appropriate for the specific offense, unless it is a safety violation related to driving on the AOA, in which case the violator must watch and pass the tests on both the SIDA video and the Driving on the AOA video and/or such other prerequisites for driving on the AOA as may then be currently in force and effect. There will be a \$25.00 administrative fee for the testing of each NOV. The fee may be paid by either the Company or the individual (billed through rates and charges or directly at the badging office) who received the NOV. The fee is required to be made prior to the test being administered.

Class I-3rd Offense: Sustained or uncontested NOVs excluding Warning NOVs will result in a permanent loss of HAS ID Badge and Access Rights.

3. Class II Offenses: May result in Immediate Temporary Suspension. Sustained or uncontested violations shall result in permanent loss of ID Badge and Access Rights at all HAS airports.

4. Company Offense: An offense that is chargeable against an organization, as opposed to, or in addition to, an individual. In a company offense, the sanction shall be performed

by the highest ranking officer, project manager, station manager, superintendent, division manager, substantial owner or other management person of a rank similar to the foregoing that is directly connected with the business, operation, or project of the charged organization for the Airport or HAS facility related to the business, operation or project. The sponsoring organization must prepare and submit a plan, acceptable to the Airport Security Manager, in the event the offense is related to security, or acceptable to the Airport Manager, in the event the offense is related to other than security, at the airport where the NOV was issued, for preventing the violator and all other employees and/or sponsored individuals from violating the specific law, ordinance, policy, procedure, rule or regulation in the future. Such acceptable plan must be presented not later than the fourteenth (14th) calendar day from the date of the last day to file a Notice of Contest or from the date of the rendering of a decision by a Hearing Panel/Hearing Officer, whichever is later.

- F. For a 3rd violation of the same rule within 365 calendar days, by an Employer/Sponsoring Organization the head of security and/or safety for the Employer/Sponsoring Organization, and if no person is normally designated as such by the Employer/Sponsoring Organization, then an officer, project manager, station manager, superintendent, division manager, substantial owner or other management person of a rank similar to the foregoing of the Employer/Sponsoring Organization must prepare and submit a plan, acceptable to the Airport General Manager, or his/her designee, of the airport where the NOV was issued, for preventing the violator and all other employees and/or sponsored individuals from violating the specific law, ordinance, policy, procedure, rule or regulation in the future. Such acceptable plan must be presented not later than the fourteenth (14th) calendar day from the date of the last day to file a Notice of Contest or from the date of the rendering of a decision by a Hearing Panel/Hearing Officer, whichever is later.
- G. Sanctions time calculations:
1. The violation sanctions for each violation will remain on an individual's record for seven hundred thirty (730) consecutive calendar days from the date of the violation.
 2. The accrual of three Class I violations, sustained or uncontested NOVs excluding Warning NOVs by any employee during a seven hundred thirty (730) consecutive day period will result in immediate suspension of and permanent loss of their HAS ID Badge and Access Rights.
 3. The accrual of one or more Class II violation, sustained or uncontested NOV by any employee will result in immediate suspension of and permanent loss of their HAS ID Badge and Access Rights.
- H. Offenses of Specific Violations:
1. Offenses of Specific Violations are attached hereto as "Attachment #2" and incorporated herein by reference, the same as if recited verbatim herein.
 2. Other O.I.s can add additional Specific Violations without the need to amend this O.I.
 3. The violation of any law, ordinance, policy, procedure, rule or regulation that is not specifically set forth in this O.I. or any other O.I. as to the sanction level is treated as a violation in accordance with the structure set forth in section VII.D above, however, if the violation results in an injury to or puts an individual in imminent danger of bodily injury, then such offense can be enhanced one (1) offense level and accrue the appropriate sanction points and monetary penalties.

VIII. THE CHARGING INSTRUMENT

- A. As hereinabove stated, the administrative charging instrument for violations of this O.I. will be a Notice of Violation (NOV) by a person authorized pursuant to this O.I. to issue NOVs. The NOV will be on the form designated by the HAS Airport Security Section ID Badging Office. A copy of the current form is attached hereto as “Attachment # 1”. The NOV form attached is for illustration purposes only.
- B. The following are the only required information on the NOV, the lack of which would render the instrument invalid:
 1. The date of the offense;
 2. The approximate time of the offense;
 3. The name and HAS ID Badge number of the issuing party;
 4. The signature of the issuing party; and
 5. A general description of the offense.
- C. Any other information requested on the form or placed upon the form is for the sole benefit of HAS and the presence or lack of presence thereof will not render the NOV invalid nor void nor voidable.
- D. NOVs shall bear a box for checking off the three (3) letter designation of the airport where the NOV is issued and shall bear a number that is specific to that particular NOV.
- E. An NOV determined to be invalid may be cured by issuance of a new and correct NOV; however, all dates for requesting hearings, etc. or compliance with sanctions shall then run from the date of issuance of the new and correct NOV.
- F. The issuer of the Notice of Violation (NOV) after properly and fully completing the NOV will:
 1. At time of issuance, provide the pink copy to the violator;
 2. Place the manila cardstock copy in their Section’s records in the manner directed by their Section management not later than the end of the issuer’s shift the same day as the date of issue -- each Section authorized to issue NOV’s will develop a filing system within their Section for the retention and rapid recall of NOV’s issued by those in that Section;
 3. Deliver the white and green copies to the HAS Airport Security Badging Office at the airport where the NOV was issued;
 4. The Badging Office will retain the white copy in their Centralized NOV file and will send the green copy to the offender’s employer or sponsor’s Authorized Signatory Authority;
 5. The Badging Office will, within 48 hours of receipt (or the next business day if the Badging Office is closed for more than 48 hours), enter the violation into the Access Control Computer System and/or any alternative computer system that may be developed and designated by the Airport Security Manager or his/her designee; and
 6. The individual issuing the NOV shall retain the yellow copy in their personal file for use at any hearing.

7. The person investigating the alleged violation and/or issuing the Notice of Violation will, at a minimum, contact the violator's Direct Line Supervisor and advise him/her of the alleged violation and subsequent investigation.
- G. Failure to perform any of the instructions contained immediately above, set forth in Section VIII.F, will not invalidate the charging instrument nor serve as the basis for the dismissal of the charged violation.
 - H. In the event that the violator leaves the scene prior to completion of the NOV or refuses to sign and/or take delivery of the NOV, good and sufficient notice and service is complete upon delivery of the green copy to the offender's employer or sponsor's Authorized Signatory Authority along with a note that service was not possible on the violator at the time of issuance -- no explanation for the lack of service at time of issuance is required; however, any time limits start to run on the day of delivery to said employer or sponsor's Authorized Signatory Authority and any person listed with the ID Badging Office as an Authorized Signatory Authority may be served.
 - I. If an offense is not witnessed by an authorized issuer of NOVs, an authorized issuer of NOVs may issue an NOV based upon the written statement of a direct witness or based upon a review of such documentation, including, but not limited to, video replays (including digital), photographs (including digital) and access control records or such other evidence, as they, in their sole discretion, conclude is probable cause to believe that an offense has taken place by one or more specific individuals.
 - J. The Airport General Manager, or his/her designee, of the HAS Airport and/or other HAS facility where the offense occurred shall have the authority, in their sole discretion, to void any NOV issued by those under their line command. All voids must be in writing, signed by the individual making the void and shall state thereon the date and reason for the voiding and the writing shall be attached by the HAS Badging Office to the White copy of the NOV and retained. A voided NOV cannot be used for the enhancement of other violations.
 - K. The Airport General Manager, or his/her designee, of an HAS Airport, and/or other HAS facility for which he/she is responsible, shall have authority to immediately suspend the HAS ID Badge of any person whom said Airport General Manager, or designee, deems, in his/her sole discretion, to be of imminent and/or continuing threat to the safety and/or security of the airport, meets disqualifying conditions, including, but not limited to, its assets (including, but not limited to, its reputation, employees, real (including improvements thereto) and personal property), tenants and concessionaires and/or their assets, contractors and/or their assets, airlines and their assets, the traveling public, the general public, dignitaries, the airport community, the airport and/or air transportation industry, the United States of America, the State of Texas and/or any of its political subdivisions, including, but not limited to the County of Harris and/or the City of Houston and/or their citizens and employees.
 - L. The Airport General Manager, or his/her designee, shall also have the authority to, in his/her sole discretion, suspend any person not holding an HAS Authorized Badge from coming onto or remaining on HAS controlled property based on the same criteria as if they were an HAS ID Badge holder.

IX. CONTEST HEARING PROCESS

- A. All hearings held on an NOV shall be heard before a Hearing Panel of 1-3 Houston Airport System employees or a contracted Hearing Officer. The Airport General Manager or designee shall appoint a Hearing Panel/Hearing Officer for the airport wherein the NOV was issued. The appointed Hearing Panel members will be, at a minimum Pay grade 23 or higher. There shall not be any matter such as an arraignment or motion docket.
- B. A failure to file a Notice of Contest within fourteen (14) calendar days from the date of issue of the Notice of Violation shall constitute a plea of No Contest and acceptance of the consequence of having committed the violation.

- C. Hearings shall be restricted to the question of whether or not the alleged violator committed the offense. The records of the Houston Airport System shall be prima facie (the burden to prove the records are wrong are upon the person who claims they are wrong) evidence of the sustaining of or entry of no contest to a prior violation.
- D. There shall not be any hearing on any question of law, mitigation, probation or reduction of sanction. Any challenge as to questions of law shall be heard in a civil court of competent jurisdiction in Harris County, Texas, and such challenge must be filed by the alleged violator in such civil court not later than the thirtieth (30th) calendar day from the date the violation is sustained either by operation of rule or by written decision of the Hearing Panel/Hearing Officer. A challenge as to a question of law need not be proceeded by a Notice of Contest, the serving of a Citation and Petition shall be sufficient notice to HAS. HAS does hereby appoint the Assistant General Manager for Security as the agent upon which to serve process in, and only in, a challenge pursuant to this O.I.
- E. An entry of “No Contest” shall not require presentation to a Hearing Panel/Hearing Officer and shall be entered in the violator’s records in the HAS Badging Office by an employee thereof.
- F. The following procedures will be the method of contesting a Notice of Violation by a holder of an authorized HAS ID Badge. The Notice of Contest challenging the factual validity that the violator committed a violation charged in the NOV must be made by the alleged violator or by their Authorized Signature Authority in writing, on a form available from the ID Badging Office, delivered to the Head Supervisor for ID Badging (or to his/her specific Designee for this purpose) by either hand delivery or receipted delivery. Delivery may not be made by e-mail or fax. A written receipt of delivery must be signed by the Head Supervisor for ID Badging (or by his/her specific Designee for this purpose) – delivery to anyone else and acceptance by anyone else will not be effective delivery.
- G. The Contest Hearing shall be in person at an office or conference room made available to the Hearing Panel/Hearing Officer and attendance of the alleged violator’s Authorized Signature Authority with the alleged violator(s) is mandatory and will be at a time and date set by the Head Supervisor for ID Badging or designee. There shall not be any resets for the convenience of the Authorized Signature Authority or for the alleged violator, unless such reset is requested in writing, delivered to the Head Supervisor for ID Badging (or to his/her specific Designee for this purpose) by either hand delivery or receipted delivery, not later than five (5) calendar days prior to the date of the Hearing. Delivery may not be made by e-mail or fax. A written receipt of delivery must be signed by the Head Supervisor for ID Badging (or by his/her specific Designee for this purpose) – delivery to anyone else and acceptance by anyone else will not be effective delivery. Failure to attend a scheduled Notice of Contest Hearing may result in a sustained verdict for the Notice of Violation being challenged. Any reset shall be at the discretion of the Hearing Panel/Hearing Officer.
- H. All consequences of an alleged violation shall be suspended until the Hearing Panel/Hearing Officer has issued a written ruling or the time has passed for the filing of a Notice of Contest. When the alleged violation is a 3rd offense, there may be an Immediate Temporary Suspension and the alleged violator may follow the process for contesting the Immediate Temporary Suspension listed in Section X below, entitled “CONTEST HEARING PROCESS – Immediate Temporary Suspension”.
- I. Any service required or desired to be made upon the alleged violator may be served directly upon the alleged violator or upon any Authorized Signatory Authority at his/her employer/sponsor.
- J. The alleged violator and HAS shall have the right to compel any individual holding an HAS airport ID Badge to appear at any hearing before a Hearing Panel/Hearing Officer and failure to appear may result in the issuance of an NOV to the individual who did not appear. To invoke this right, the alleged violator must file a Requested Compelled ID Badged Witness List with the Head Supervisor for ID Badging (or to his/her specific Designee for this purpose) by either hand delivery or receipted delivery, not later than ten (10) calendar days prior to the date of the Hearing. The individual being compelled shall be entitled to a

total fee of \$ 20.00; including mileage, for their attendance and said fees must be attached to the Requested Compelled ID Badged Witness List. Delivery may not be made by e-mail or fax. A written receipt of delivery must be signed by the Head Supervisor for ID Badging (or to his/her specific Designee for this purpose) – delivery to anyone else and acceptance by anyone else will not be effective delivery. Such compunction shall not be available for Contest of Immediate Temporary Suspension and it shall be incumbent on the alleged violator to produce his/her own witnesses.

- K. At any hearing, all of which are administrative in nature, the Hearing Panel/Hearing Officer hearing the matter shall review the details of the offense, receive the testimony of the alleged violator, the person issuing the NOV, witnesses called by the alleged violator and witnesses called by HAS, the testimony of the alleged violator's Authorized Signature Authority (ASA) (if desired by the alleged violator, the ASA or HAS), and shall, in their sole discretion, make all decisions regarding the factual nature of the testimony, including, but not limited to, the weight to be given to the testimony of any party or witness, whether or not to believe the testimony, in whole or in part, apply the facts as determined by the person hearing the matter, in their sole discretion, to the elements of the offense and render a decision in accordance other provisions of this O.I.
- L. Each side shall be allowed five (5) minutes for an opening statement, thirty (30) minutes for examination of witnesses (this time includes, direct, cross, redirect, re-cross and rebuttal examination) and each party shall be allowed ten (10) minutes for a closing statement/argument.
- M. The Hearing Panel/Hearing Officer hearing the matter shall, not later than 24 hours after the hearing, issue and start delivery to the alleged violator's Employer and/or Sponsoring Organization and to the Assistant General Manager of Security, a written decision on the form then presently in use, save and except for a Contest of Immediate Temporary Suspension, the decision for which will be issued and be delivered at the conclusion of the hearing.
- N. The ruling of the Hearing Panel/Hearing Officer hearing the matter need not be detailed nor technical; a simple statement along the lines of "After hearing was held on XX day of XXXX, 2XXX, the undersigned person(s) sitting as the Hearing Panel/Hearing Officer hearing the contest of NOV # XXXXX wherein the party alleged to have committed the violation of XXXXX XXXXXX finds that the charge in the NOV is (either sustained or overruled)." and the Hearing Panel/Hearing Officer shall sign and date the same and file the ruling with the appropriate ID Badging Office.
- O. Except as otherwise specifically stated, the burden of proof shall be upon HAS as to the ultimate question in each hearing; the ultimate question being did the alleged violator commit the offense.
- P. The standard of proof shall be preponderance of evidence.
- Q. All decisions as to matters of fact and applying the facts to the violated rule shall be in the sole discretion of the Hearing Panel/Hearing Officer.
- R. Formal rules of evidence shall not apply; copies of documents may be introduced providing that the Hearing Panel/Hearing Officer hearing the matter, in their sole discretion, after considering testimony and argument as to the trustworthiness or lack of trustworthiness of the copy, has been heard and considered.
- S. There shall not be any pre-hearing discovery allowed.
- T. At any hearing, whether an NOV is issued by Airport Security personnel or by Airport Operations personnel, the HAS representative, i.e., the person who shall present the case on behalf of HAS, shall be the person who wrote the NOV.
- U. In any and all hearings, either HAS or the alleged violator may invoke "the Rule", requiring that witnesses not be allowed to hear the testimony of other witnesses.

- V. In any and all hearings, all witnesses shall testify under oath or affirmation with penalty of perjury.
- W. All hearings shall be either voice or video (with voice) recorded. The Badging Office will retain the recordings, records and will be provided to either HAS or the alleged violator upon written request.
- X. There is not an automatic stay of sanctions when an individual appeals to a court of competent jurisdiction; the appealing party must seek and obtain injunctive relief for a stay.
- Y. An alleged violator may not be represented by any individual or organization other than himself/herself at any hearing, save and except for an Attorney at Law duly licensed in the State of Texas.
- Z. The decision of the Hearing Panel/Hearing Officer is final and non-appealable as to the facts and the sanctions imposed.

X. CONTEST HEARING PROCESS – Immediate Temporary Suspension

- A. In the event of an Immediate Temporary Suspension the same shall be timely contestable by Notice of Contest of Immediate Temporary Suspension, in writing, on the form available from the ID Badging Office and filed by the alleged violator or their Authorized Signature Authority with the ID Badging Office at the HAS Airport where the NOV was issued. A hearing, restricted solely as to the issue as to whether or not the continued presence of the alleged violator represents any level of danger to the airport or other individuals or entities at the airport shall be heard by the Airport General Manager of Security (or to his/her specific Designee for this purpose) not later than forty-eight (48) hours after the request for such a hearing is delivered to the ID Badging Office by either hand delivery or receipted delivery. Delivery may not be made by e-mail or fax. A written receipt of delivery must be signed by the Head Supervisor for ID Badging (or by his/her specific Designee for this purpose) – delivery to anyone else and acceptance by anyone else will not be effective delivery.
- B. There shall not be any hearing on any question of law, mitigation, probation or reduction of sanction. Any challenge as to questions of law shall be heard in a civil court of competent jurisdiction in Harris County, Texas.
- C. Any service required or desired to be made upon the alleged violator may be served directly upon the alleged violator or upon any Authorized Signatory Authority at his/her employer/sponsor.
- D. When the alleged violation is a 3rd Class I violation there may be an immediate suspension. The alleged violator may be instructed to immediately surrender and deliver their ID Badge to the Head Supervisor for ID Badging, or his/her specific Designee for this purpose, and may be instructed to immediately thereafter remove themselves from HAS owned or lease property and any presence on HAS owned or lease property during the period of suspension, save and except for the specific purpose of arrival or departure from the HAS airport on a flight, shall be trespass after warning and the violator shall be subject to arrest. If it is necessary for the suspended party to visit their employer who is located on HAS owned or lease property at the employer's request, the suspended party must notify the Airport Communications Center's Security Dispatch at least sixty (60) minutes prior to coming upon HAS owned or lease property and must be and remain, at all times, under an escort, even in the public areas, from the employer. During this time the suspended party MAY NOT engage in any business on behalf of the employer or any other party -- the suspended party shall not be or remain on the HAS owned or leased property in excess of sixty (60) minutes.
- E. The Assistant General Manager of Security or Operations (or designee) will render a decision which will be issued at the conclusion of the hearing.
- F. The ruling of the party hearing a Contest of Immediate Temporary Suspension need not be detailed nor technical; a simple statement along the lines of "After hearing was held on XX day of XXXX, 2XXX, the

undersigned person hearing the Contest of Suspension due to NOV # XXXXX wherein the party alleged to have committed the violation of XXXXX XXXXXX finds (choose one of the following) (a) that the alleged violator presents an unreasonable risk and it is in the interest of the safety and/or security of the HAS community alleged violator be immediately suspended and the Immediate Temporary Suspension is sustained or (b) that the alleged violator does not present an unreasonable risk to the safety or security of the HAS community and the Immediate Temporary Suspension is overruled)." and the person hearing the matter shall sign the same.

- G. The burden of proof in a hearing on a Contest of Immediate Temporary Suspension shall be on the alleged violator to prove that the alleged violator's presence on HAS property does not present an unreasonable risk of safety and/or security of the HAS community.
- H. In, and only in, a Contest of Immediate Temporary Suspension the alleged violator, the alleged violator's employer and/or sponsor and the Assistant General Manager of Security and the Airport General Manager may, prior to the contest hearing, enter into an Agreed Order to remove the Immediate Temporary Suspension, subject to certain conditions being imposed upon the alleged violator and those conditions and the decision to enter into such an Agreed Order or not to enter into such an Agreed Order shall be in the sole discretion of HAS -- all of the foregoing parties must agree in order for there to be a valid Agreed Order and a lifting of the Immediate Temporary Suspension.
- I. All decisions as to matters of fact and applying the facts to the rule shall be in the sole discretion of the Assistant General Manager for Security (or designee).
- J. Formal rules of evidence shall not apply; copies of documents may be introduced providing that the Assistant General Manager of Security (or designee) hearing the matter, in their sole discretion, after considering testimony and argument as to the trustworthiness or lack of trustworthiness of the copy, has been heard and considered.
- K. HAS and/or the alleged violator may introduce written statements.
- L. An alleged violator may not be represented by any individual or organization other than himself/herself at any hearing, save and except for an Attorney at Law duly licensed in the State of Texas.
- M. The decision of the Assistant General Manager of Security is final and non-appealable as to the facts and the sanctions imposed.

XI. MISCELLANEOUS MATTERS

- A. Having an HAS ID Badge and/or access rights is not a right, but is a privilege, and persons so having such ID Badge and/or access rights are only entitled to the same for so long as they meet all qualifications, including, but not limited to, being employed with an authorized sponsor and not having been found to have committed a violation or violations which can result in the suspension of the ID Badge and/or access rights.
- B. In the event that acts or omissions could result in the issuance of an NOV for more than one offense and if an NOV is, in fact, issued for multiple offenses, all of the charged offenses may be adjudicated and accrue sanctions for each sustained violation; however, in any event, it shall not be a defense, nor a mitigating fact that an NOV could have been written for only one violation;
- C. The Airport General Manager, in his/her sole discretion, has the right to make reasonable and prudent changes, clarifications, modifications, additions and/or subtractions to this O.I. and to O.I. 05-02, provided that the changes do not result in the loss of any substantive rights to any individual or entity charged with a violation prior to the date of the changes, clarifications, modifications, additions and/or subtractions or within 30 calendar days of posting such revision in the I.D. Badging Office.

- D. In the event that any agency, department, or division named herein changes its name or its duties be assigned to another agency, department or division, such new name or the name of the successor/replacement agency, department or division shall be substituted in place of any such agency, department or division presently named herein and no amendment to this O.I. shall be required;
- E. In the event that any specifically enumerated law, ordinance, rule or regulation set forth herein shall be renumbered, modified or replaced, then the new number and/or law, ordinance, rule or regulation that, in the sole discretion of the Airport General Manager, deems to be appropriate for replacement of the present law, ordinance, rule or regulation shall be substituted in place thereof and no amendment to this O.I. shall be required, however, the replacement law, ordinance, rule or regulation so designated shall be posted in the ID Badging Office and shall be obtainable therefrom.
- F. Any and all violations listed within this document can be charged as company offense.

ATTACHMENT # 1

Notice Of Violation Sample
(FRONT)

Sample	(To Fit on Approx. 5 x 7.5)	(Printer to Reduce to Fit)	Sample
	Houston Airport System		
Date of Issue: _____	NOTICE OF VIOLATION		NO. XXXXXXXXXXX
<input type="checkbox"/> IAH	Date of Offense: _____	Time: _____	am or pm _____
<input type="checkbox"/> HOU	Name of Offender: _____		
	Last	First	MI
<input type="checkbox"/> EFD	HAS Badge No.: _____	Expires: _____	
Badge Sponsoring Organization: _____			
Supervisor's Name: _____		Supervisor's Phone #: _____	
If Vehicle/Equipment Involved: _____			
	Type	Make	Plate or ID Number
If No HAS Badge Driver's License #: _____ State: _____ D/O/B: _____			
Offense Approximate Location: _____			
Rule #: _____			SIDA AOA STERILE (Circle If Applicable)
Violation Details: _____			
Signature of Violator	Signature of Issuer	Name Printed	ID Badge No.
White - Badging Pink - Violator Green - Badging Yellow - Issuer Manila - Division Mgmt. of Issuer			

ATTACHMENT # 1

Notice Of Violation Sample
(BACK)

YOU HAVE THE RIGHT TO CONTEST THIS NOV VIA A HEARING.

TO EXERCISE THIS RIGHT YOU MUST FILE A NOTICE OF CONTEST WITHIN FOURTEEN CALENDAR DAYS FROM THE DATE OF ISSUE OF THIS NOV.

Failure to file the Notice of Contest within the aforesaid time will forever bar you from contesting the NOV and a plea of "No Contest" shall be automatically entered into your records and you will be assessed the sanctions for such violation and you will accrue sanction points for possible enhancement in the event of further violations, etc.

An entry of "No Contest" shall not require presentation to a Hearing Panel/Hearing Officer and shall be entered in the violator's records in the HAS Badging Office by an employee thereof.

The following procedures will be the method of contesting a Notice of Violation by a holder of an authorized HAS ID Badge. The Notice of Contest challenging the factual validity that the violator committed the violation charged in the NOV must be made by the alleged violator in writing, on a form available from the ID Badging Office, delivered to the Head Supervisor for ID Badging (or to his/her specific Designee for this purpose) by either hand delivery or receipted delivery. All Notice of Contests must include a complete statement as to the basis of the appeal as well as any and all supporting documentation, including but not limited to witness statements. Delivery may not be made by e-mail or fax. A written receipt of delivery must be signed by the Head Supervisor for ID Badging (or by his/her specific Designee for this purpose) – delivery to anyone else and acceptance by anyone else will not be effective delivery.

In the event of an immediate HAS ID Media confiscation, the Airport General Manager, or his/her designee, will review the relevant circumstances to determine if the immediate confiscation should remain in effect or be over-ruled. This review will be completed and a ruling issued no later than the following two (2) business days. The alleged violator retains the option to file a Notice of Contest within fourteen (14) calendar days of the immediate confiscation.

At any hearing, all of which are administrative in nature, the Hearing Panel/Hearing Officer reviewing the matter shall review the Notice of Contest documentation, to include details of the offense, the written appeal statement of the alleged violator, any applicable written witness statements, the written report from the person issuing the NOV, and shall, in their sole discretion, make all decisions regarding the factual nature of the written statements, apply the facts as determined by the Hearing Panel/Hearing Officer reviewing the matter to the elements of the offense and render a decision in accordance with other provisions of this O.I.

The ID Badging Office will at time of filing of your Notice of Contest supply you with a copy of the hearing procedures and rules therefore. **These procedures and rules are also set forth in an Operating Instruction (O.I.) of the Houston Airport System - your badge sponsor and/or employer should have a copy of the O.I., but you are personally responsible to make sure you are going by the most current version of the O.I.**

Violations:

CLASS I Notice of Violations:

1. Failure to display valid HAS approved identification and/or ID Badge or HAS authorized ID Badge that is appropriate for the airport and the area in the prescribed manner, for special management and security purposes, specifically authorized in writing by either the Airport Security Manager or Director of Operations;
2. Failure to challenge someone in a controlled or restricted area who is not properly displaying an ID badge;
3. Possession of an HAS ID Badge that is substantially damaged, broken, faded, illegible;
4. Using a portal in a manner that has not been specifically authorized by HAS;
5. Failure to follow picketing/solicitation procedures anywhere on the Airport;
6. Smoking in an area where smoking is unauthorized and/or unlawful;
7. Violation of the Ten Foot Clear Zone, i.e., having an asset, including a disposed or abandoned asset, located closer than 10 feet to the perimeter fence line in areas where a 10 feet or greater distance is required, either inside or outside of the fence line – This may be a company offense for the company or organization whose assets are located in violation;
8. Failure to properly secure or dispose of Sensitive Security Information;
9. Displaying and/or using an ID Badge that has been reported lost, stolen, is expired or has been deactivated;
10. Failure to show an HAS authorized ID Badge appropriate for the airport and the area when challenged;
11. Piggybacking -- when one or more individuals, who are holders of an HAS ID Badge, follow another individual through a controlled access point without using their own ID Badge, Security Key or PIN Number (unless they are under proper escort procedures and have a legitimate need to move through that portal);

12. Tailgating -- when one or more individuals, who are not holders of an HAS authorized ID Badge, or are holders of an HAS authorized ID Badge but do not have it on their person, follows another HAS ID Badged individual through a controlled access point -- the HAS ID Badged individual who does have their ID Badge with them must ensure the portal is secure prior to moving away and challenge the person, peacefully attempt to get them to leave the area, immediately notify Airport Security Dispatch, 281-230-1300 IAH or 713-845-6555 HOU and EFD and assist Airport Operations Officers in attempting to locate the tailgater;
13. Failure to challenge a Piggybacker or Tailgater -- a HAS ID Badged individual must ensure the portal is secure prior to moving away and challenge someone who is piggybacking. They shall wait while the piggybacker exits and returns through the portal properly using their own ID Badge, Security Key or PIN Number, if the piggybacker refuses to so comply a HAS ID Badged Individual must attempt to obtain the piggybacker's name, but whether they can obtain the name or not they must immediately report the same to Airport Security Dispatch, 281-230-1300 IAH or 713-845-6555 HOU and EFD and assist Airport Operations Officers in attempting to locate the piggybacker;
14. Failure to secure or follow stop and wait procedures any portal: which is required to be secure if not in use for operational needs, including, but not limited to a vehicle gate, pedestrian gate, door, and/or other portal;
15. Failure to use the "timed override" door function at a controlled portal where or when required;
16. Violation of Escort Procedures -- the escorting of one or more individuals (on foot or in a vehicle) into a restricted or controlled area and not strictly following the procedures related to proper identification, vehicle signs, and/or the requirement to remain with the individual/vehicle being escorted. **Special Note #1** Individuals who have been issued an ID Badge but who do not have the badge in their possession (left it home, in vehicle, lost, etc.) may not be escorted through any security access point or in or into any restricted or controlled area -- to do so is an offense for both the escorter and the escortee. **Special Note #2** Individuals who have applied for, but have not yet been issued an HAS or HAS Authorized Badge, if even allowed at all in a restricted or controlled area, must be at all times escorted and remain under strict escort and control of the escorting party at all times they are in a restricted or controlled area.
17. Failure to properly store and/or secure TSA prohibited items in a secured, restricted, or sterile area;
18. Failure to obey airside traffic controls, postings, or devices;
19. The violation of any other law, ordinance, policy, procedure, rule or regulation related to HAS and its security, airside safety, life safety or operations, including but not limited to business and field operations;

20. Performance of any action in conflict with FAR Part 139, the Airport Certification Manual, and/or the AOA Driver Training program;
21. Failure to yield to an aircraft under either tow or taxiing;
22. Failure to yield to an emergency vehicle;
23. Operating a vehicle on the airside without airport authorization;
24. Conducting and/or permitting an unsafe fueling operation anywhere on the airport;
25. Failure to control, as opposed to failure to properly escort, personnel and equipment in a secured area ;
26. Failure to submit to or perform the requirements of sanctions, after the sanctions have become final under this O.I., within the time allotted in this O.I. -- is a separate offense;
27. Towing an excessive number of trailer devices;
28. Operating a ground vehicle on the airside without having required lights in proper working order and/or not having lights in operation;
29. Operating a ground vehicle on the airside without a valid driver's license;
30. Operating a vehicle on the airside without required markings;
31. Unauthorized vehicle on the Aircraft Operating Area;
32. Abandoning a disabled vehicle in a secure area;
33. Failure to report a "reportable" hazardous material spill anywhere on the airport;
34. Operating and/or permitting the operation, including the movement thereof, of improperly maintained fueling equipment anywhere on the Airport;
35. Improper cleanup and/or permitting improper cleanup of a hazardous material spill anywhere on the airport;

36. Failure to follow prescribed engine run-up procedures;
37. Operating a ground vehicle in the secured area in excess of posted or published speed limit, in a reckless/unsafe manner and/or in excess of a safe speed limit considering the conditions of traffic (including, but not limited to pedestrian, aircraft, equipment and/or vehicular), driving surface, weather conditions, and/or exigent circumstances and;
38. Failure to display appropriate company signage on both sides of a vehicle or displaying or attempting to use any other expired permissive vehicle media, on an unattended vehicle parked in a “No Parking”, “Tow Away” , “Restricted” parking area, SIDA or AOA;
39. Allowing an individual to begin work in a controlled or secured area prior to successfully completing and submitting the required HAS badge application and process even if under escort.

Class II Sanctions are:

Permanent loss of ID Badge and Access Rights at all HAS airports. Violators of a Class II violation are subject to Immediate Temporary Suspension.

CLASS II Notice of Violations:

40. Displaying, loaning and/or permitting use of an HAS authorized ID Badge, assigned keys or PIN Number to or by another individual;
41. Failure or refusal to fully, completely, timely and truthfully cooperate -- including appearing when and at the place designated, with an investigation, audit or a proceeding by or instituted by or flowing from the acts of any Division of HAS. Misrepresentation or falsification, including but not limited to, intentionally or knowingly or recklessly leaving off any relevant information on any document delivered to HAS ;
42. Failure to surrender an individual's own ID Badge upon termination of employment to the individual's employer/sponsor/HAS ID Badging Office, or failure to surrender ID Badge upon request to anyone authorized to issue an NOV or a Law Enforcement Officer;
43. Intentionally or knowingly interfering with or failure to follow legitimate instructions from an employee of or contractor to HAS Airport Security or Airport Operations in the performance of their official duties;
44. Use, duplicate, or reproduce media or keys or authorizing access to any controlled or restricted area without written permission from either the ID Badging Office or, if the access device is not under the control of the ID Badging Office, the owner of the access device;
45. The failure to immediately notify the HAS ID Badging Office of an arrest for an HAS listed disqualifying crime;
46. Displaying and/or using an ID Badge that is not the violator's own badge;
47. Intentionally physically forcing a secured portal open instead of using an ID badge, PIN pad, or key;
48. Sabotaging, damaging, destroying a security or life safety device or system or any portion thereof; or disabling, bypassing, removal or modifying a security or life safety device or system or any portion thereof, without written permission of the Airport Security Manager or Director of Operations, or his/her designee -- *the actual existence of a life safety emergency is exception to the enforcement of this subparagraph, however, the burden of proof of the actual existence of a life safety emergency is upon the alleged violator -- this offense may also be a company offense, if any supervisor for the company had any knowledge that such may be occurring;*

49. Causing a runway vehicle/pedestrian incursion and or entering the airside Movement Area without an Air Traffic Control Tower clearance and/or failure to obey instructions from the Air Traffic Control Tower;
50. Theft in any amount occurring upon HAS property;
51. Introducing or having a prohibited weapon or weapons (other than tools, knives and other items that are essential and authorized for a work related purpose) or a firearm by an employee other than law enforcement officers and/or Security personnel specifically authorized to do so other in a restricted, secured or controlled area. Replicas or non-functional devices will be treated as prohibited weapons for the purpose of this policy.
52. Possession or consumption of alcoholic beverages or controlled substances on HAS property, other than by a person licensed or employed by a licensee in the course and scope of their employment for the beverage or controlled substance including if the violator is driving on the AOA, airside ramps and/or tug tunnels, or part of their primary work function involves driving in one or more of the foregoing areas and then offense is a Class II Violation;
53. Refusing, or failing to comply with a required inspection, search, or screening of an individual or an individual's accessible property.
54. An airline employee including, but not limited to, Flight Crew, Cabin Crew, mechanic or any other employee boarding or attempting to board an aircraft as a passenger or any individual not specifically designated or acting as an active crew member for that flight that accessed the sterile area through an access point other than a TSA screening checkpoint.

Appendix G. Spill Response and Procedures

SPILL RESPONSE & PROCEDURES

Is this an **INCIDENTAL RELEASE**?

- The spill is small in size and/or **CAN** be cleaned up safely by 1 or 2 people &
- The material spilled is a **KNOWN** material &
- The hazards associated with the material are **KNOWN** and **NOT** considered extremely dangerous.

Or

Is this an **EMERGENCY RESPONSE**?

- The spill is large in size and/or **CANNOT** be cleaned safely by 1 or 2 people or
(More than **3 Gallons** or **50 Square Feet** (5 * 10 Foot Area) *NFPA-407*
- The material spilled is an **UNKNOWN** material or
- **Aircraft Overfill/Mechanical Failure of Fuel System** *NFPA-407*

INCIDENTAL RELEASE

PROTECT THE STORM DRAINS AND CONTAIN THE SPILL USING SPILL KITS. WEAR APPROPRAITE PERSONAL PROTECTIVE EQUIPMENT, BE SAFE, & DON'T WALK OR DRIVE THROUGH SPILL.

NOTIFY AIRSIDE OPERATIONS AND PROVIDE SPILL REPORT
IAH: (281) 233-1131
HOU: (713) 417-5710
EFD: (281) 433-1612

CLEAN-UP CONTAMINATED MATERIALS AND PLACE INTO APPROPRAITE CONTAINERS AND LABEL CONTAINERS TO REFLECT CONTENTS
ENSURE ALL MATERIALS PROPERLY CLEANED UP & NOT FOD HAZARD

STORE CONTAINERS IN A SECURED COVERED LOCATION

CONTACT YOUR COMPANY'S ENVIRONMENTAL REPESENTATIVE TO INSURE PROPER PICK-UP AND DISPOSAL

IAH, EFD, HOU
JAMES PARISE (ENVIRONMENTAL INVESTIGATOR V)
 OFFICE..... 281/233-1756
 CELL..... 281/684-5156
 EMAIL: JIM.PARISE@HOUSTONTX.GOV

EMERGENCY RESPONSE

CLEAR AREA IF POSSIBLE & REMAIN UP-WIND & OUT OF THE MATERIAL

CALL **911** & AIRPORT DISPATCH
IAH: (281) 230-1300
HOU/EFD: (713) 641-4100
 PROVIDE THE FOLLOWING INFORMATION:

- ANY INJURIES
- NAME OF MATERIAL SPILLED
- ESTIMATED AMOUNT SPILLED
- LOCATION OF SPILL (GATE/RAMP)

PROTECT STORM DRAINS IF ABLE & AWAIT EMERGENCY RESPONDERS

PERFORM CLEAN-UP AS DIRECTED BY EMERGENCY RESPONDERS. WEAR APPROPRIATE PERSONAL PROTECTIVE EQUIPMENT & UTILIZE SPILL KITS IN AREA. BE SAFE & DON'T WALK OR DRIVE THROUGH SPILL.
ENSURE ALL MATERIALS PROPERLY CLEANED UP & NOT FOD HAZARD

PLACE CONTAMINATED MATERIALS INTO APPROPRIATE CONTAINERS AND LABEL CONTAINERS TO REFLECT CONTENTS

STORE CONTAINERS IN A SECURED COVERED LOCATION

CONTACT YOUR COMPANY'S ENVIRONMENTAL REPESENTATIVE TO INSURE PROPER PICK-UP AND DISPOSAL

REMEMBER TO FOLLOW THIS PROCEDURE FOR SPILLS IN ADDITION TO ANY COMPANY PROCEDURES OR PROTICALS REGARDING SPILLS.

SAFETY IS EVERYONE'S RESPONSIBILITY!