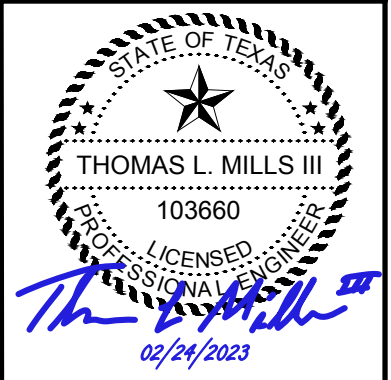


REVISIONS

NO.	DESCRIPTION	DATE	BY
0	ISSUED FOR BID	02/24/2023	SC

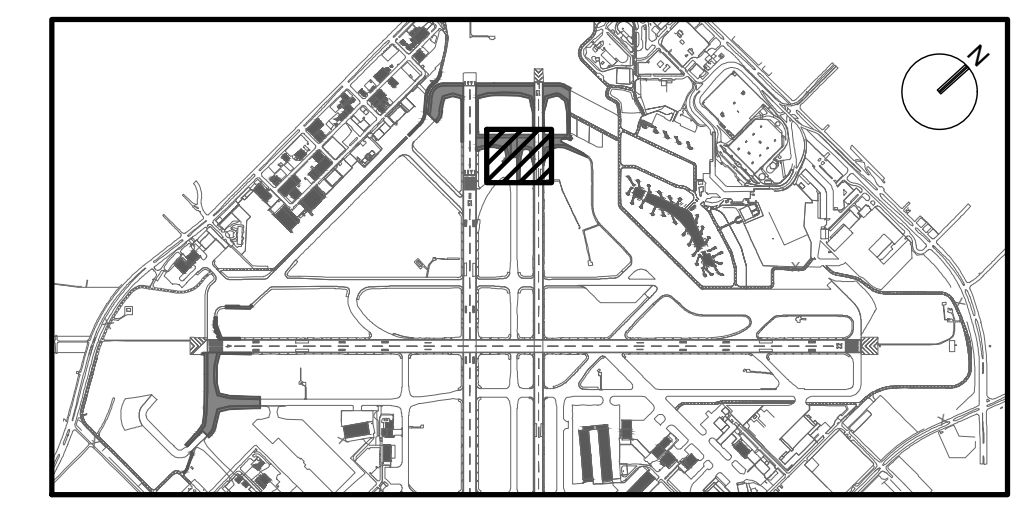
FAA NON-STANDARD TAXIWAYS PROJECT  
**PAVEMENT JOINTING & REINFORCEMENT PLAN**  
 - PHASE 1

PROJECT MGR: S. CHILDERS  
 DESIGNER: D. CRAWFORD  
 DRAWN BY: C. MCLAIN  
 CHECKED BY: R. EHTESHAM  
 SCALE: AS SHOWN  
 DATE: 02/24/2023



APPROVED BY: \_\_\_\_\_ DATE: \_\_\_\_\_  
 DIRECTOR  
 HOUSTON AIRPORT SYSTEM

PROJECT NO: 770  
 C.I.P. NO: 3-48-0110-044  
 H.A.S. NO: N/A  
 SHEET NO: CP101-P1  
 of



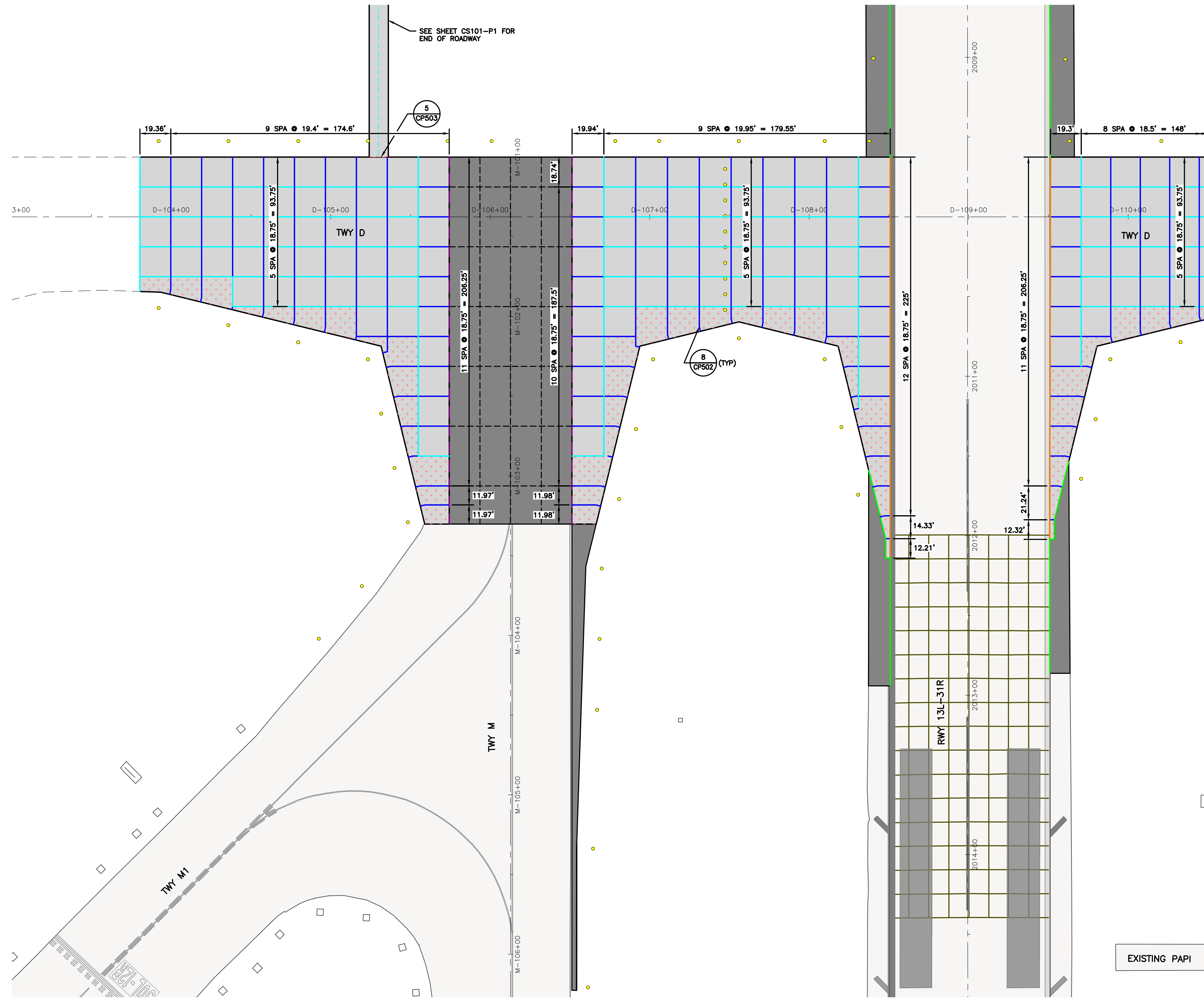
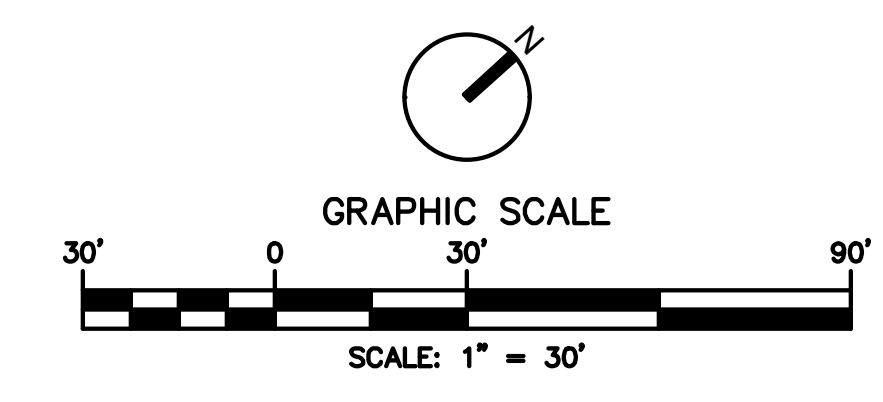
**KEY MAP**  
 NTS

**NOTES:**

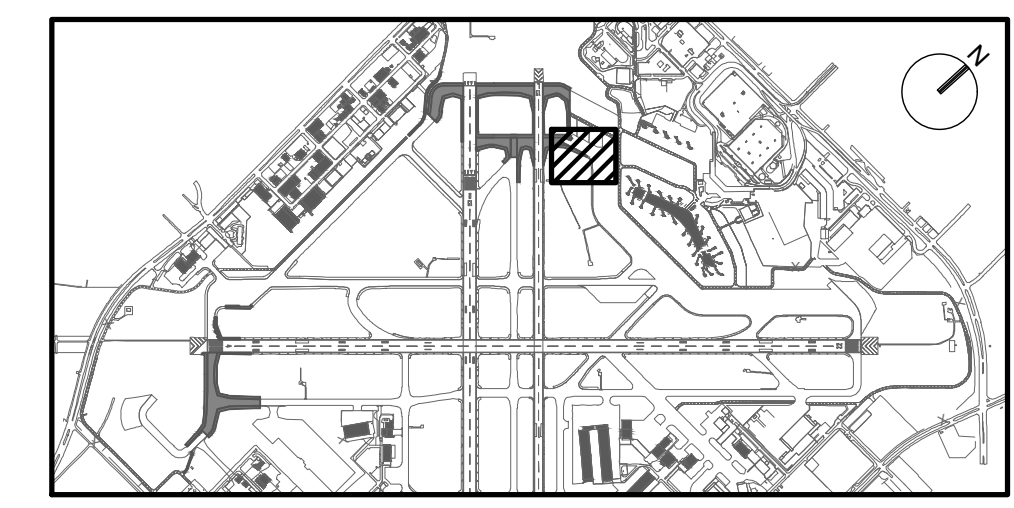
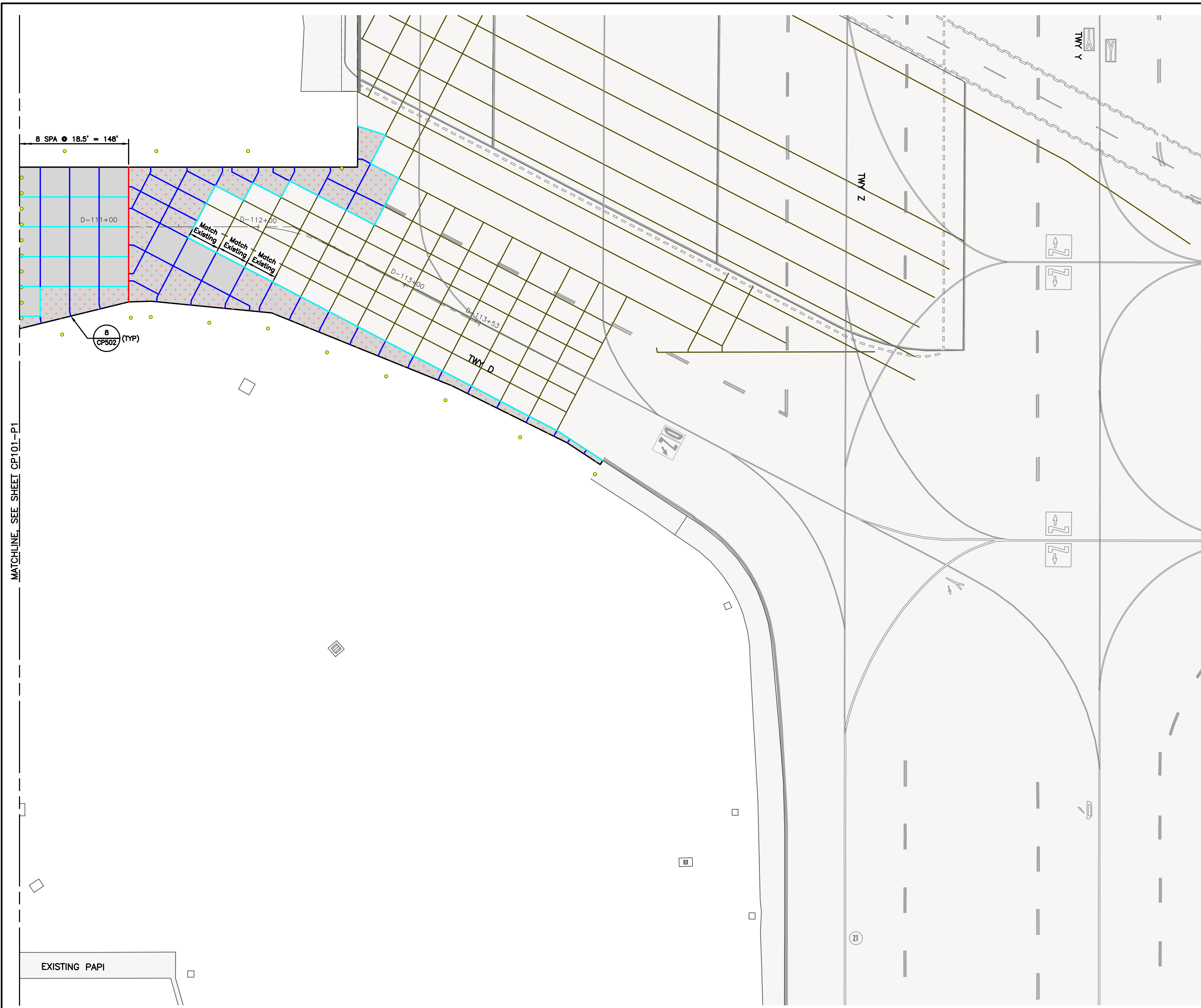
- FOR PAVEMENT JOINT TYPES AND REINFORCEMENT PLAN, SEE SHEETS CP701-P1 THROUGH CP710-P6.
- FOR PAVEMENT TYPICAL SECTIONS, SEE SHEETS CS001 THROUGH CS003.
- FOR GEOMETRIC LAYOUT, SEE SHEETS CS101-P1 THROUGH CS106-P6.
- ALL PAVEMENT JOINTS SHALL MATCH INTO EXISTING PAVEMENT JOINTS AT TIE-IN LOCATIONS.
- THE CONTRACTOR SHALL SUBMIT A PROPOSED PAVING PLAN TO THE ENGINEER FOR APPROVAL. DOWELS SHALL BE INSTALLED IN ALL CONSTRUCTION JOINTS IN ACCORDANCE WITH SPECIFICATION P-501. INTERIOR SLAB LOCATIONS SHOWN WITH A DOWELED CONSTRUCTION JOINT MAY BE MODIFIED TO A DOWELED CONTRACTION JOINT, WITH APPROVAL OF THE ENGINEER.
- CLEAN UP OF WASTE MATERIAL SHALL BE CONTINUOUS DURING THE JOINT SAWCUTTING OPERATION. CLEAN UP SHALL USE A VACUUM SWEEPER.
- THE CONTRACTOR SHALL LOCATE AND PROTECT ALL UTILITIES AND PAVED AREAS TO REMAIN. ALL UTILITIES ARE TO REMAIN, UNLESS OTHERWISE NOTED. DEMOLITION ACTIVITIES SHALL BE CAREFULLY CONTROLLED TO PREVENT DAMAGE TO ADJACENT CONCRETE PAVEMENT, THE UNDERLYING MATERIAL, OR EXISTING STRUCTURES TO REMAIN IN-PLACE. ANY DAMAGE TO EXISTING STRUCTURES OR PAVEMENT SHALL BE IMMEDIATELY REPORTED TO REPRESENTATIVE AND REPAIRED WITH APPROVED MATERIALS AND PROCEDURES AT THE CONTRACTORS EXPENSE.
- CARE SHALL BE TAKEN NOT TO UNDERMINE THE EXISTING PAVEMENT TO REMAIN.
- STRUCTURES AND LIGHTS SHOWN ARE FOR REFERENCE ONLY. SEE RESPECTIVE SHEETS FOR DETAILED DESIGN INFORMATION AND LEGEND.

**LEGEND:**

- PROPOSED CONCRETE PAVEMENT (PCC)
- PROPOSED ASPHALT PAVEMENT
- EXISTING PAVEMENT
- REINFORCED PCC **5** CP502
- PCC PAVEMENT JOINT - TYPE A (THICKENED EDGE ISOLATION) **1** CP501
- PCC PAVEMENT JOINT - TYPE AG (THICKENED EDGE ISOLATION) **2** CP501
- PCC PAVEMENT JOINT - TYPE A1 (REINFORCED ISOLATION) **3** CP501
- PCC PAVEMENT JOINT - TYPE C (DOWELED CONTRACTION) **4** CP501
- PCC PAVEMENT JOINT - TYPE E (DOWELED CONSTRUCTION) **5** CP501
- PCC PAVEMENT JOINT - TYPE G (JOINT BETWEEN PCC & HMA) **6** CP501
- PCC ISOLATION JOINT - TYPE AG1 **7** CP501
- PCC ROADWAY JOINT - TYPE E1 **3** CP503
- ASPHALT PAVEMENT GRID
- EXISTING PAVEMENT JOINT
- PROPOSED GEOMETRY OUTSIDE OF CURRENT PHASE (FOR REFERENCE ONLY)
- PROPOSED AIRFIELD BASE CAN FIXTURE, SEE NOTE 9







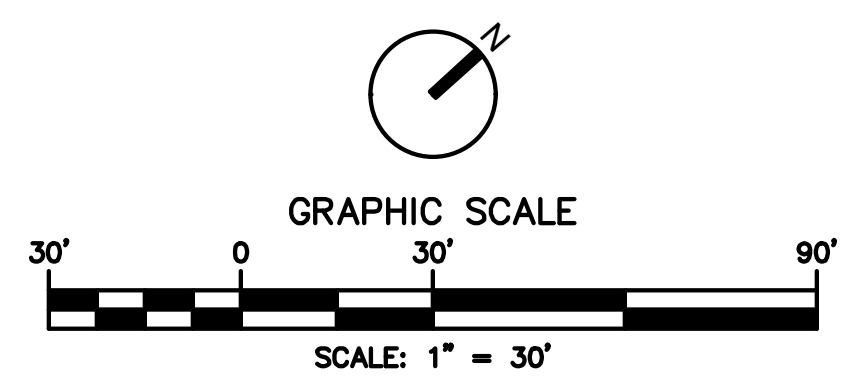
**KEY MAP**  
NTS

**NOTES:**

- FOR PAVEMENT JOINT TYPES AND REINFORCEMENT PLAN, SEE SHEETS CP701-P1 THROUGH CP710-P6.
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**LEGEND:**

- PROPOSED CONCRETE PAVEMENT (PCC)
- PROPOSED ASPHALT PAVEMENT
- EXISTING PAVEMENT
- REINFORCED PCC 5  
CP502
- PCC PAVEMENT JOINT - TYPE A (THICKENED EDGE ISOLATION) 1  
CP501
- PCC PAVEMENT JOINT - TYPE AG (THICKENED EDGE ISOLATION) 2  
CP501
- PCC PAVEMENT JOINT - TYPE A1 (REINFORCED ISOLATION) 3  
CP501
- PCC PAVEMENT JOINT - TYPE C (DOWELED CONTRACTION) 4  
CP501
- PCC PAVEMENT JOINT - TYPE E (DOWELED CONSTRUCTION) 5  
CP501
- PCC PAVEMENT JOINT - TYPE G (JOINT BETWEEN PCC & HMA) 6  
CP501
- PCC ISOLATION JOINT - TYPE AG1 7  
CP501
- PCC ROADWAY JOINT - TYPE E1 3  
CP503
- ASPHALT PAVEMENT GRID
- EXISTING PAVEMENT JOINT
- PROPOSED GEOMETRY OUTSIDE OF CURRENT PHASE (FOR REFERENCE ONLY)
- PROPOSED AIRFIELD BASE CAN FIXTURE, SEE NOTE 9

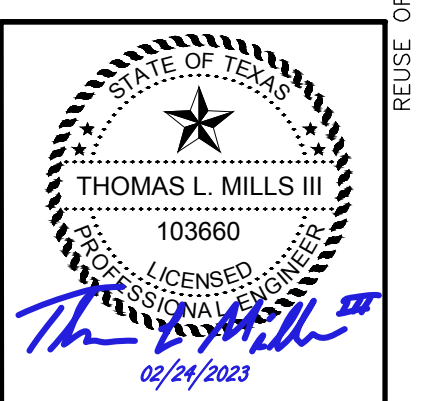


VERIFY SCALE BAR IS ONE INCH ON ORIGINAL DRAWING: 1"

NO.	DESCRIPTION	DATE	BY
0	ISSUED FOR BID	02/24/2023	SC

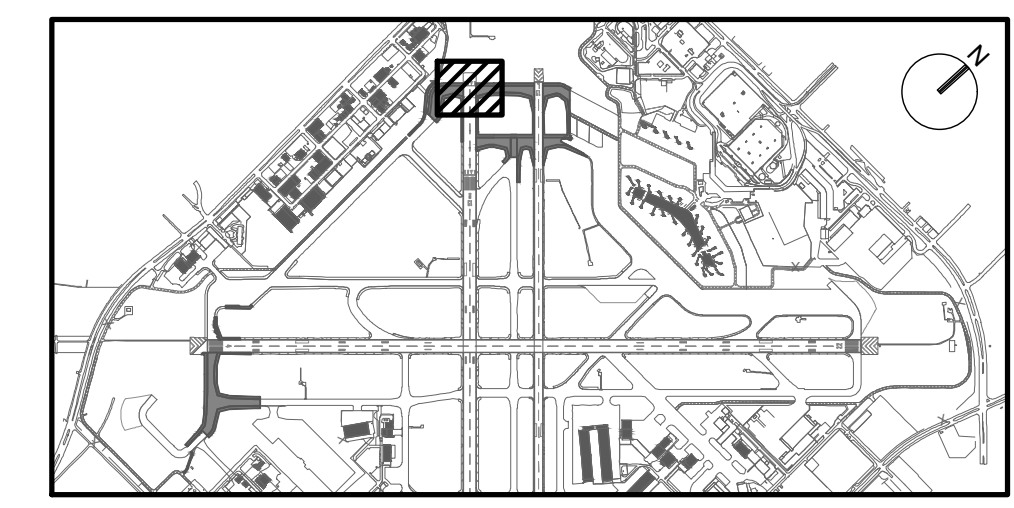
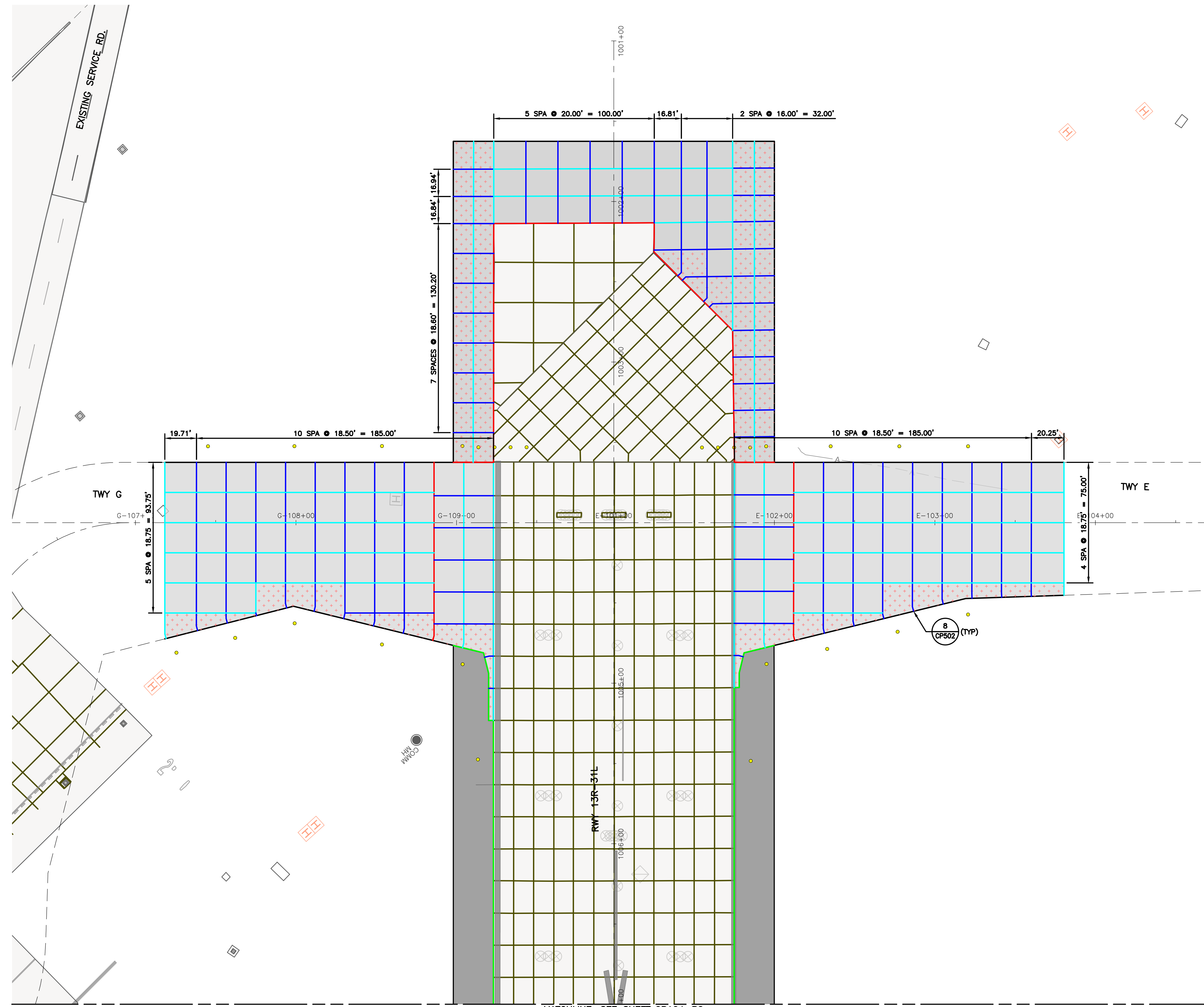
FAA NON-STANDARD TAXIWAYS PROJECT  
 PAVEMENT JOINTING & REINFORCEMENT PLAN  
 -- PHASE 1

PROJECT MGR:  
DESIGNER:  
DRAWN BY:  
CHECKED BY:  
SCALE: AS SHOWN  
DATE: 02/24/2023



APPROVED BY: \_\_\_\_\_ DATE: \_\_\_\_\_  
 DIRECTOR  
 HOUSTON AIRPORT SYSTEM  
 PROJECT NO: 770  
 C.I.P. NO: 3-48-0110-044  
 H.A.S. NO: N/A  
 SHEET NO: CP102-P1  
 of





**KEY MAP**  
NTS

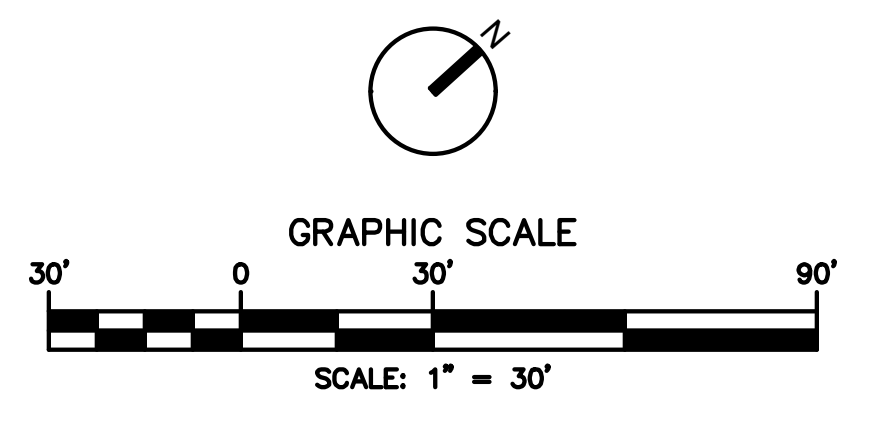
**NOTES:**

- FOR PAVEMENT JOINT TYPES AND REINFORCEMENT PLAN, SEE SHEETS CP701-P1 THROUGH CP710-P6.
- FOR PAVEMENT TYPICAL SECTIONS, SEE SHEETS CS001 THROUGH CS003.
- FOR GEOMETRIC LAYOUT, SEE SHEETS CS101-P1 THROUGH CS106-P6.
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**LEGEND:**

- PROPOSED CONCRETE PAVEMENT (PCC)
- PROPOSED ASPHALT PAVEMENT
- EXISTING PAVEMENT
- REINFORCED PCC (5 CP502)
- PCC PAVEMENT JOINT - TYPE A (THICKENED EDGE ISOLATION)
- PCC PAVEMENT JOINT - TYPE AG (THICKENED EDGE ISOLATION)
- PCC PAVEMENT JOINT - TYPE A1 (REINFORCED ISOLATION)
- PCC PAVEMENT JOINT - TYPE C (DOWELED CONTRACTION)
- PCC PAVEMENT JOINT - TYPE E (DOWELED CONSTRUCTION)
- PCC PAVEMENT JOINT - TYPE G (JOINT BETWEEN PCC & HMA)
- PCC ISOLATION JOINT - TYPE AG1
- PCC ROADWAY JOINT - TYPE E1
- ASPHALT PAVEMENT GRID
- EXISTING PAVEMENT JOINT
- PROPOSED GEOMETRY OUTSIDE OF CURRENT PHASE (FOR REFERENCE ONLY)
- PROPOSED AIRFIELD BASE CAN FIXTURE, SEE NOTE 9

- 1 CP501
- 2 CP501
- 3 CP501
- 4 CP501
- 5 CP501
- 6 CP501
- 7 CP501
- 8 CP502 (TYP)
- 3 CP503

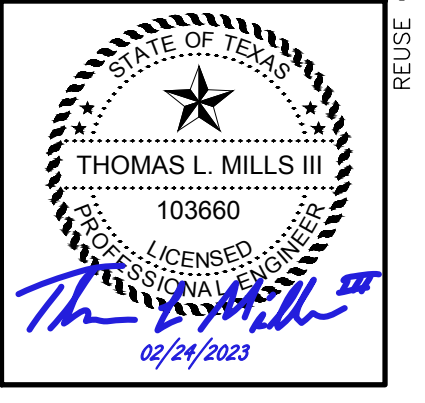


REVISIONS

NO.	DESCRIPTION	DATE	BY
0	ISSUED FOR BID	02/24/2023	SC

**FAA NON-STANDARD TAXIWAYS PROJECT**  
**PAVEMENT JOINTING & REINFORCEMENT PLAN**  
**— PHASE 2**

PROJECT MGR: S. CHILDERS  
 DESIGNER: D. CRAWFORD  
 DRAWN BY: C. MCCLAIN  
 CHECKED BY: R. EHTESHAM  
 SCALE: AS SHOWN  
 DATE: 02/24/2023



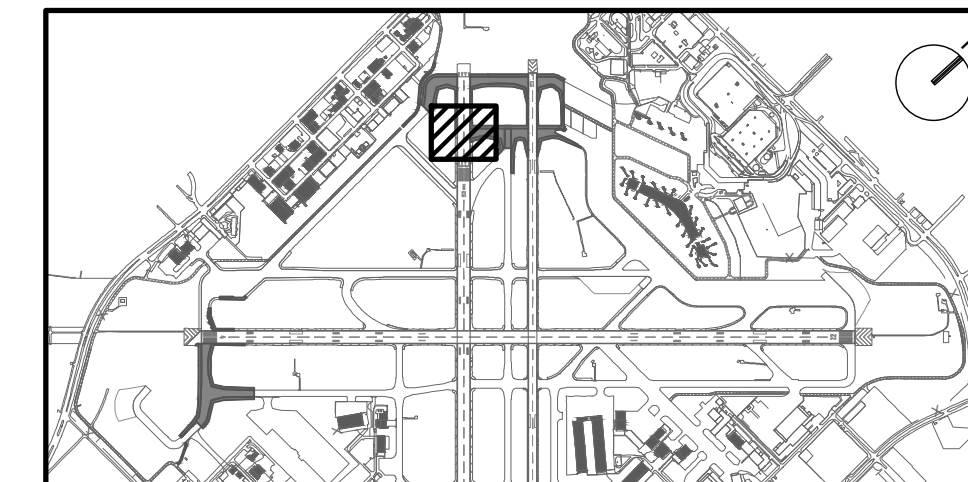
APPROVED BY: \_\_\_\_\_ DATE: \_\_\_\_\_  
 DIRECTOR  
 HOUSTON AIRPORT SYSTEM

PROJECT NO: 770  
 C.I.P. NO: 3-48-0110-044  
 H.A.S. NO: N/A  
 SHEET NO: CP103-P2  
 of

MATCHLINE, SEE SHEET CP104-P2



MATCHLINE, SEE SHEET CP103-P2



KEY MAP  
NTS

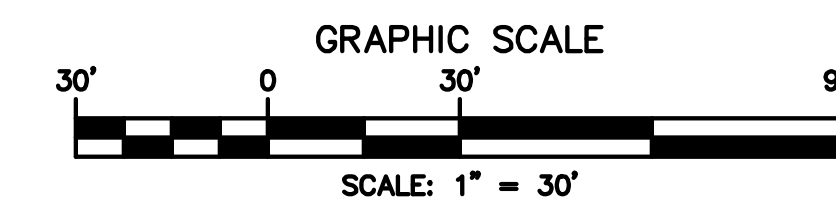
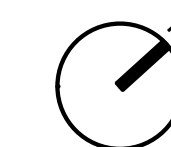
REVISIONS			
NO.	DESCRIPTION	DATE	BY
0	ISSUED FOR BID	02/24/2023	SC

NOTES:

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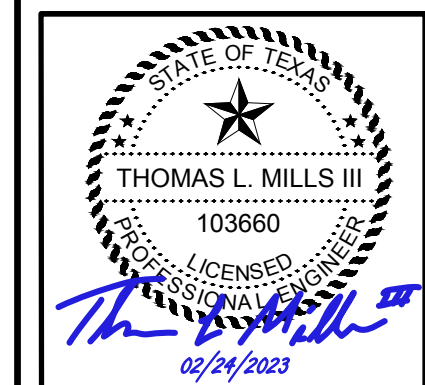
LEGEND:

- PROPOSED CONCRETE PAVEMENT (PCC)
- PROPOSED ASPHALT PAVEMENT
- EXISTING PAVEMENT
- REINFORCED PCC 5  
CP502
- PCC PAVEMENT JOINT - TYPE A (THICKENED EDGE ISOLATION) 1  
CP501
- PCC PAVEMENT JOINT - TYPE AG (THICKENED EDGE ISOLATION) 2  
CP501
- PCC PAVEMENT JOINT - TYPE A1 (REINFORCED ISOLATION) 3  
CP501
- PCC PAVEMENT JOINT - TYPE C (DOWELED CONTRACTION) 4  
CP501
- PCC PAVEMENT JOINT - TYPE E (DOWELED CONSTRUCTION) 5  
CP501
- PCC PAVEMENT JOINT - TYPE G (JOINT BETWEEN PCC & HMA) 6  
CP501
- PCC ISOLATION JOINT - TYPE AG1 7  
CP501
- PCC ROADWAY JOINT - TYPE E1 3  
CP503
- ASPHALT PAVEMENT GRID
- EXISTING PAVEMENT JOINT
- PROPOSED GEOMETRY OUTSIDE OF CURRENT PHASE (FOR REFERENCE ONLY)
- PROPOSED AIRFIELD BASE CAN FIXTURE, SEE NOTE 9



FAA NON-STANDARD TAXIWAYS PROJECT  
 PAVEMENT JOINTING & REINFORCEMENT PLAN  
 - PHASE 2

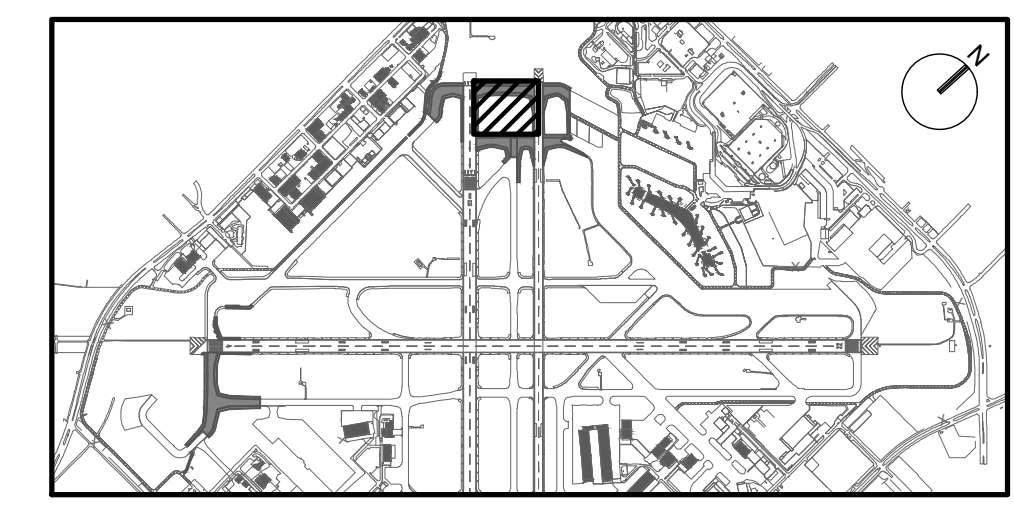
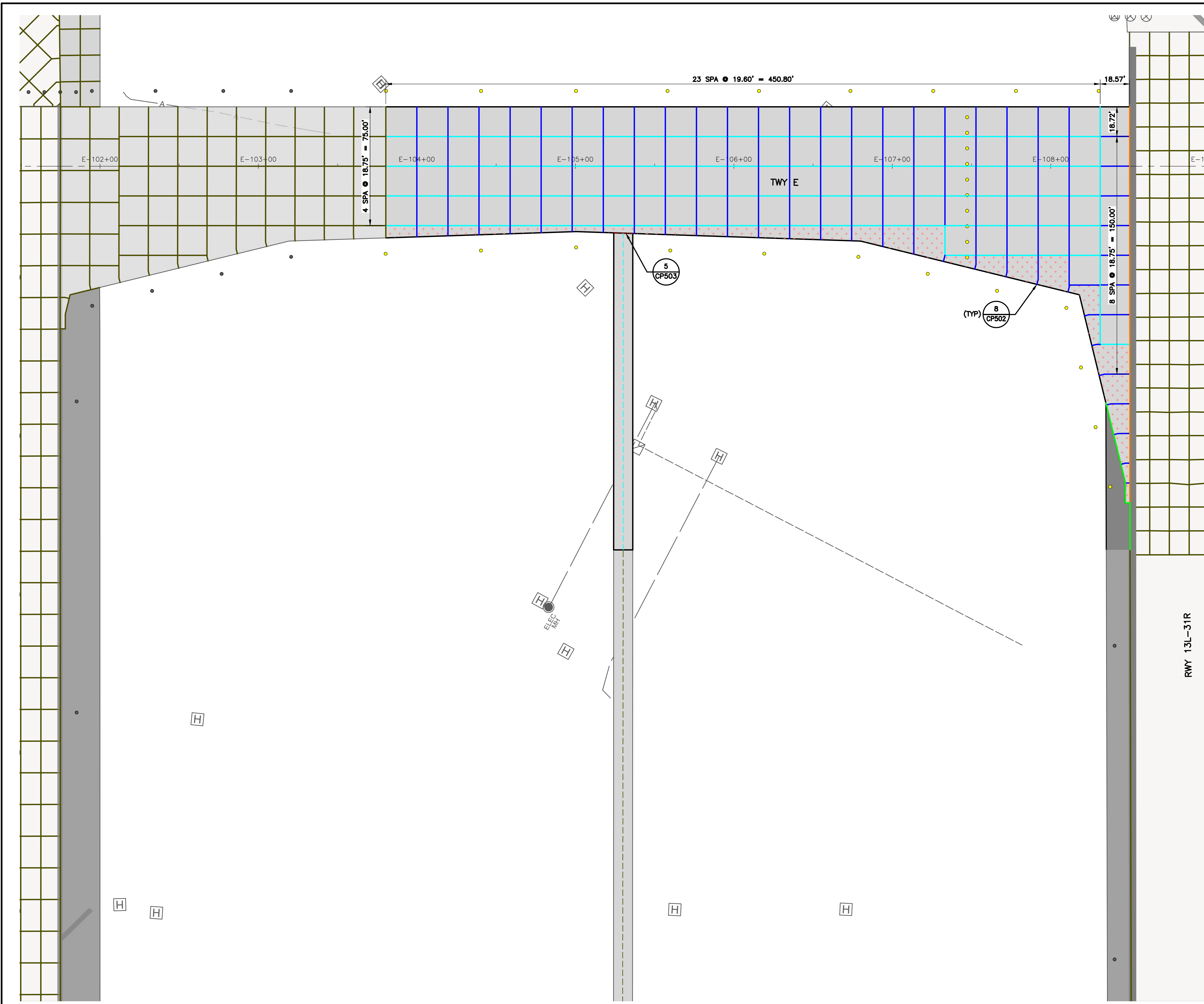
PROJECT MGR: S. CHILDERS  
 DESIGNER: D. CRAWFORD  
 DRAWN BY: C. MCLAIN  
 CHECKED BY: R. EHTESHAM  
 SCALE: AS SHOWN  
 DATE: 02/24/2023



APPROVED BY: \_\_\_\_\_ DATE: \_\_\_\_\_  
 DIRECTOR  
 HOUSTON AIRPORT SYSTEM

PROJECT NO: 770  
 C.I.P. NO: 3-48-0110-044  
 H.A.S. NO: N/A  
 SHEET NO: CP104-P2  
 of





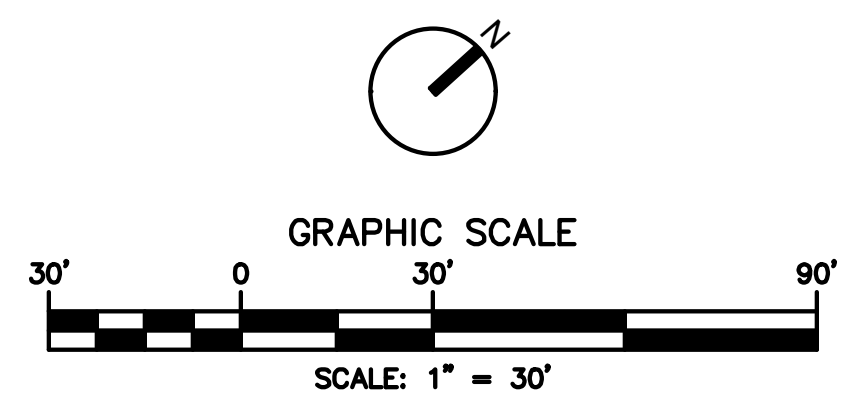
**KEY MAP**  
NTS

**NOTES:**

- FOR PAVEMENT JOINT TYPES AND REINFORCEMENT PLAN, SEE SHEETS CP701-P1 THROUGH CP710-P6.
- FOR PAVEMENT TYPICAL SECTIONS, SEE SHEETS CS001 THROUGH CS003.
- FOR GEOMETRIC LAYOUT, SEE SHEETS CS101-P1 THROUGH CS106-P6.
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**LEGEND:**

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- PROPOSED ASPHALT PAVEMENT
- EXISTING PAVEMENT
- REINFORCED PCC 5  
CP502
- PCC PAVEMENT JOINT - TYPE A (THICKENED EDGE ISOLATION) 1  
CP501
- PCC PAVEMENT JOINT - TYPE AG (THICKENED EDGE ISOLATION) 2  
CP501
- PCC PAVEMENT JOINT - TYPE A1 (REINFORCED ISOLATION) 3  
CP501
- PCC PAVEMENT JOINT - TYPE C (DOWELED CONTRACTION) 4  
CP501
- PCC PAVEMENT JOINT - TYPE E (DOWELED CONSTRUCTION) 5  
CP501
- PCC PAVEMENT JOINT - TYPE G (JOINT BETWEEN PCC & HMA) 6  
CP501
- PCC ISOLATION JOINT - TYPE AG1 7  
CP501
- PCC ROADWAY JOINT - TYPE E1 3  
CP503
- ASPHALT PAVEMENT GRID
- EXISTING PAVEMENT JOINT
- PROPOSED GEOMETRY OUTSIDE OF CURRENT PHASE (FOR REFERENCE ONLY)
- PROPOSED AIRFIELD BASE CAN FIXTURE, SEE NOTE 9

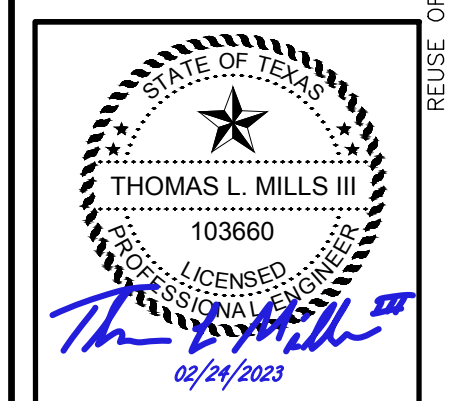


REVISIONS

NO.	DESCRIPTION	DATE	BY
0	ISSUED FOR BID	02/24/2023	SC

**FAA NON-STANDARD TAXIWAYS PROJECT**  
**PAVEMENT JOINTING & REINFORCEMENT PLAN**  
**— PHASE 3**

PROJECT MGR: S. CHILDERS  
 DESIGNER: D. CRAWFORD  
 DRAWN BY: C. MCLAIN  
 CHECKED BY: R. EHTESHAM  
 SCALE: AS SHOWN  
 DATE: 02/24/2023

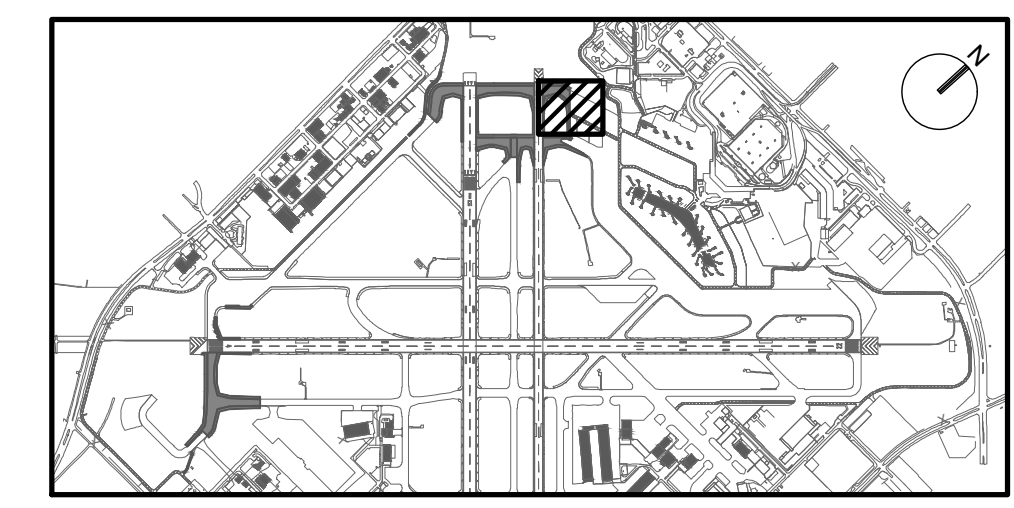
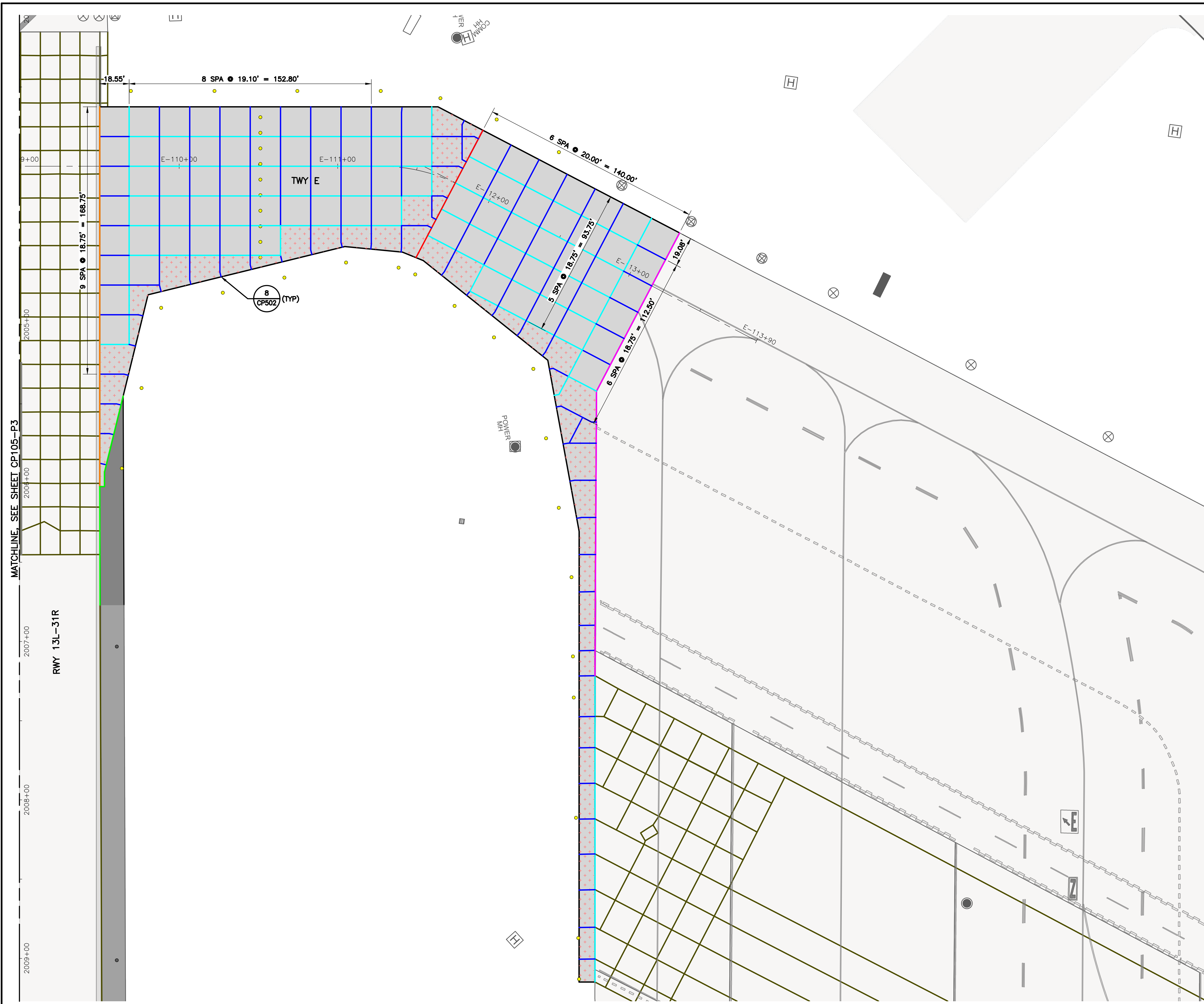


APPROVED BY: \_\_\_\_\_ DATE: \_\_\_\_\_  
 DIRECTOR  
 HOUSTON AIRPORT SYSTEM

PROJECT NO: 770  
 C.I.P. NO: 3-48-0110-044  
 H.A.S. NO: N/A  
 SHEET NO: CP105-P3  
 of

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**KEY MAP**  
NTS

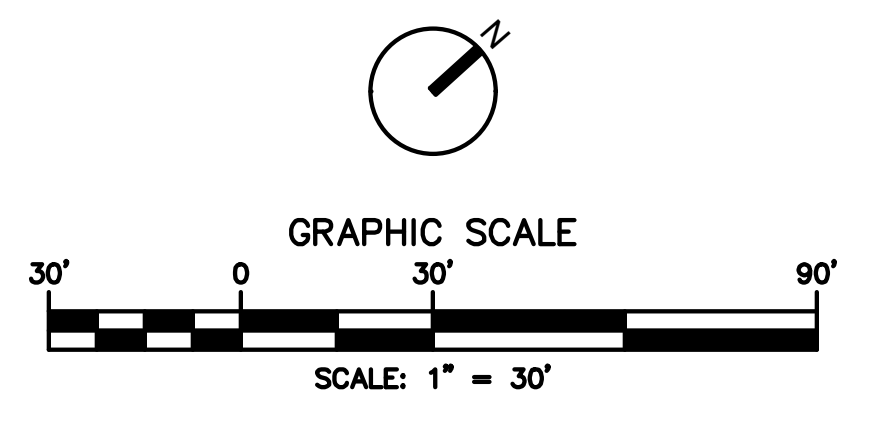
**NOTES:**

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**LEGEND:**

- PROPOSED CONCRETE PAVEMENT (PCC)
- PROPOSED ASPHALT PAVEMENT
- EXISTING PAVEMENT
- REINFORCED PCC **5 CP502**
- PCC PAVEMENT JOINT - TYPE A (THICKENED EDGE ISOLATION)
- PCC PAVEMENT JOINT - TYPE AG (THICKENED EDGE ISOLATION)
- PCC PAVEMENT JOINT - TYPE A1 (REINFORCED ISOLATION)
- PCC PAVEMENT JOINT - TYPE C (DOWELED CONTRACTION)
- PCC PAVEMENT JOINT - TYPE E (DOWELED CONSTRUCTION)
- PCC PAVEMENT JOINT - TYPE G (JOINT BETWEEN PCC & HMA)
- PCC ISOLATION JOINT - TYPE AG1
- PCC ROADWAY JOINT - TYPE E1
- ASPHALT PAVEMENT GRID
- EXISTING PAVEMENT JOINT
- PROPOSED GEOMETRY OUTSIDE OF CURRENT PHASE (FOR REFERENCE ONLY)
- PROPOSED AIRFIELD BASE CAN FIXTURE, SEE NOTE 9

- 1 CP501**
- 2 CP501**
- 3 CP501**
- 4 CP501**
- 5 CP501**
- 6 CP501**
- 7 CP501**
- 3 CP503**

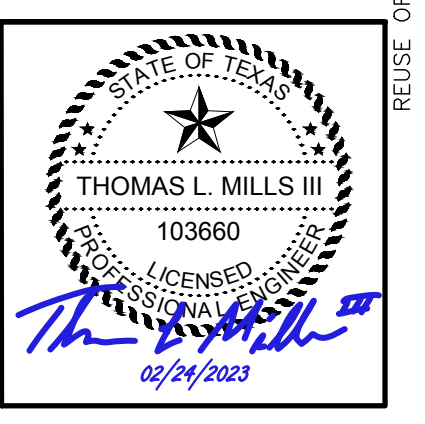


REVISIONS

NO.	DESCRIPTION	DATE	BY
0	ISSUED FOR BID	02/24/2023	SC

**FAA NON-STANDARD TAXIWAYS PROJECT**  
**PAVEMENT JOINTING & REINFORCEMENT PLAN**  
**— PHASE 3**

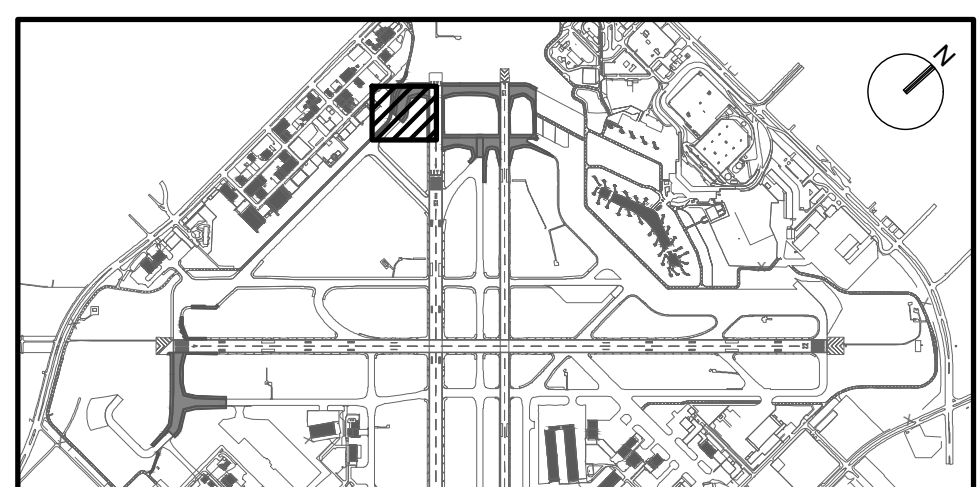
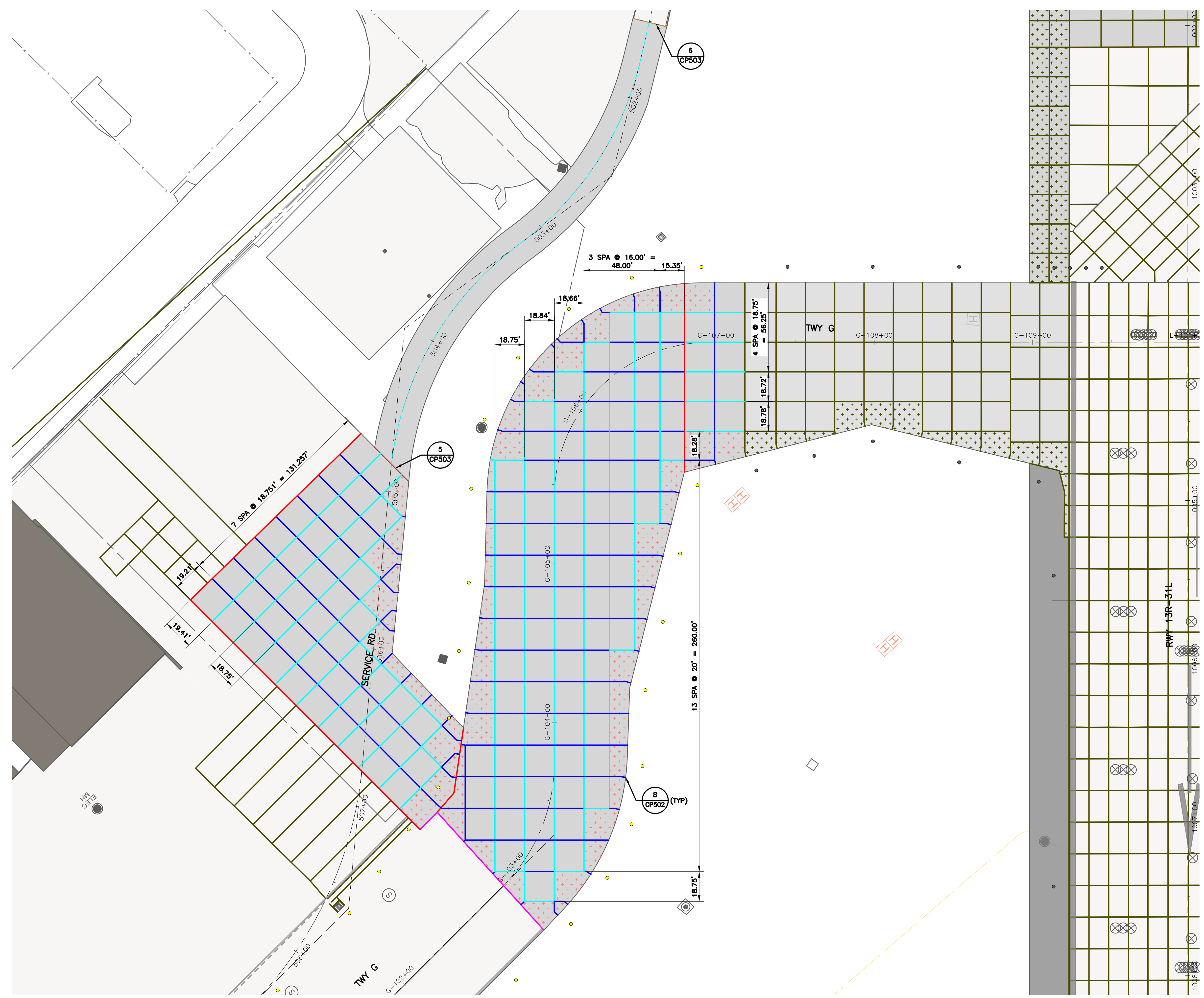
PROJECT MGR: S. CHILDERS  
DESIGNER: D. CRAWFORD  
DRAWN BY: C. MCLAIN  
CHECKED BY: R. EHTESHAM  
SCALE: AS SHOWN  
DATE: 02/24/2023



APPROVED BY: \_\_\_\_\_ DATE: \_\_\_\_\_  
DIRECTOR  
HOUSTON AIRPORT SYSTEM

PROJECT NO: 770  
C.I.P. NO: 3-48-0110-044  
H.A.S. NO: N/A  
SHEET NO: CP106-P3  
of





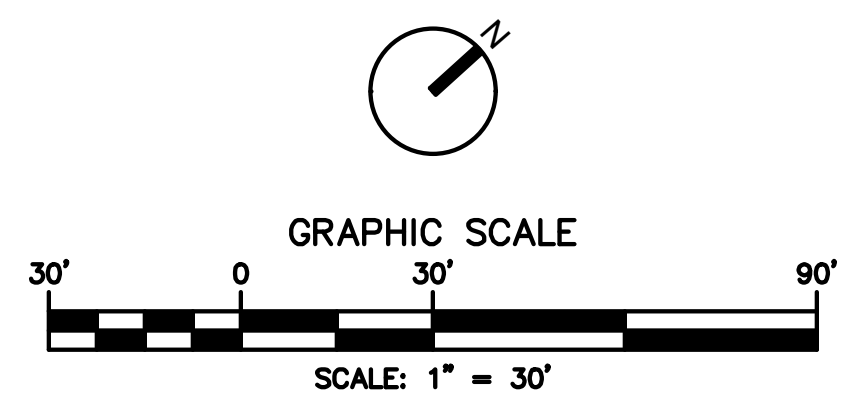
**KEY MAP**  
NTS

**NOTES:**

- FOR PAVEMENT JOINT TYPES AND REINFORCEMENT PLAN, SEE SHEETS CP701-P1 THROUGH CP710-P6.
- FOR PAVEMENT TYPICAL SECTIONS, SEE SHEETS CS001 THROUGH CS003.
- FOR GEOMETRIC LAYOUT, SEE SHEETS CS101-P1 THROUGH CS106-P6.
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- THE CONTRACTOR SHALL SUBMIT A PROPOSED PAVING PLAN TO THE ENGINEER FOR APPROVAL. DOWELS SHALL BE INSTALLED IN ALL CONSTRUCTION JOINTS IN ACCORDANCE WITH SPECIFICATION P-501. INTERIOR SLAB LOCATIONS SHOWN WITH A DOWELED CONSTRUCTION JOINT MAY BE MODIFIED TO A DOWELED CONTRACTION JOINT, WITH APPROVAL OF THE ENGINEER.
- CLEAN UP OF WASTE MATERIAL SHALL BE CONTINUOUS DURING THE JOINT SAWCUTTING OPERATION. CLEAN UP SHALL USE A VACUUM SWEEPER.
- THE CONTRACTOR SHALL LOCATE AND PROTECT ALL UTILITIES AND PAVED AREAS TO REMAIN. ALL UTILITIES ARE TO REMAIN, UNLESS OTHERWISE NOTED. DEMOLITION ACTIVITIES SHALL BE CAREFULLY CONTROLLED TO PREVENT DAMAGE TO ADJACENT CONCRETE PAVEMENT, THE UNDERLYING MATERIAL, OR EXISTING STRUCTURES TO REMAIN IN-PLACE. ANY DAMAGE TO EXISTING STRUCTURES OR PAVEMENT SHALL BE IMMEDIATELY REPORTED TO REPRESENTATIVE AND REPAIRED WITH APPROVED MATERIALS AND PROCEDURES AT THE CONTRACTORS EXPENSE.
- CARE SHALL BE TAKEN NOT TO UNDERMINE THE EXISTING PAVEMENT TO REMAIN.
- STRUCTURES AND LIGHTS SHOWN ARE FOR REFERENCE ONLY. SEE RESPECTIVE SHEETS FOR DETAILED DESIGN INFORMATION AND LEGEND.

**LEGEND:**

- PROPOSED CONCRETE PAVEMENT (PCC)
- PROPOSED ASPHALT PAVEMENT
- EXISTING PAVEMENT
- REINFORCED PCC **5** CP502
- PCC PAVEMENT JOINT - TYPE A (THICKENED EDGE ISOLATION) **1** CP501
- PCC PAVEMENT JOINT - TYPE AG (THICKENED EDGE ISOLATION) **2** CP501
- PCC PAVEMENT JOINT - TYPE A1 (REINFORCED ISOLATION) **3** CP501
- PCC PAVEMENT JOINT - TYPE C (DOWELED CONTRACTION) **4** CP501
- PCC PAVEMENT JOINT - TYPE E (DOWELED CONTRACTION) **5** CP501
- PCC PAVEMENT JOINT - TYPE G (JOINT BETWEEN PCC & HMA) **6** CP501
- PCC ISOLATION JOINT - TYPE AG1 **7** CP501
- PCC ROADWAY JOINT - TYPE E1 **3** CP503
- ASPHALT PAVEMENT GRID
- EXISTING PAVEMENT JOINT
- PROPOSED GEOMETRY OUTSIDE OF CURRENT PHASE (FOR REFERENCE ONLY)
- PROPOSED AIRFIELD BASE CAN FIXTURE, SEE NOTE 9

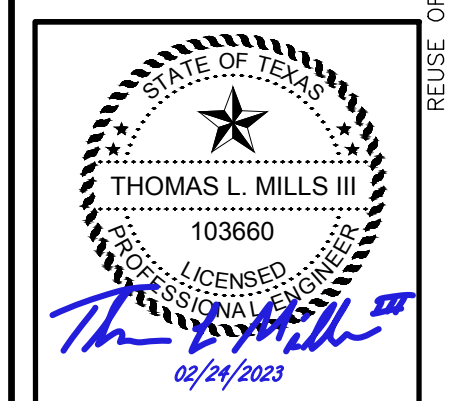


REVISIONS

NO.	DESCRIPTION	DATE	BY
0	ISSUED FOR BID	02/24/2023	SC

**FAA NON-STANDARD TAXIWAYS PROJECT**  
**PAVEMENT JOINTING & REINFORCEMENT PLAN**  
**— PHASE 4**

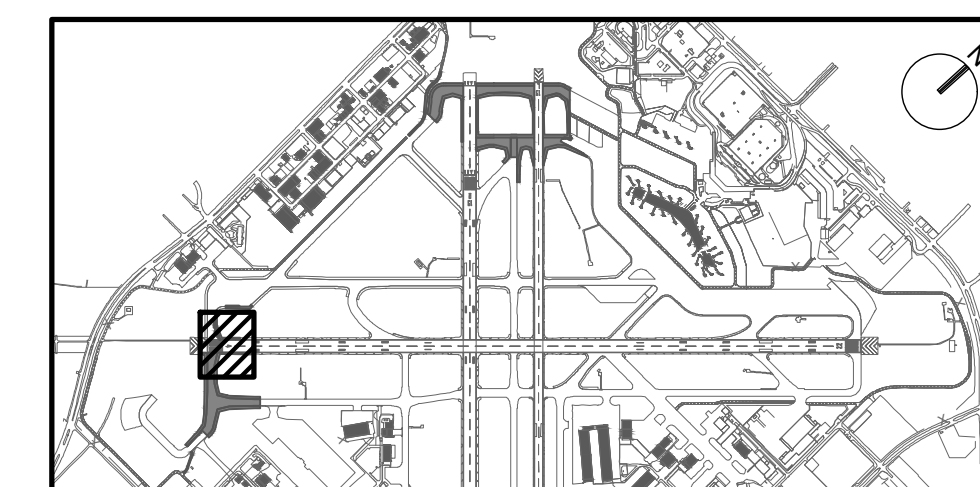
PROJECT MGR: S. CHILDERS  
 DESIGNER: D. CRAWFORD  
 DRAWN BY: C. MCLAIN  
 CHECKED BY: R. EHTESHAM  
 SCALE: AS SHOWN  
 DATE: 02/24/2023



APPROVED BY: \_\_\_\_\_ DATE: \_\_\_\_\_  
 DIRECTOR  
 HOUSTON AIRPORT SYSTEM

PROJECT NO: 770  
 C.I.P. NO: 3-48-0110-044  
 H.A.S. NO: N/A  
 SHEET NO: CP107-P4  
 of





**KEY MAP**  
NTS

**NOTES:**

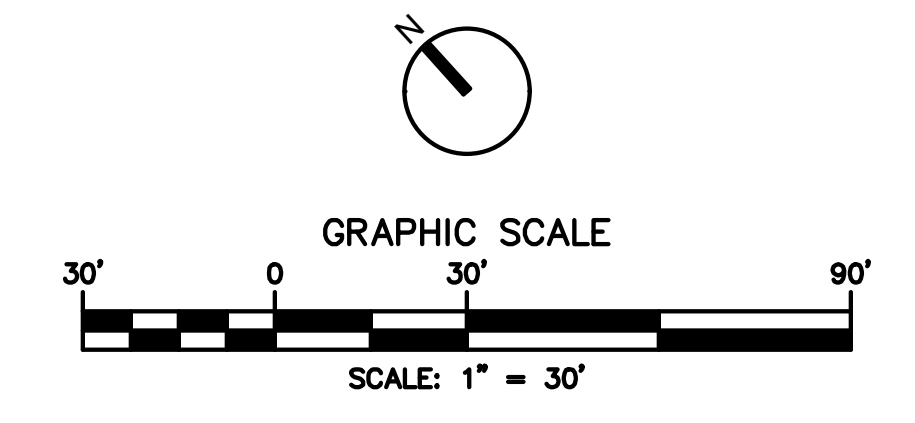
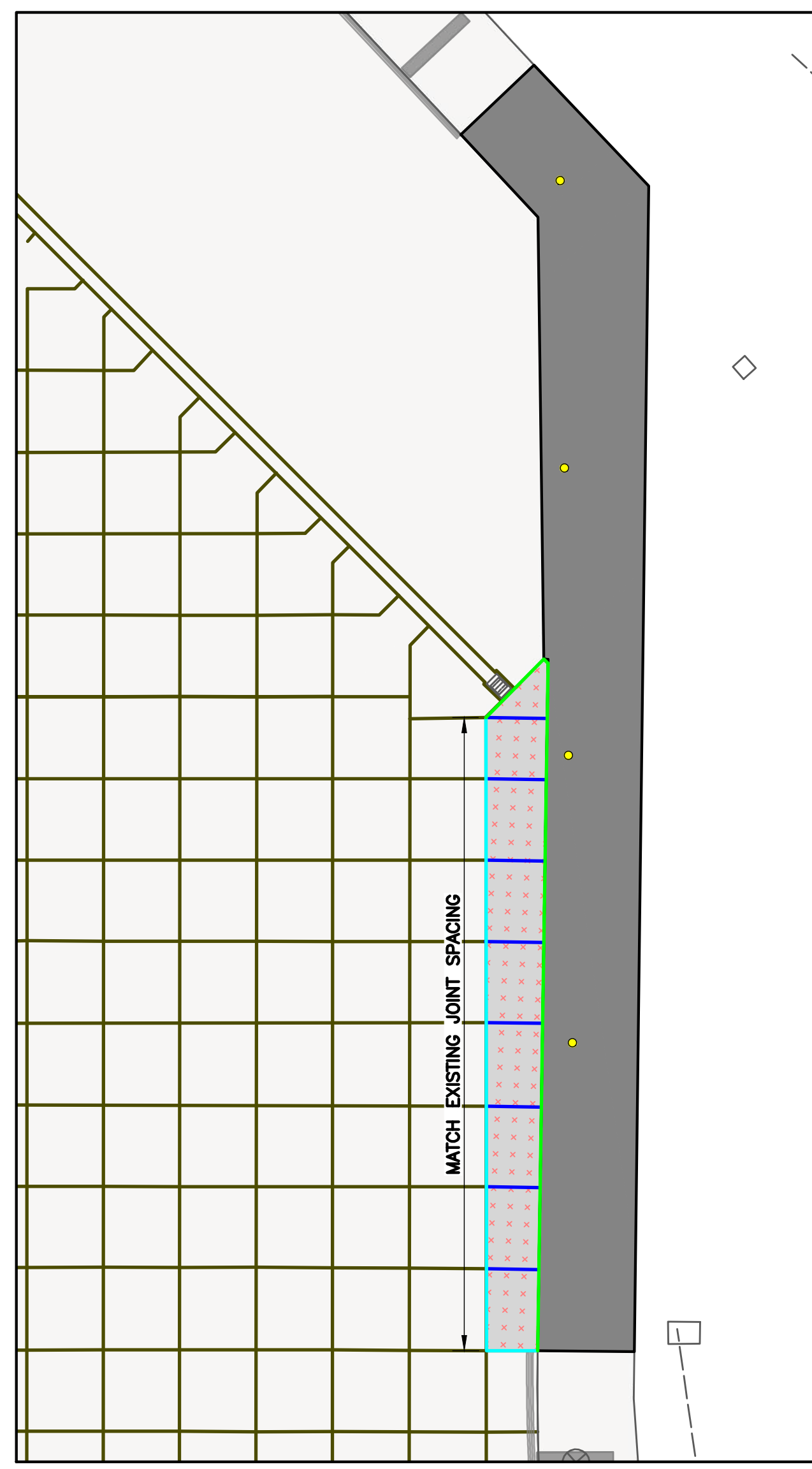
- FOR PAVEMENT JOINT TYPES AND REINFORCEMENT PLAN, SEE SHEETS CP701-P1 THROUGH CP710-P6.
- FOR PAVEMENT TYPICAL SECTIONS, SEE SHEETS CS001 THROUGH CS003.
- FOR GEOMETRIC LAYOUT, SEE SHEETS CS101-P1 THROUGH CS106-P6.
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**LEGEND:**

- PROPOSED CONCRETE PAVEMENT (PCC)
- PROPOSED ASPHALT PAVEMENT
- EXISTING PAVEMENT
- REINFORCED PCC **5** CP502
- PCC PAVEMENT JOINT - TYPE A (THICKENED EDGE ISOLATION)
- PCC PAVEMENT JOINT - TYPE AG (THICKENED EDGE ISOLATION)
- PCC PAVEMENT JOINT - TYPE A1 (REINFORCED ISOLATION)
- PCC PAVEMENT JOINT - TYPE C (DOWELED CONTRACTION)
- PCC PAVEMENT JOINT - TYPE E (DOWELED CONTRACTION)
- PCC PAVEMENT JOINT - TYPE G (JOINT BETWEEN PCC & HMA)
- PCC ISOLATION JOINT - TYPE AG1
- PCC ROADWAY JOINT - TYPE E1
- ASPHALT PAVEMENT GRID
- EXISTING PAVEMENT JOINT
- PROPOSED GEOMETRY OUTSIDE OF CURRENT PHASE (FOR REFERENCE ONLY)
- PROPOSED AIRFIELD BASE CAN FIXTURE, SEE NOTE 9

- 1** CP501
- 2** CP501
- 3** CP501
- 4** CP501
- 5** CP501
- 6** CP501
- 7** CP501
- 3** CP503

**A** INSET

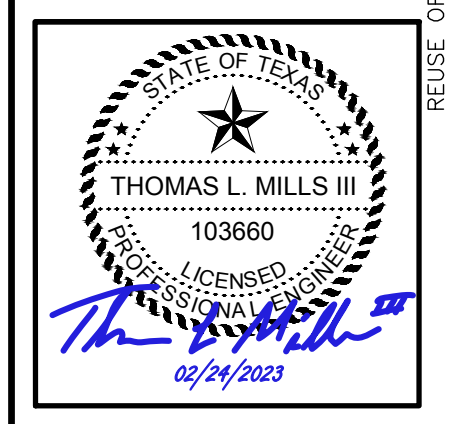


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NO.	DESCRIPTION	DATE	BY
0	ISSUED FOR BID	02/24/2023	SC

**FAA NON-STANDARD TAXIWAYS PROJECT**  
**PAVEMENT JOINTING & REINFORCEMENT PLAN**  
**— PHASE 5**

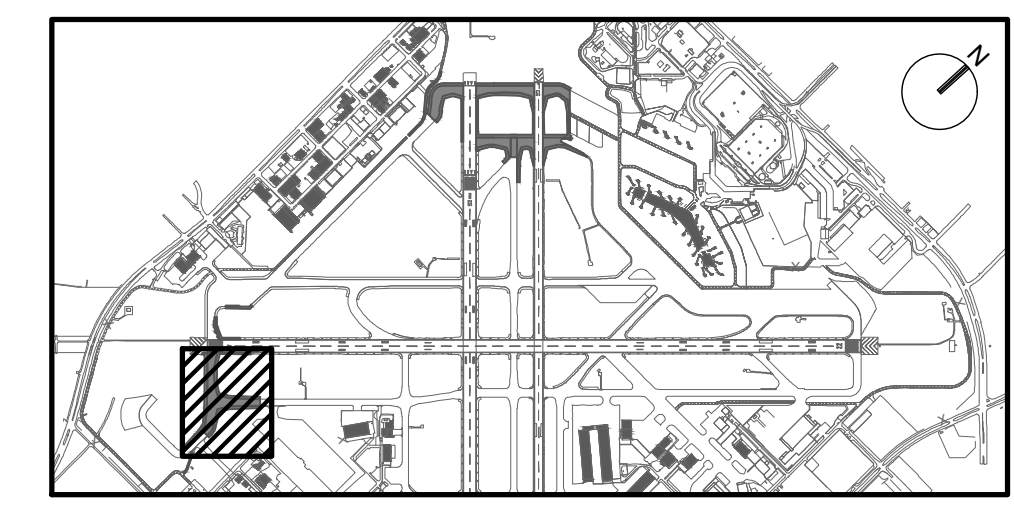
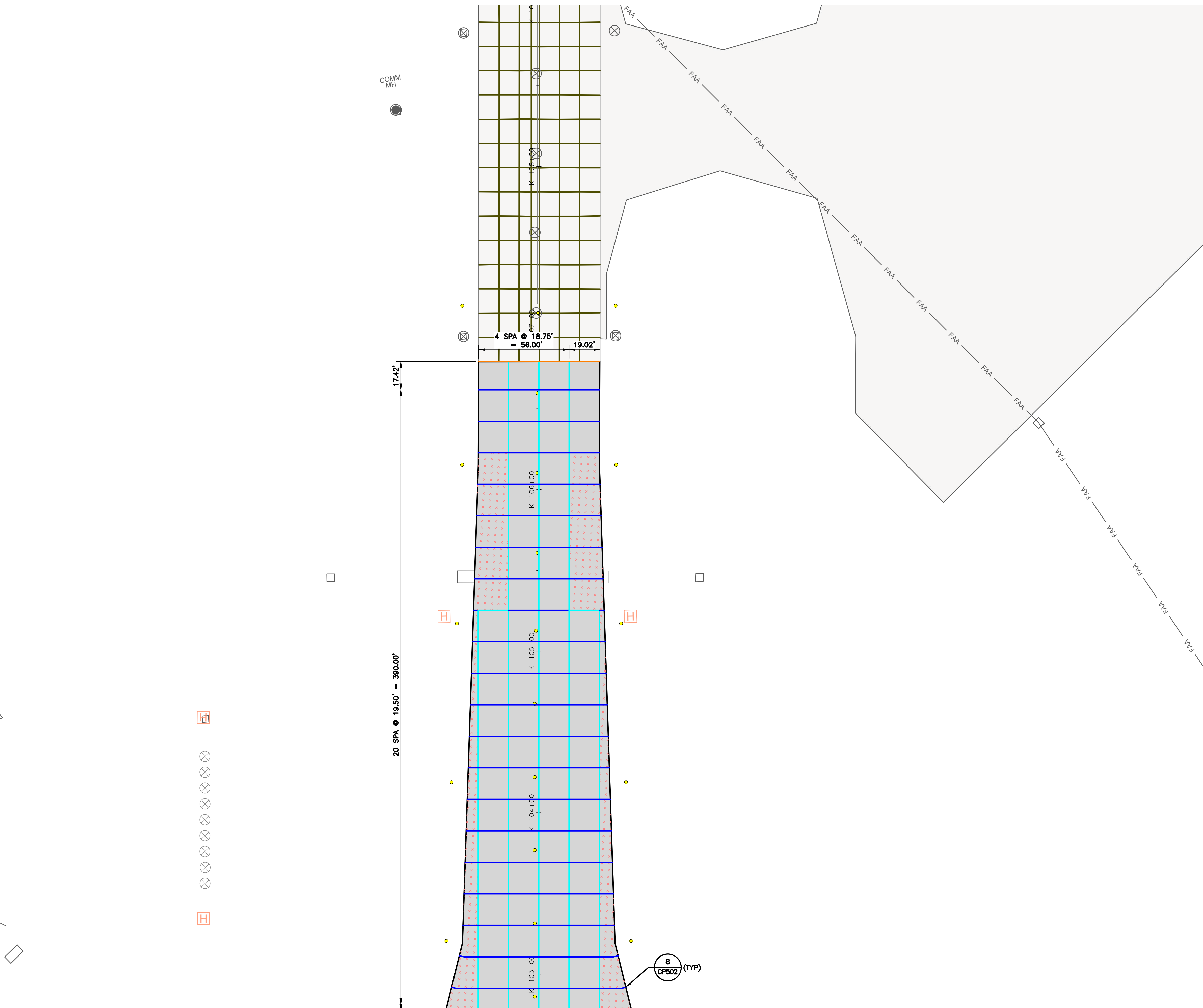
PROJECT MGR: S. CHILDERS  
DESIGNER: D. CRAWFORD  
DRAWN BY: C. MCLAIN  
CHECKED BY: R. EHTESHAM  
SCALE: AS SHOWN  
DATE: 02/24/2023



APPROVED BY: \_\_\_\_\_ DATE: \_\_\_\_\_  
DIRECTOR  
HOUSTON AIRPORT SYSTEM

PROJECT NO: 770  
C.I.P. NO: 3-48-0110-044  
H.A.S. NO: N/A  
SHEET NO: CP108-P5  
of





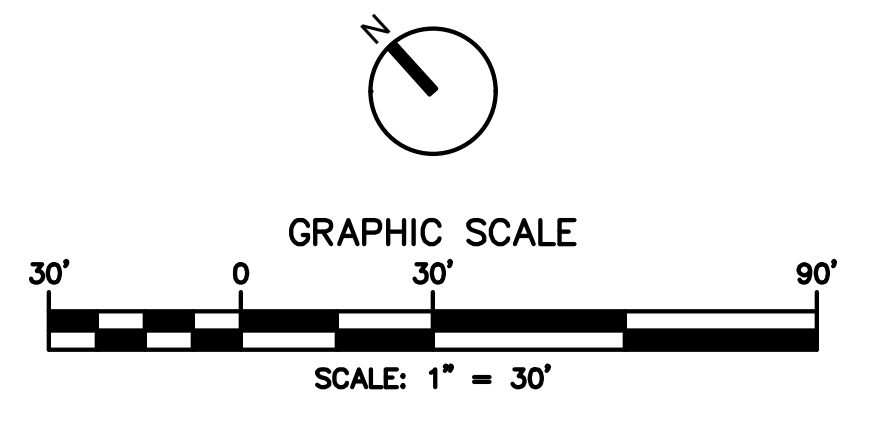
**KEY MAP**  
NTS

**NOTES:**

- FOR PAVEMENT JOINT TYPES AND REINFORCEMENT PLAN, SEE SHEETS CP701-P1 THROUGH CP710-P6.
- FOR PAVEMENT TYPICAL SECTIONS, SEE SHEETS CS001 THROUGH CS003.
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**LEGEND:**

- PROPOSED CONCRETE PAVEMENT (PCC)
- PROPOSED ASPHALT PAVEMENT
- EXISTING PAVEMENT
- REINFORCED PCC **5** CP502
- PCC PAVEMENT JOINT - TYPE A (THICKENED EDGE ISOLATION) **1** CP501
- PCC PAVEMENT JOINT - TYPE AG (THICKENED EDGE ISOLATION) **2** CP501
- PCC PAVEMENT JOINT - TYPE A1 (REINFORCED ISOLATION) **3** CP501
- PCC PAVEMENT JOINT - TYPE C (DOWELED CONTRACTION) **4** CP501
- PCC PAVEMENT JOINT - TYPE E (DOWELED CONSTRUCTION) **5** CP501
- PCC PAVEMENT JOINT - TYPE G (JOINT BETWEEN PCC & HMA) **6** CP501
- PCC ISOLATION JOINT - TYPE AG1 **7** CP501
- PCC ROADWAY JOINT - TYPE E1 **3** CP503
- ASPHALT PAVEMENT GRID
- EXISTING PAVEMENT JOINT
- PROPOSED GEOMETRY OUTSIDE OF CURRENT PHASE (FOR REFERENCE ONLY)
- PROPOSED AIRFIELD BASE CAN FIXTURE, SEE NOTE 9



**Jacobs**  
JACOBS ENGINEERING GROUP INC.  
5995 ROGERDALE ROAD  
HOUSTON, TEXAS 77072  
+1-832-351-6000  
WWW.JACOBS.COM  
TEXAS P.E. FIRM F-2966

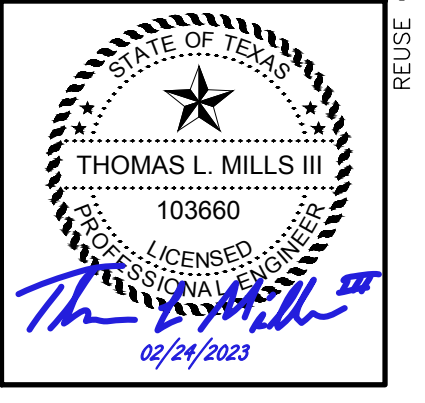
VERIFY SCALE  
BAR IS ONE INCH ON  
ORIGINAL DRAWING.  
0 1"

**REVISIONS**

NO.	DESCRIPTION	DATE	BY
0	ISSUED FOR BID	02/24/2023	SC

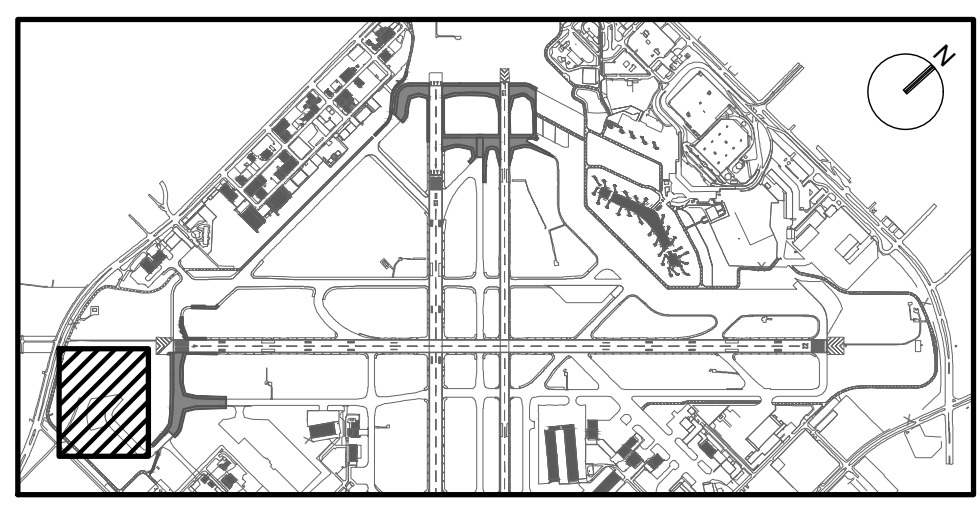
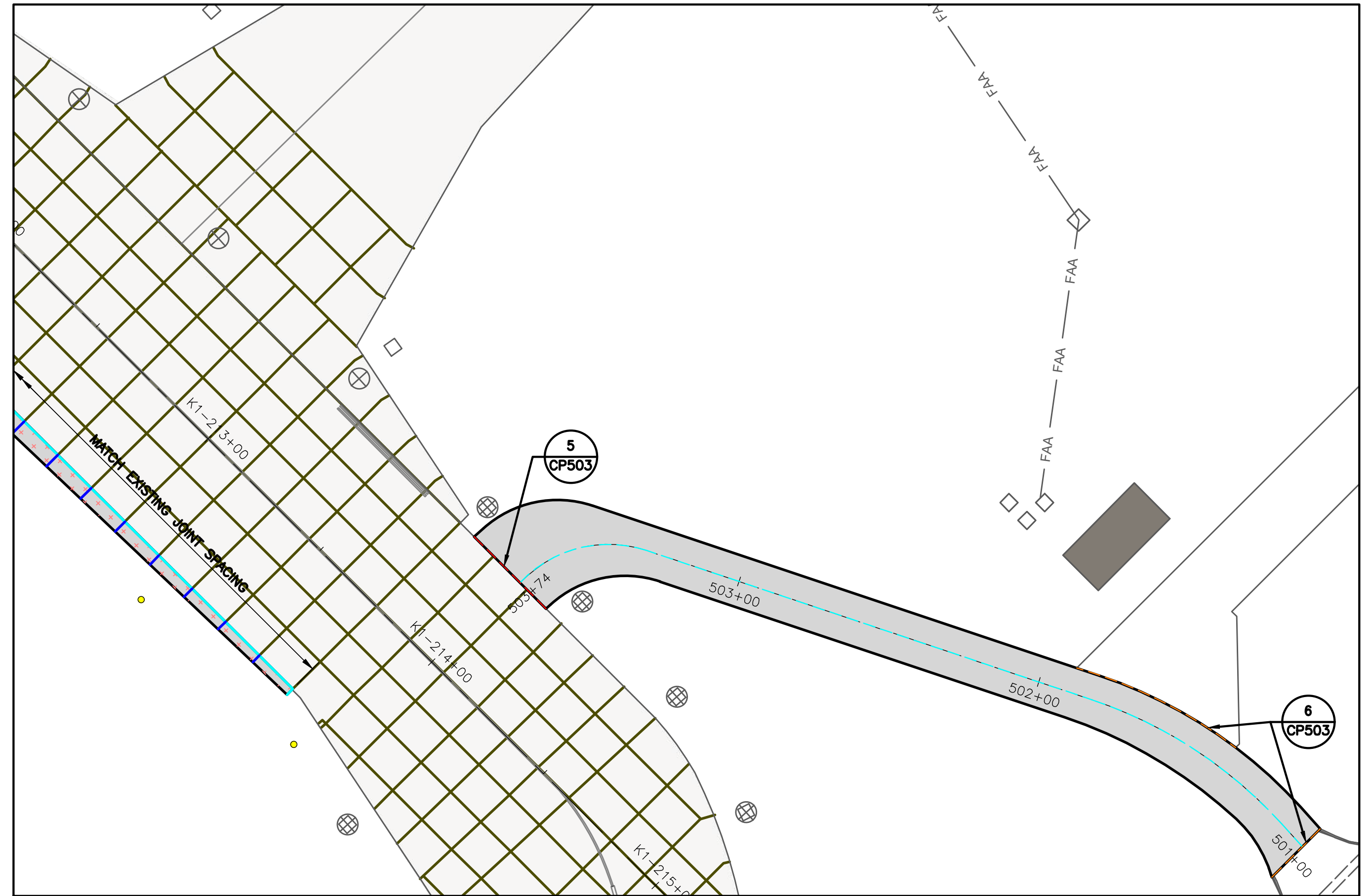
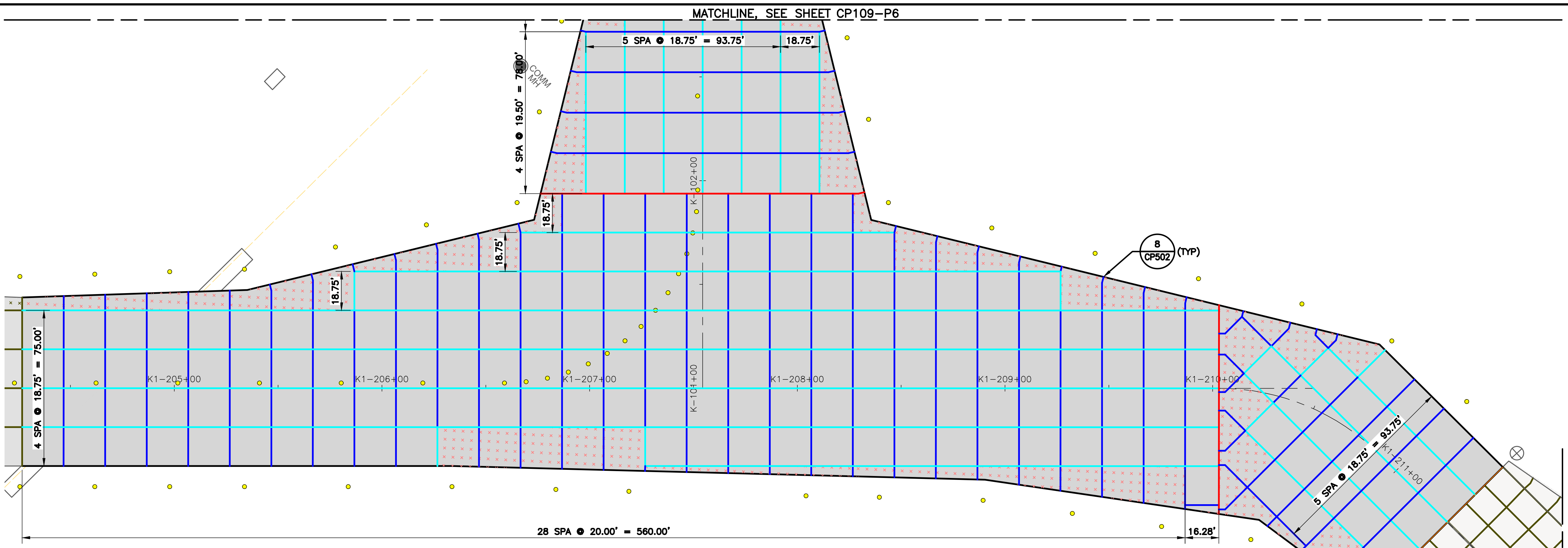
FAA NON-STANDARD TAXIWAYS PROJECT  
 PAVEMENT JOINTING & REINFORCEMENT PLAN  
 - PHASE 6A & 6B

PROJECT MGR: S. CHILDERS  
 DESIGNER: D. CRAWFORD  
 DRAWN BY: C. MCCLAIN  
 CHECKED BY: R. EHTESHAM  
 SCALE: AS SHOWN  
 DATE: 02/24/2023



APPROVED BY: \_\_\_\_\_ DATE: \_\_\_\_\_  
 DIRECTOR  
 HOUSTON AIRPORT SYSTEM  
 PROJECT NO: 770  
 C.I.P. NO: 3-48-0110-044  
 H.A.S. NO: N/A  
 SHEET NO: CP109-P6  
 of





**HOUSTON AIRPORT SYSTEM**  
**WILLIAM P. HOBBY AIRPORT**  
 HOUSTON TEXAS

**Jacobs**  
 JACOBS ENGINEERING GROUP INC.  
 5995 ROGERDALE ROAD  
 HOUSTON, TEXAS 77072  
 +1-832-351-6000  
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VERIFY SCALE  
 BAR IS ONE INCH ON  
 ORIGINAL DRAWING.  
 0 1"

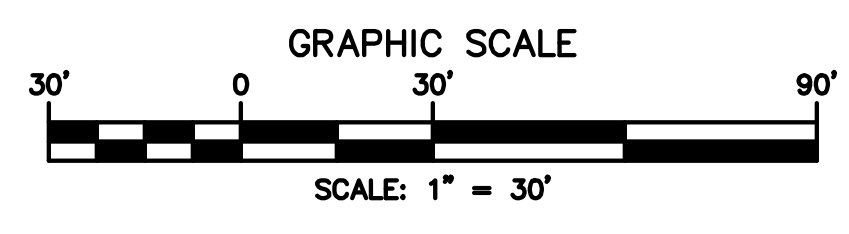
**NOTES:**

- FOR PAVEMENT JOINT TYPES AND REINFORCEMENT PLAN, SEE SHEETS CP701-P1 THROUGH CP710-P6.
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**LEGEND:**

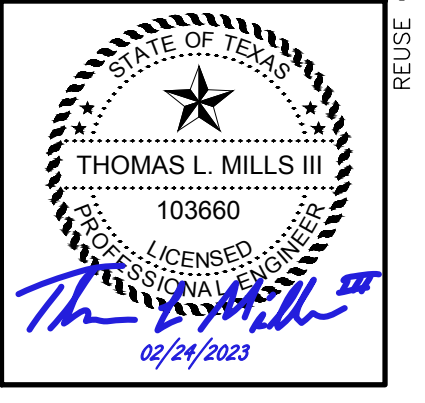
- PROPOSED CONCRETE PAVEMENT (PCC)
- PROPOSED ASPHALT PAVEMENT
- EXISTING PAVEMENT
- REINFORCED PCC (5 CP502)
- PCC PAVEMENT JOINT - TYPE A (THICKENED EDGE ISOLATION)
- PCC PAVEMENT JOINT - TYPE AG (THICKENED EDGE ISOLATION)
- PCC PAVEMENT JOINT - TYPE A1 (REINFORCED ISOLATION)
- PCC PAVEMENT JOINT - TYPE C (DOWELED CONTRACTION)
- PCC PAVEMENT JOINT - TYPE E (DOWELED CONSTRUCTION)
- PCC PAVEMENT JOINT - TYPE G (JOINT BETWEEN PCC & HMA)
- PCC ISOLATION JOINT - TYPE AG1
- PCC ROADWAY JOINT - TYPE E1
- ASPHALT PAVEMENT GRID
- EXISTING PAVEMENT JOINT
- PROPOSED GEOMETRY OUTSIDE OF CURRENT PHASE (FOR REFERENCE ONLY)
- PROPOSED AIRFIELD BASE CAN FIXTURE, SEE NOTE 9

- 1 CP501
- 2 CP501
- 3 CP501
- 4 CP501
- 5 CP501
- 6 CP501
- 7 CP501
- 3 CP503



FAA NON-STANDARD TAXIWAYS PROJECT  
 PAVEMENT JOINTING & REINFORCEMENT PLAN  
 - PHASE 6A & 6B

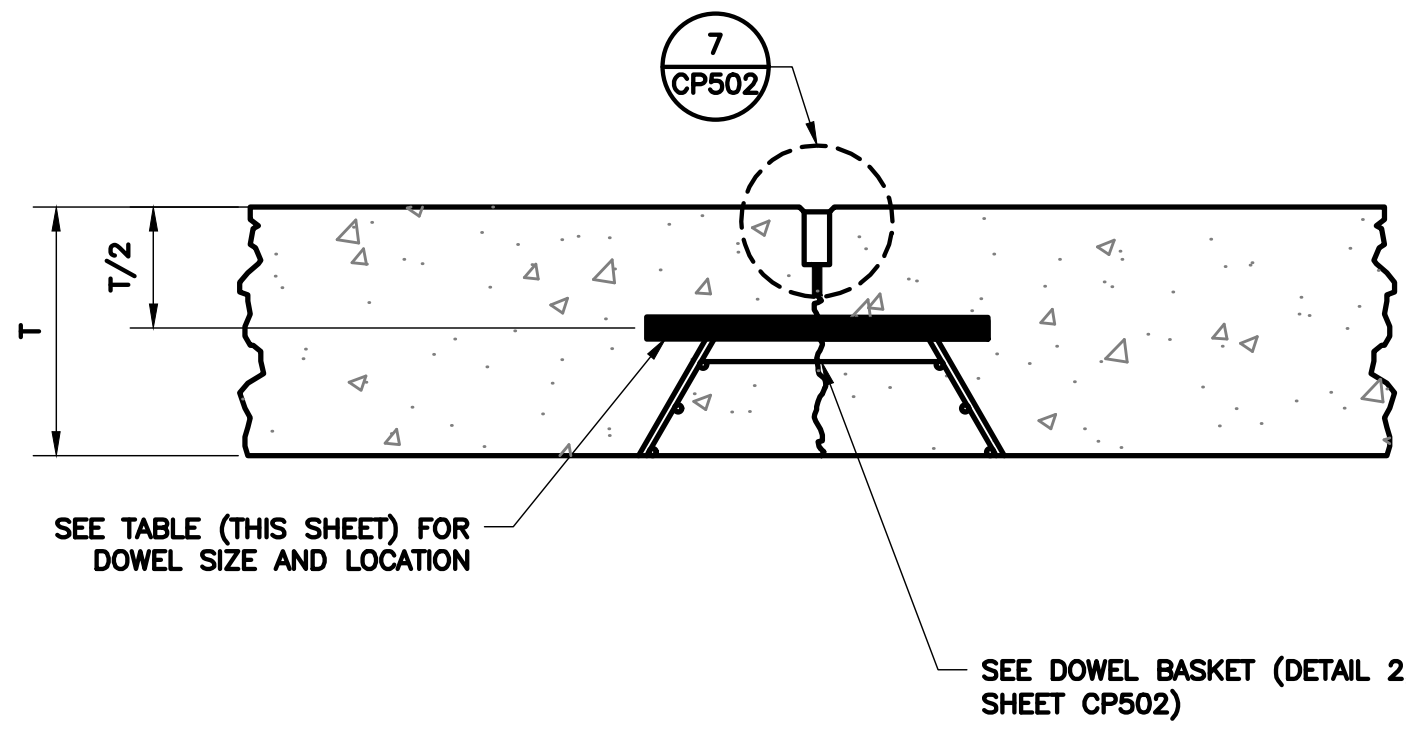
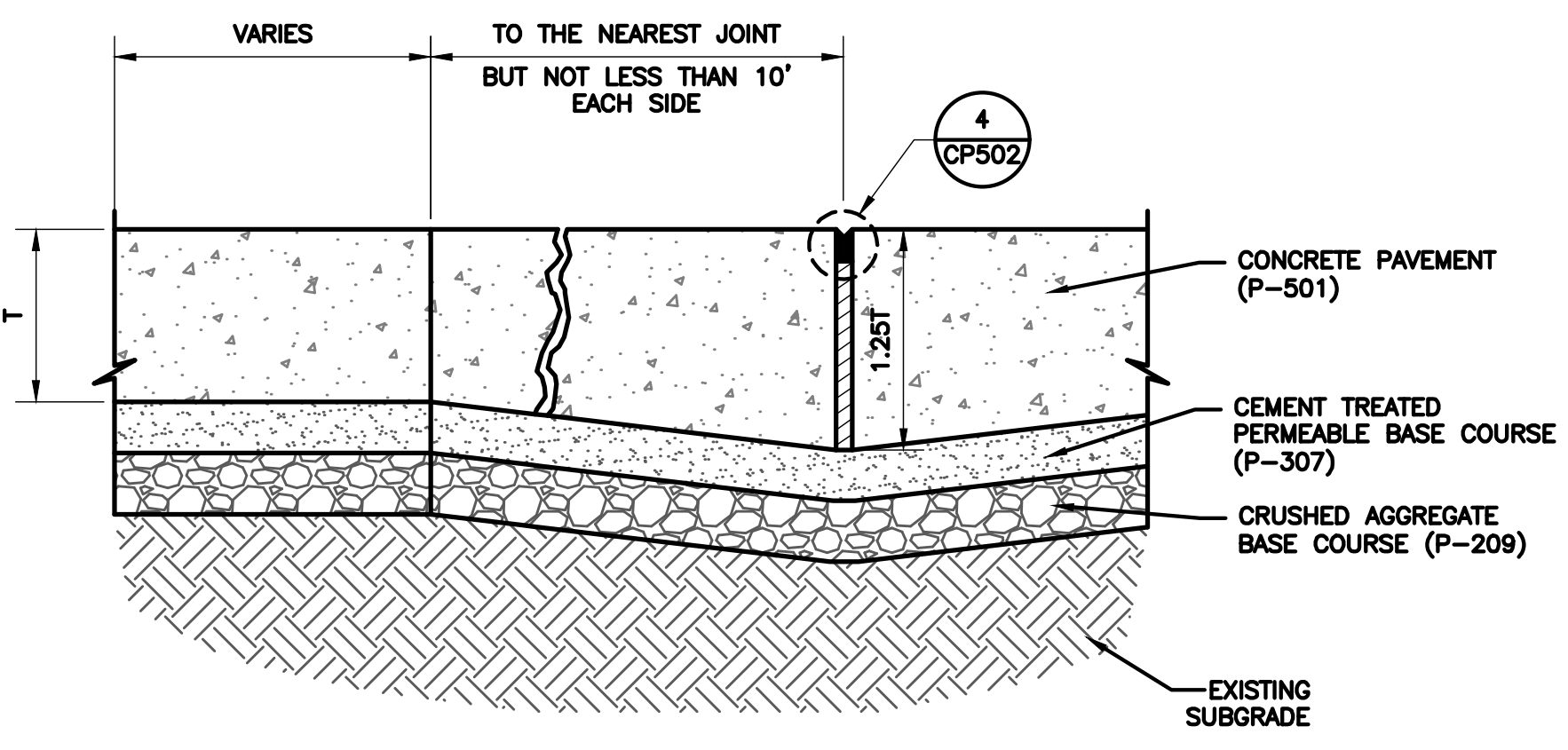
PROJECT MGR: S. CHILDERS  
 DESIGNER: D. CRAWFORD  
 DRAWN BY: C. MCLAIN  
 CHECKED BY: R. EHTESHAM  
 SCALE: AS SHOWN  
 DATE: 02/24/2023



APPROVED BY: \_\_\_\_\_ DATE: \_\_\_\_\_  
 DIRECTOR  
 HOUSTON AIRPORT SYSTEM

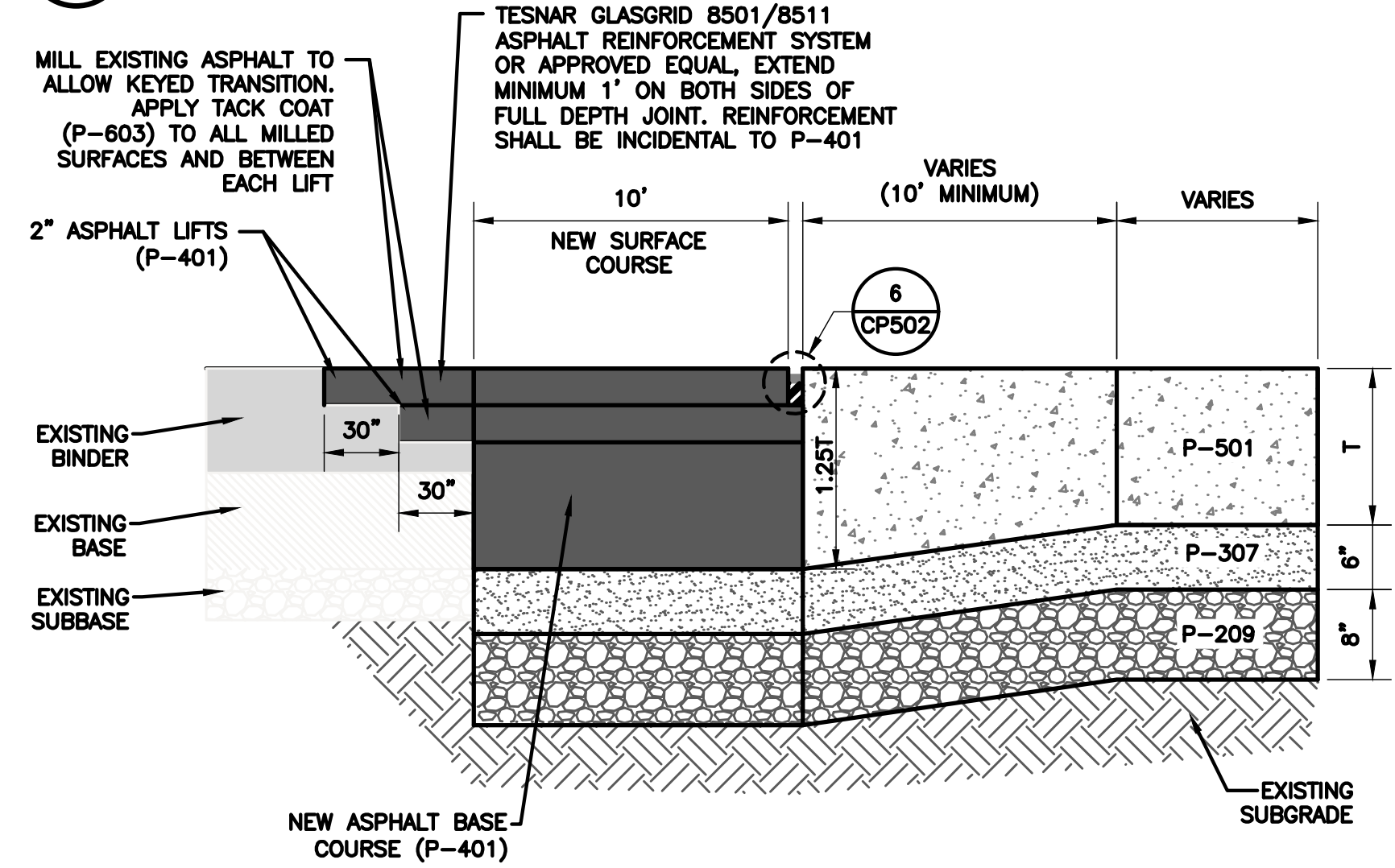
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 C.I.P. NO: 3-48-0110-044  
 H.A.S. NO: N/A  
 SHEET NO: CP110-P6  
 of



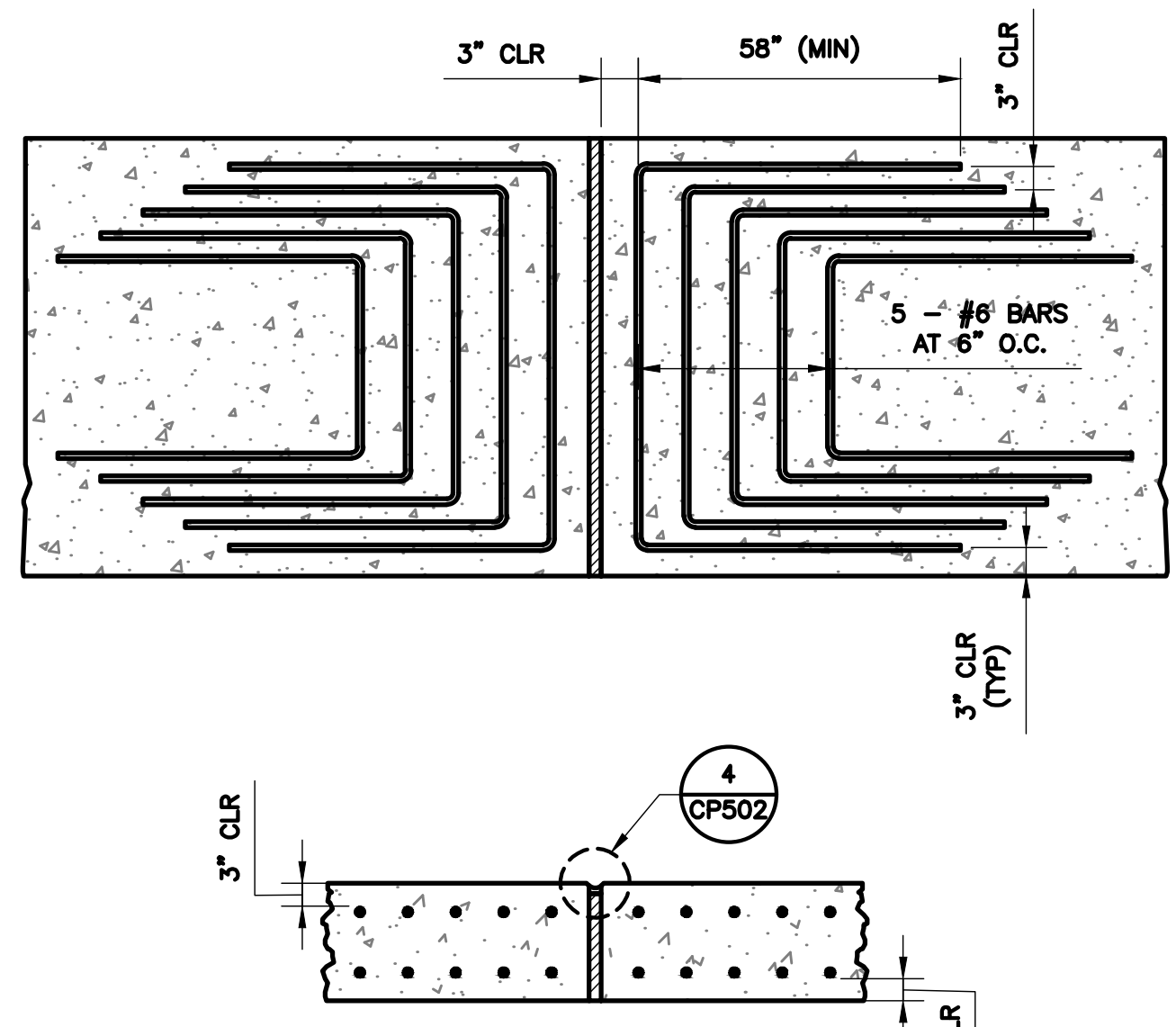


**4 TYPE 'C' DOWELED CONTRACTION JOINT**  
N.T.S.

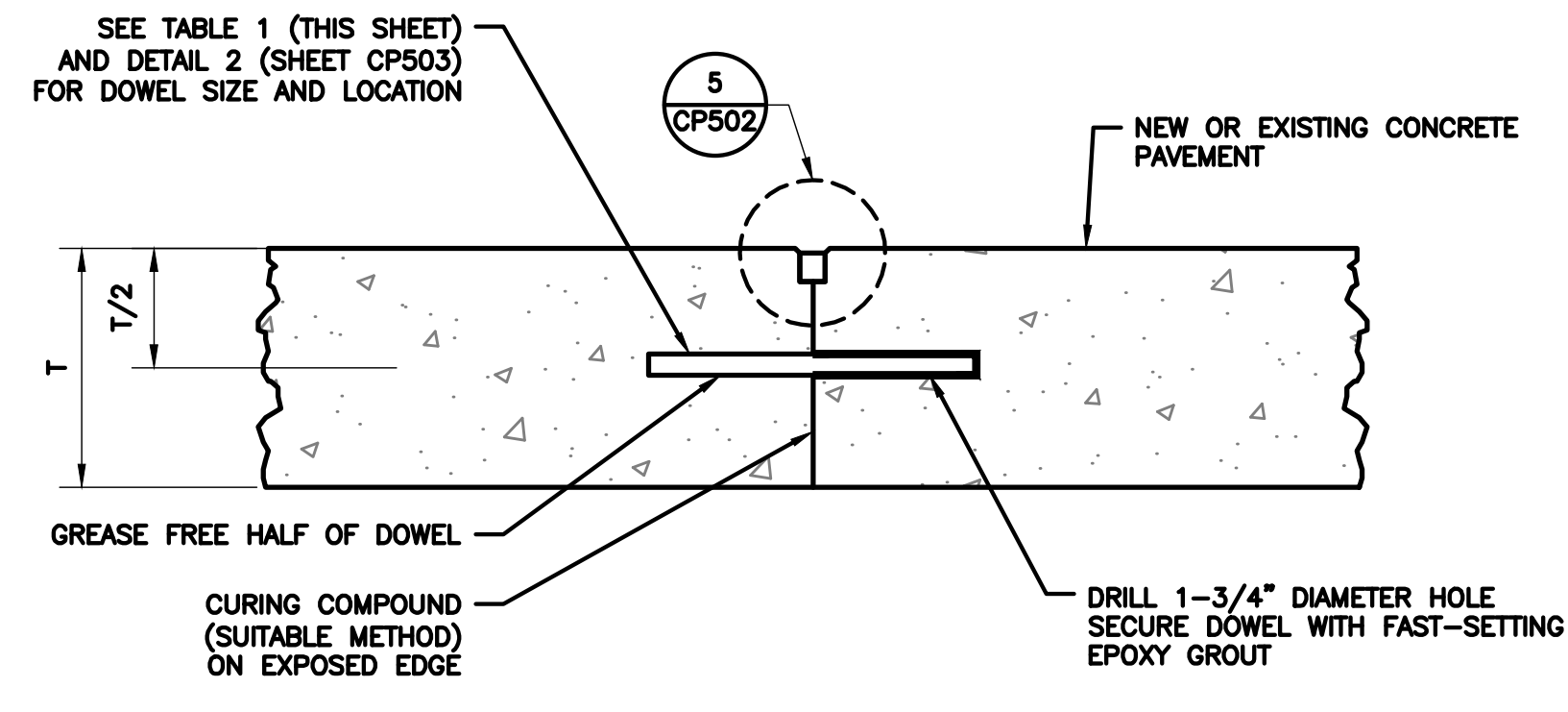
**1 TYPE 'A' THICKENED EDGE ISOLATION JOINT PCC-PCC TRANSITION**  
N.T.S.



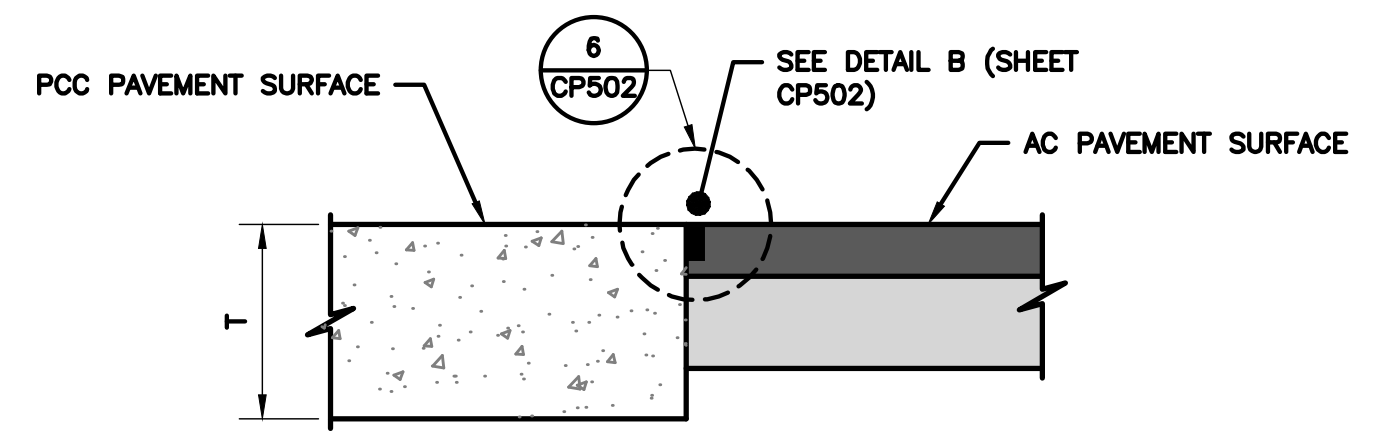
**2 TYPE 'AG' THICKENED EDGE ISOLATION JOINT PCC-ASPHALT TRANSITION**  
N.T.S.



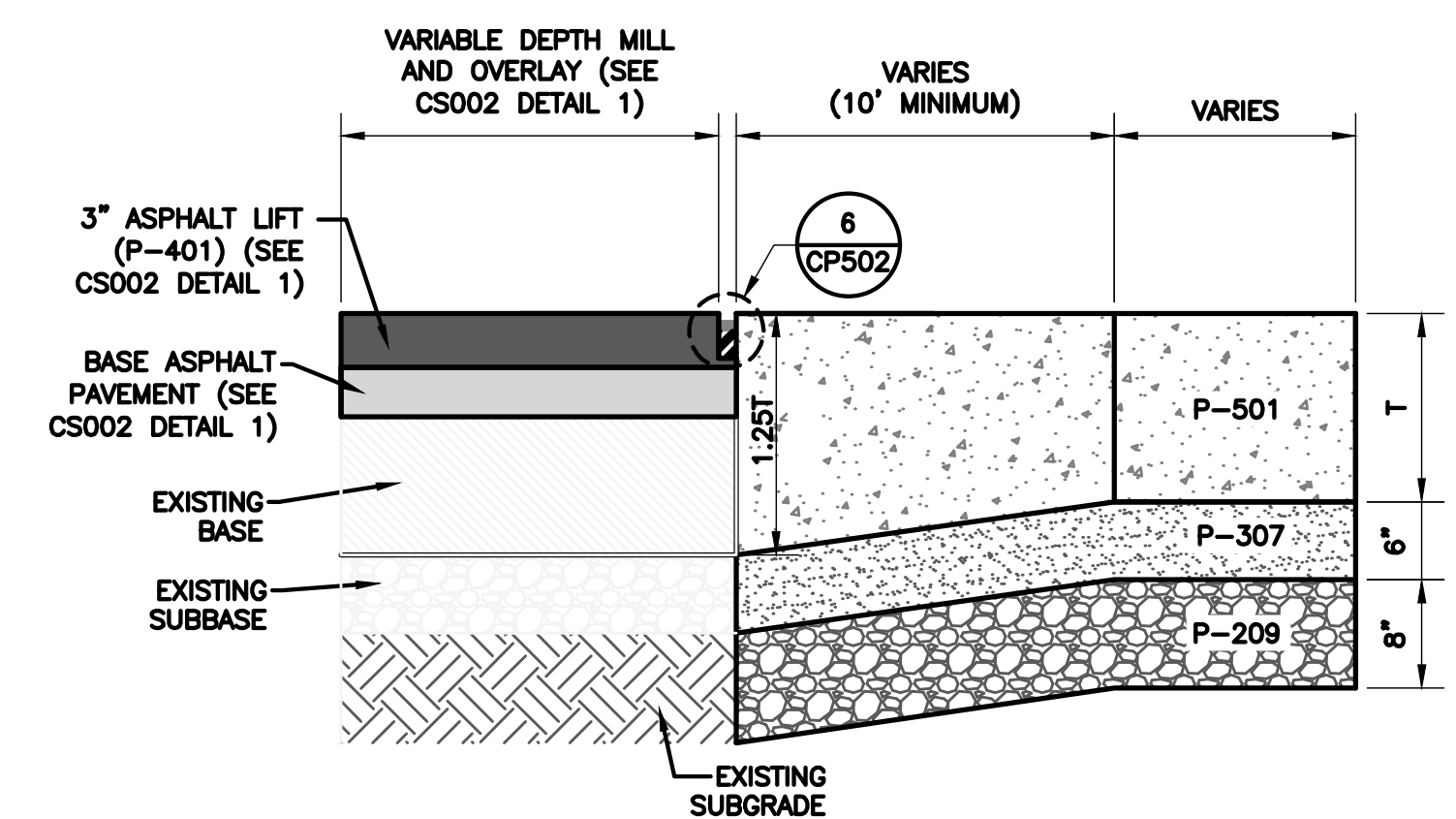
**3 TYPE 'A-1' REINFORCED ISOLATION JOINT**  
N.T.S.



**5 TYPE 'E' DOWELED CONSTRUCTION JOINT**  
N.T.S.



**6 TYPE 'G' BUTT CONSTRUCTION JOINT**  
N.T.S.



**7 TYPE 'AG-1' THICKENED EDGE ISOLATION JOINT PCC-ASPHALT TRANSITION**  
N.T.S.

**GENERAL NOTES:**

1. ALL JOINT CONSTRUCTION AND MATERIALS SHALL BE IN ACCORDANCE WITH SPECIFICATION P-501 OR AS NOTED. THE COST OF ALL JOINT CONSTRUCTION IS INCIDENTAL TO P-501.
2. THE CONTRACTOR MAY INTERCHANGE CONSTRUCTION JOINTS AND CONTRACTION JOINTS PER OWNER APPROVED SHOP DRAWINGS WITH ENGINEER'S APPROVAL.
3. EDGES OF CONCRETE SLABS SHALL BE COVERED WITH AN APPROVED CURING MATERIAL AT THE SAME TIME AS THE SURFACE IS CURED. AT FORMED LOCATIONS, SLAB SIDES SHALL BE CURED WHEN FORMS ARE REMOVED.
4. THE CONTRACTOR SHALL MAKE SPECIFICATIONS TO KEEP THE FINISH CONCRETE SURFACE FREE OF FOREIGN MATERIAL (I.E. AGGREGATE, OILS, ETC.) THROUGHOUT THE PROJECT. SWEEPERS, VACUUM TRUCKS, ETC. SHALL BE UTILIZED AS NECESSARY BY THE CONTRACTOR AND/OR AS DIRECTED BY THE REPRESENTATIVE TO MAINTAIN THIS PROVISION.
5. CONCRETE JOINT SPALLS, POP OUTS, AND OTHER CONCRETE SURFACE FAILURES, AS DEEMED EXCESSIVE BY THE REPRESENTATIVE AND WHICH CANNOT BE REASONABLY FILLED IN WITH SEALANT MATERIAL, SHALL BE REPAIRED BY THE CONTRACTOR AT NO EXPENSE TO THE AIRPORT AUTHORITY IN ACCORDANCE WITH SPECIFICATION P-501.
6. ANY PAVEMENT SHOWN WITH THICKENED EDGE JOINTS WILL BE PAID FOR AS NOMINAL THICKNESS (T) PAVEMENT.
7. JOINTS SHALL NOT INTERSECT THE EDGE OF PAVEMENT NOR ANY OTHER JOINT AT AN ANGLE OF LESS THAN 90°, UNLESS OTHERWISE NOTED.

**DRILLING AND BAR INSTALLATION NOTES:**

1. DOWELS SHALL BE FULLY SHOP EPOXY COATED PER ASTM A1078.
2. PRIOR TO INSTALLATION IN CONTRACTION JOINTS, DOWELS SHALL BE LIGHTLY GREASED WITH A THIN COAT OF HIGH MELTING POINT GREASE OR APPROVED EQUAL AS INDICATED. IN CONTRACTION JOINTS, ONLY THE FREE END SHALL BE GREASED, AND ONLY AFTER INSERTION AND GROUTING INTO PLACE.
3. ALL CONSTRUCTION JOINT DOWELS SHALL BE GANG-DRILLED AND EPOXY GROUTED. INSERTION EQUIPMENT WILL NOT BE ALLOWED.
4. DOWEL DRILLING METHOD SHALL BE CAPABLE OF MAINTAINING DRILL HOLES PARALLEL TO THE CONCRETE SURFACE AND NORMAL TO THE JOINT LINE WITHIN 1/4" +/- AT THE END OF THE DOWEL. DRILL HOLES SHALL BE ACCURATELY LAID OUT SO THAT THE MAXIMUM DEVIATION DOES NOT EXCEED 1" IN THE HORIZONTAL DIRECTION AND 1/2" IN THE VERTICAL DIRECTION. DRILL HOLE DIAMETER TO BE OF SUFFICIENT SIZE TO ACCEPT THE TYPE AND SIZE DOWEL REQUIRED.
5. AFTER DRILLING IS COMPLETE AND PRIOR TO THE INSTALLATION OF THE DOWELS, THE HOLES SHALL BE THOROUGHLY CLEANED TO REMOVE DRILLING DUST, CONCRETE CHIPS AND ANY OTHER MATERIAL DETRIMENTAL TO DEVELOPING A BOND.
6. EPOXY SHALL BE INJECTED UNIFORMLY TO THE ENTIRE CIRCUMFERENCE OF THE DOWEL HOLE SURFACE (CONTRACTOR SHALL NOT DIP AND INSERT DOWEL) AND SUFFICIENT MATERIAL PLACED IN THE HOLE SO THAT A SLIGHT AMOUNT WILL BE FORCED OUT FROM AROUND THE ENTIRE CIRCUMFERENCE WHEN THE DOWEL IS INSERTED AND TAPPED TO THE CORRECT POSITION. SMALL WEDGES MAY BE USED TO SUPPORT THE DOWEL IN CORRECT ALIGNMENT UNTIL THE MATERIAL HARDENS. THE TOLERANCE FOR DOWEL ALIGNMENT IN EITHER THE HORIZONTAL OR VERTICAL PLANE IS 1/8" PER FOOT OF DOWEL BAR.
7. SUBMIT PROPOSED EPOXY MANUFACTURER INFORMATION FOR APPROVAL BY THE REPRESENTATIVE PRIOR TO STARTING WORK.
8. ALL LONGITUDINAL AND TRANSVERSE CONTRACTION JOINT DOWELS SHALL BE INSTALLED USING A WELDED WIRE BASKET ASSEMBLY APPROVED BY THE REPRESENTATIVE FIRMLY ANCHORED TO THE BASE WITH A MINIMUM OF 6 GALVANIZED STRAPS AND NAILS PER ASSEMBLY. POSITION ANCHOR STRAPS ON ALTERNATING SIDES OF THE BASKET ASSEMBLY.
9. A TRANSVERSE CONSTRUCTION JOINT SHALL BE INSTALLED AT A PLANNED JOINT WHEN PAVING OPERATIONS ARE INTERRUPTED FOR MORE THAN 30 MINUTES. IF THE INTERRUPTION OCCURS BETWEEN PLANNED JOINTS, THE FRESH CONCRETE SHALL BE REMOVED BACK TO THE PREVIOUSLY INSTALLED JOINT. NO JOINTS WILL BE ALLOWED BETWEEN THE JOINTS SHOWN ON THE JOINTING PLAN.
10. DOWEL BARS SHALL NOT BE PLACED MECHANICALLY.

**JOINT SEALANT NOTES:**

1. ALL JOINT CONSTRUCTION, MATERIALS AND JOINT SEALER SHALL BE IN ACCORDANCE WITH SPECIFICATION P-403, P-501, AND P-605 UNLESS OTHERWISE NOTED.
2. SEALANT RESERVOIR SHAPE FACTOR, W/D, SHALL BE PER THE MANUFACTURER'S RECOMMENDATIONS.
3. ALL JOINT SEALANT RESERVOIRS SHOWN ON THIS SHEET SHALL BE VERIFIED BY THE SEALANT MANUFACTURER PRIOR TO CONSTRUCTION. REFER TO SECTION P-605 OF THE SPECIFICATIONS FOR FURTHER INFORMATION.
4. EXISTING JOINT SEALANT AND BACKER ROD SHALL BE COMPLETELY REMOVED BY SAWCUTTING.
5. JOINT CLEANING SHALL BE ACCOMPLISHED BY COMPRESSED AIR OR OTHER APPROVED METHOD. OTHER CLEANING METHODS SHALL BE SUBMITTED TO THE REPRESENTATIVE FOR APPROVAL.
6. CONTRACTOR SHALL THOROUGHLY CLEAN ALL JOINTS, REMOVING ALL LOOSE MATERIAL AND SHALL THOROUGHLY SWEEP (VACUUM) PAVEMENT SURFACE TO THE SATISFACTION OF THE REPRESENTATIVE PRIOR TO ACCEPTANCE.
7. SAWCUT WIDTH SHALL BE 1/2" WIDE OR THE WIDTH OF THE EXISTING CONTRACTION OR CONSTRUCTION JOINTS AND 1" WIDE FOR EXPANSION JOINTS. NO EXTRA PAYMENT WILL BE MADE FOR SAWCUTS, CLEANING, BACKER ROD OR SEALANT IN JOINTS EXCEEDING DETAILED WIDTHS.
8. SAWCUT DEPTHS SHALL BE TO THE DEPTHS SHOWN IN THE DETAILS OR THE DEPTH NECESSARY TO FULLY REMOVE EXISTING BACKER ROD. NO EXTRA PAYMENT WILL BE MADE FOR SAWCUTS IN EXCESS OF DETAILED DEPTHS.
9. THE CONTRACTOR SHALL VACUUM ALL SLURRY FROM SAWCUTTING ADJACENT PAVEMENT.

**DOWEL BASKET NOTES:**

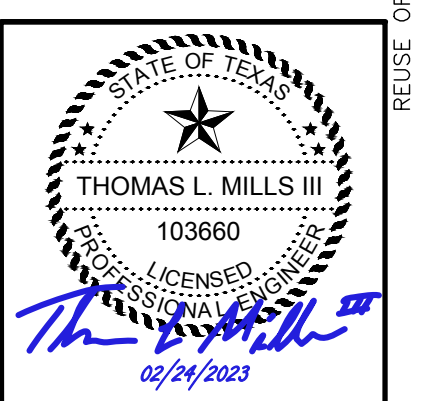
1. BASKETS SHALL BE FIRMLY ANCHORED TO THE BASE.
2. ALL WIRE SIZES SHOWN ARE MINIMUM SIZES.
3. DOWELS SHALL BE HELD FIRMLY IN THE WELDED ASSEMBLY AS SHOWN

CONCRETE PAVEMENT THICKNESS TABLE					
SLAB THICKNESS (T)	T/2	1.25T	DOWEL DIAMETER	DOWEL LENGTH	DOWEL SPACING
16"	8"	20"	1 1/4"	20"	15"

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NO.	DESCRIPTION	DATE
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FAA NON-STANDARD TAXIWAYS PROJECT  
PAVEMENT JOINTING DETAILS

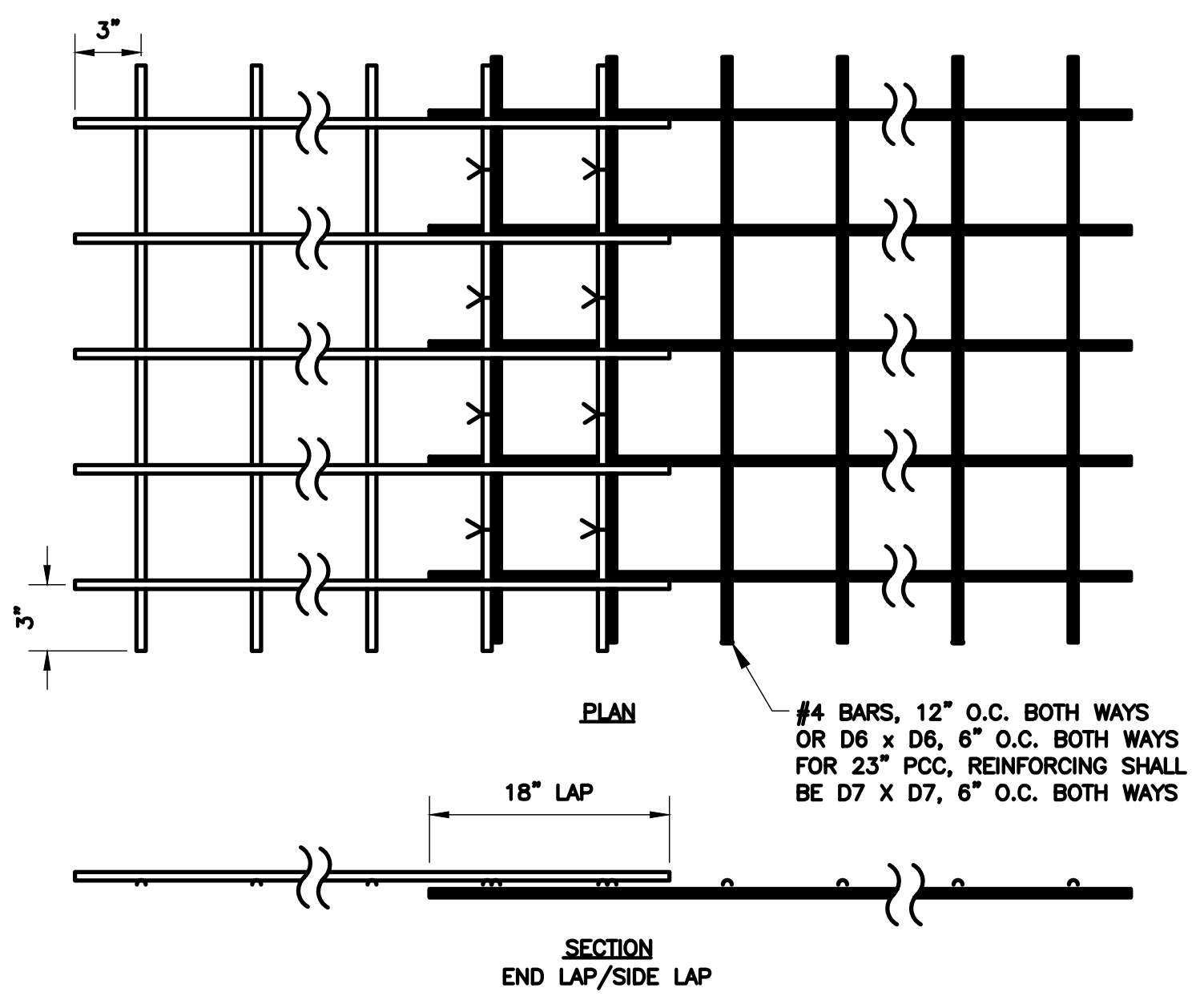
PROJECT MGR:	S. CHILDERS
DESIGNER:	D. CRAWFORD
DRAWN BY:	C. MCLAIN
CHECKED BY:	R. EHTESHAM
SCALE:	AS SHOWN
DATE:	02/24/2023



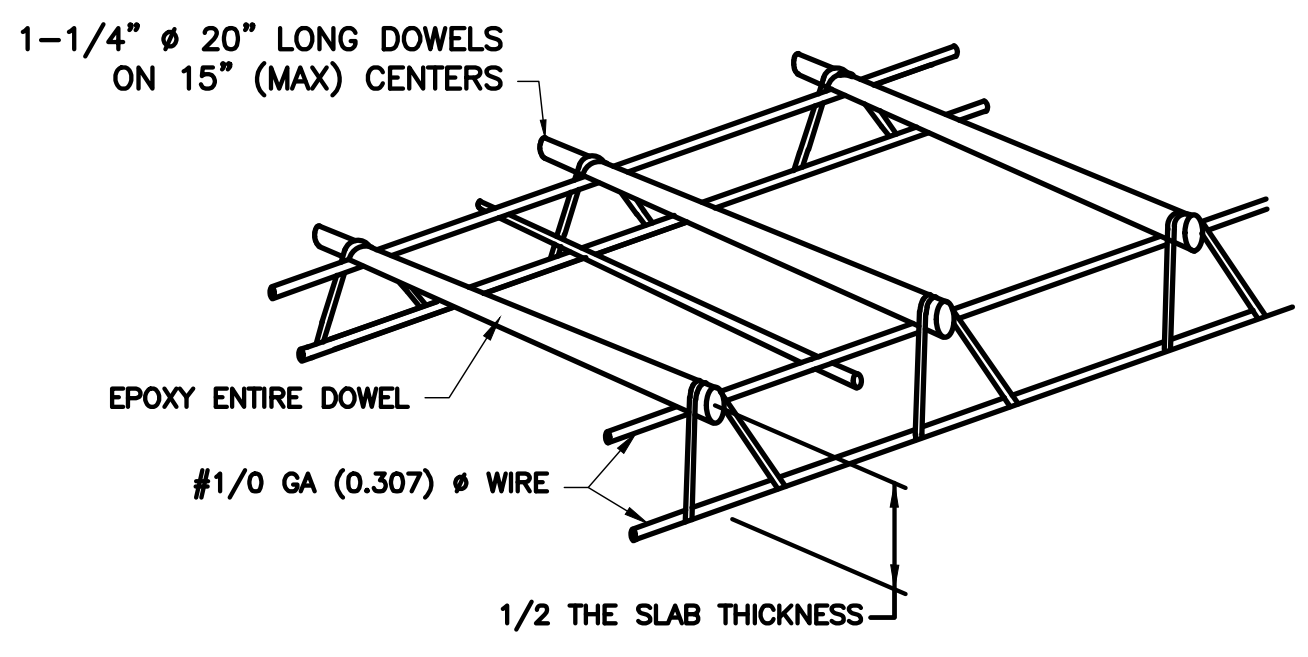
APPROVED BY: \_\_\_\_\_ DATE: \_\_\_\_\_  
DIRECTOR  
HOUSTON AIRPORT SYSTEM

PROJECT NO:	770
C.I.P. NO:	3-48-0110-044
H.A.S. NO:	N/A
SHEET NO:	CP501



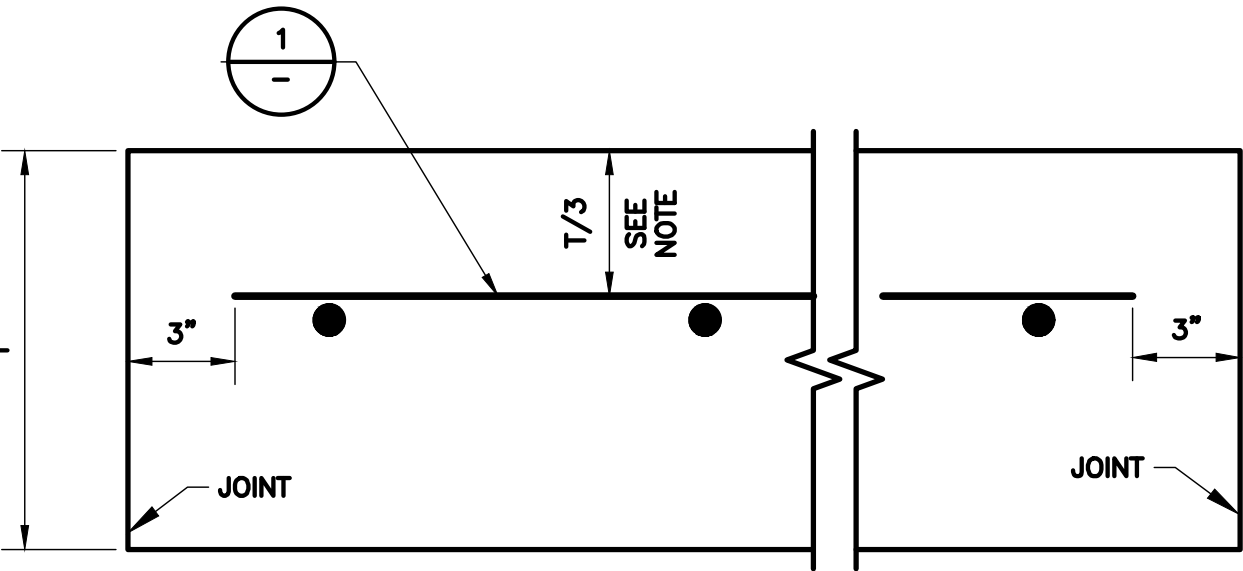


**1 TAXIWAY PAVEMENT WELDED WIRE FABRIC DETAIL**  
N.T.S.



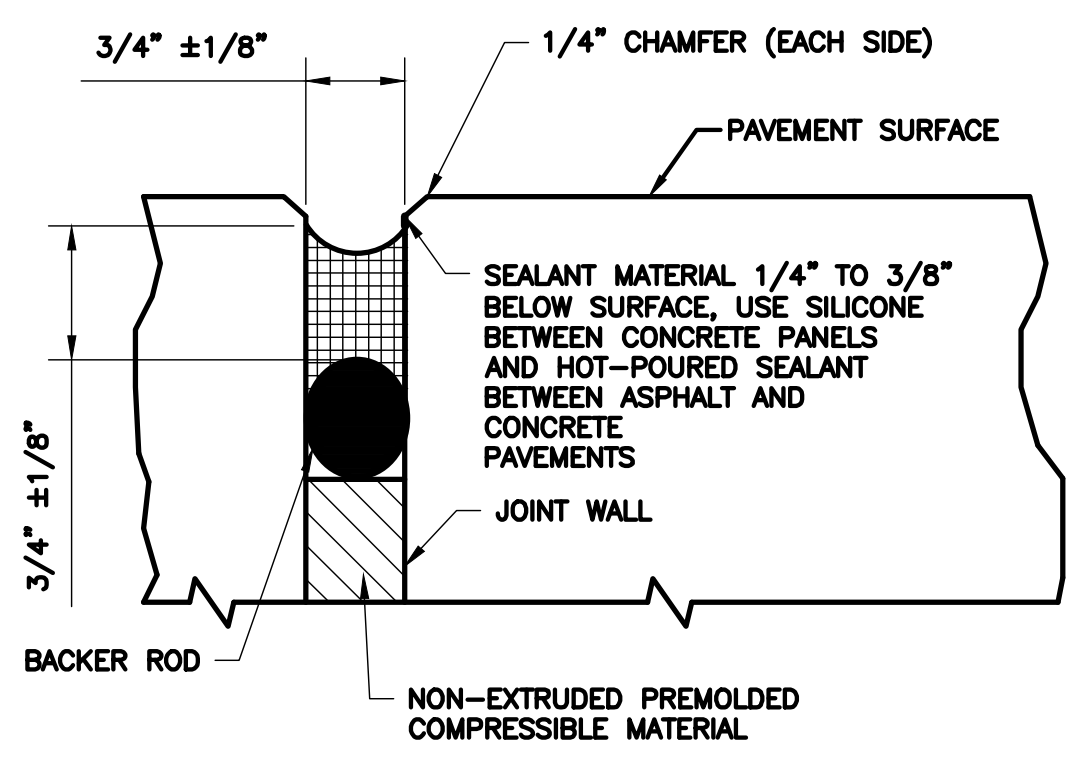
**2 TYPICAL DOWEL BASKET ASSEMBLY**  
N.T.S.

NOTE:  
1. ANY PORTION OF THE BASKET IN CONTACT WITH THE SUBGRADE, INCLUDING CTB OR BOND BREAKER, SHALL BE EPOXY COATED.

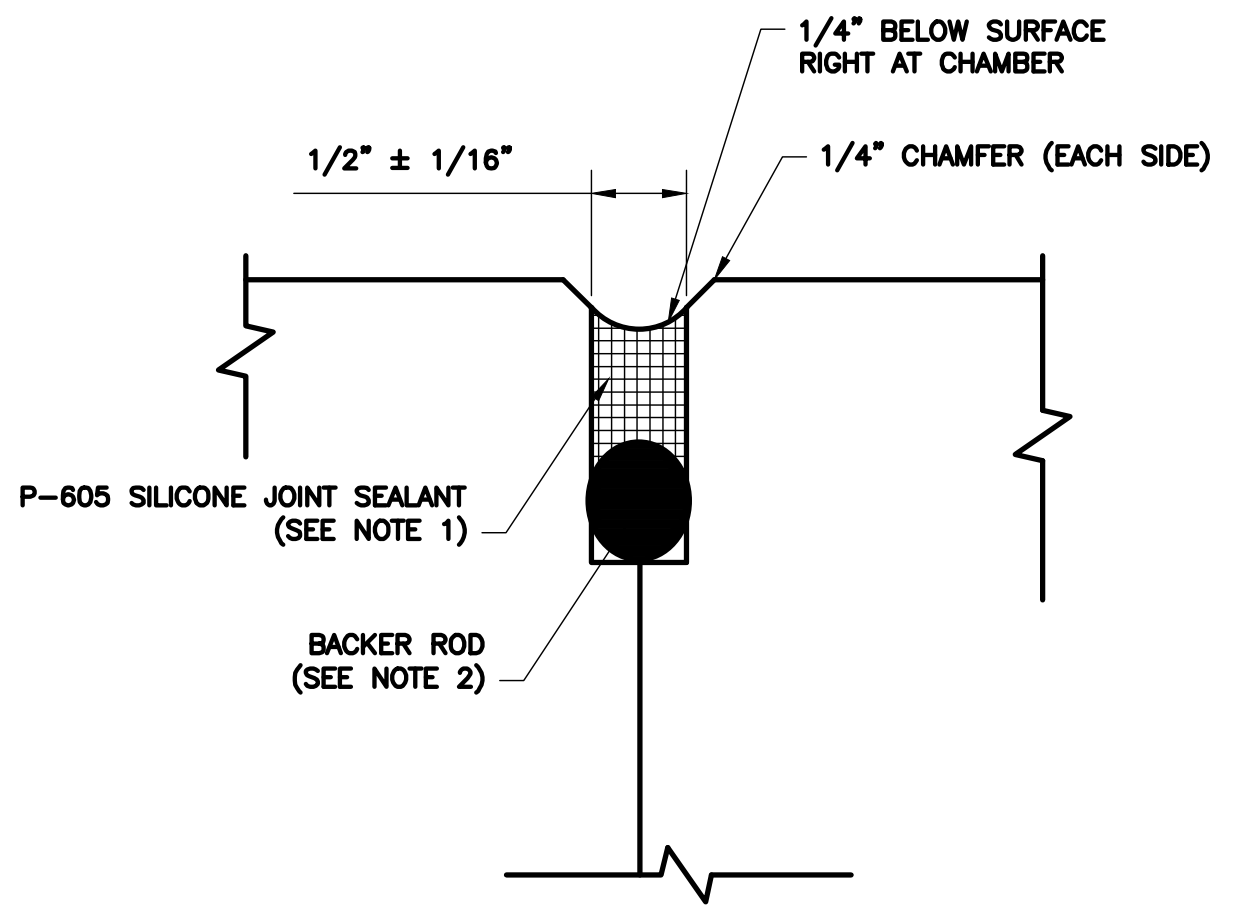


**3 REINFORCED PAVEMENT (TAXIWAY PAVEMENT)**  
N.T.S.

NOTE:  
1. REINFORCING SHALL BE PLACED AT APPROXIMATELY THE UPPER 1/3 OF THE PANEL. IT MAY BE MOVED UP TO DECONFLICT WITH DOWELS, TO A MAXIMUM OF T/1+1" FROM THE TOP OF SLAB.

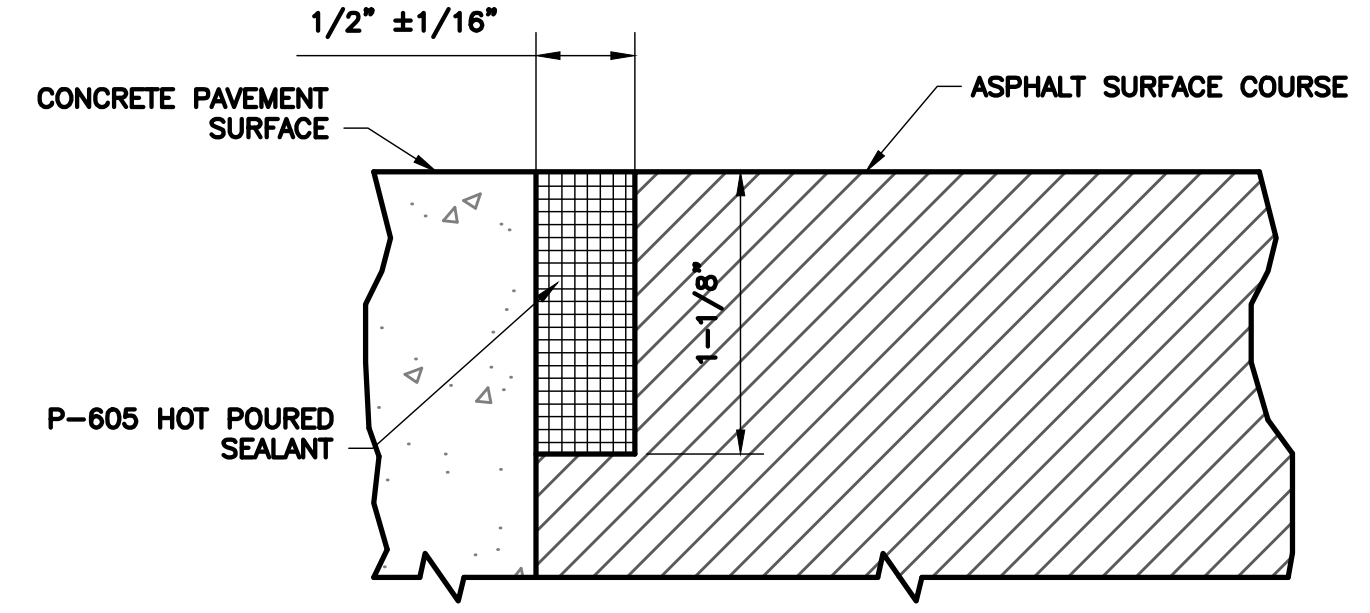


**4 DETAIL A - ISOLATION JOINT**  
N.T.S.



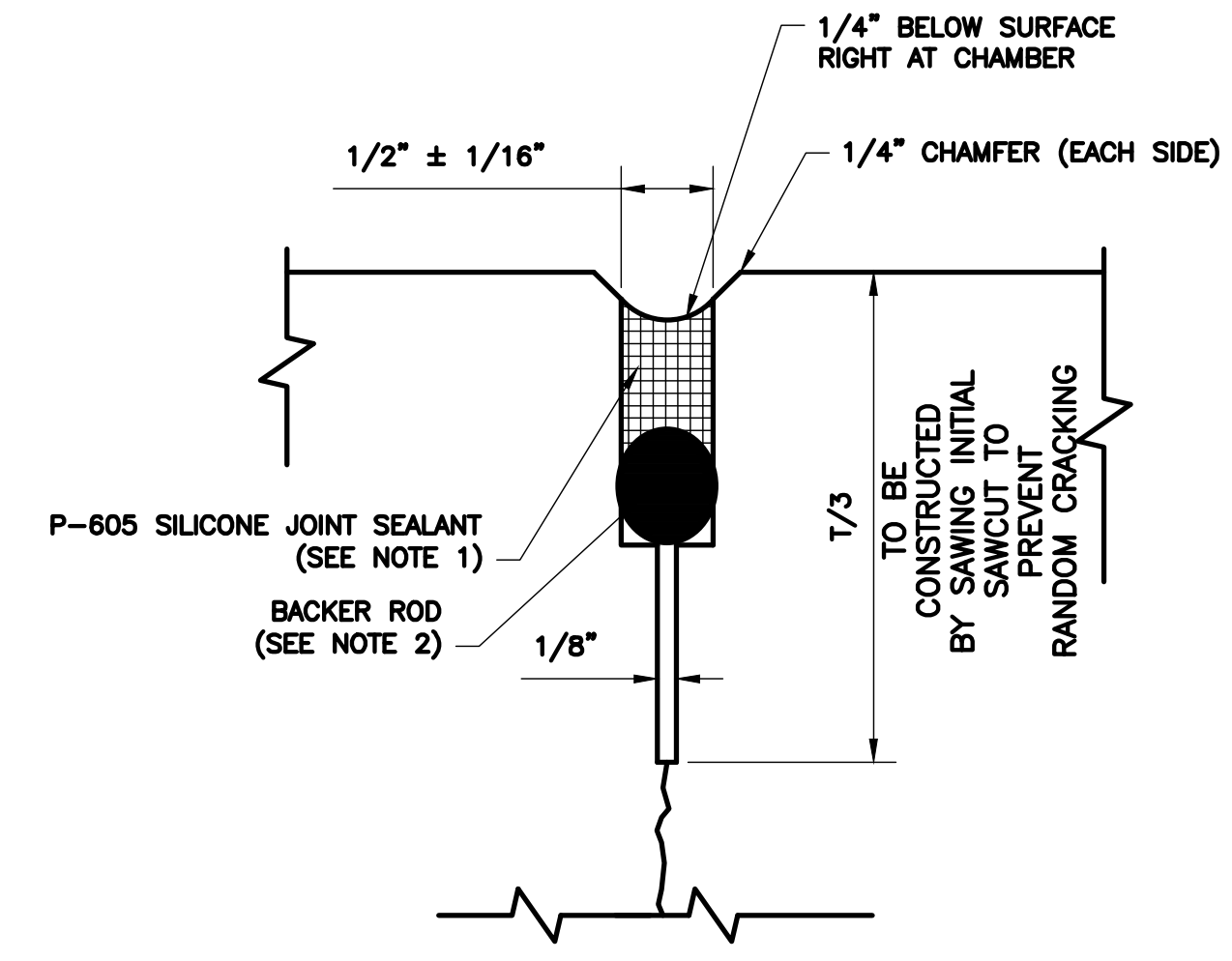
**5 DETAIL C - CONSTRUCTION JOINT**  
N.T.S.

NOTES:  
1. DEPTH PER MANUFACTURER'S GUIDELINES (BASED ON THE WIDTH).  
2. BACKER ROD MATERIAL MUST BE COMPATIBLE WITH THE TYPE OF SEALANT USED AND SIZED TO PROVIDE THE DESIRED SHAPE FACTOR.



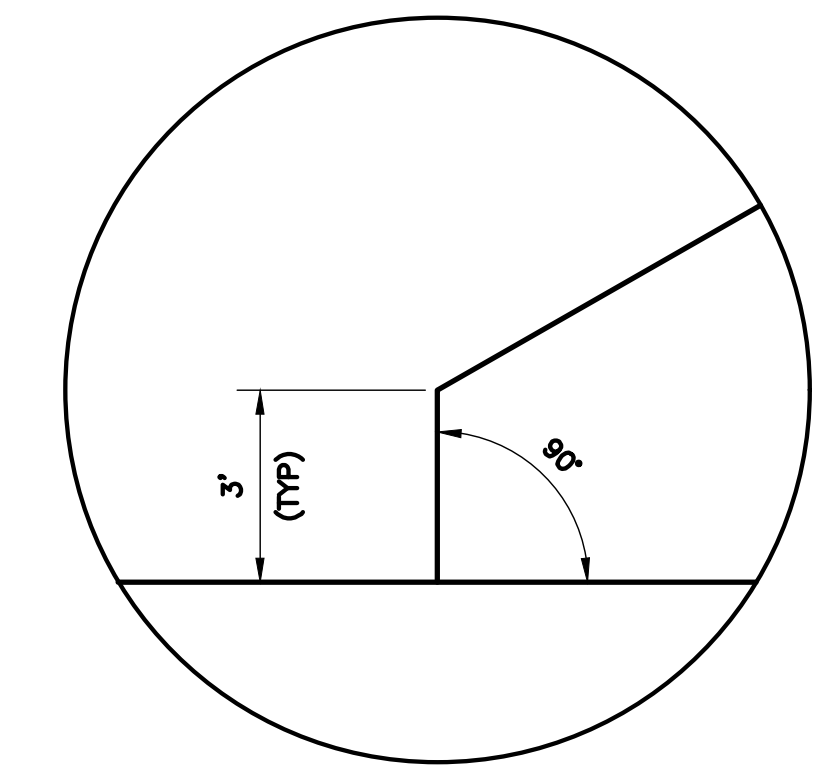
**6 DETAIL B - ASPHALT JOINT**  
N.T.S.

NOTES:  
1. THE SHOULDER ASPHALT SURFACE COURSE (P-403) SHALL BE 0" TO 1/16" BELOW THE CONCRETE SURFACE.

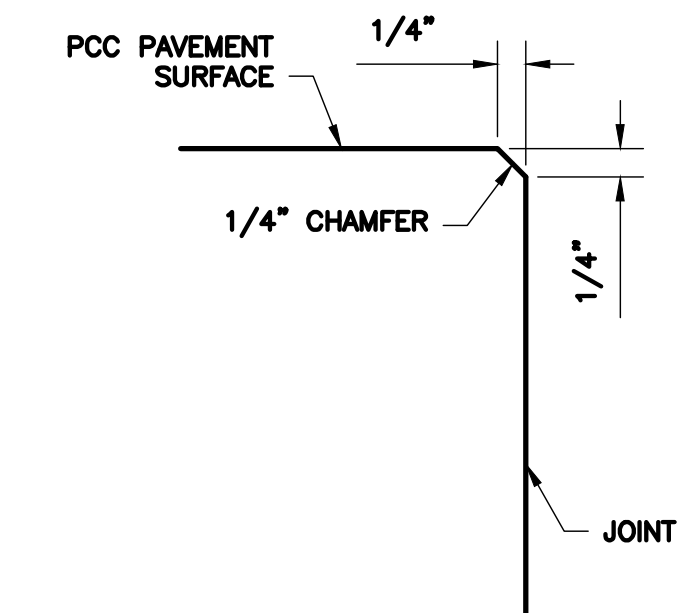


**7 DETAIL D - CONTRACTION JOINT**  
N.T.S.

NOTES:  
1. DEPTH PER MANUFACTURER'S GUIDELINES (BASED ON THE WIDTH).  
2. BACKER ROD MATERIAL MUST BE COMPATIBLE WITH THE TYPE OF SEALANT USED AND SIZED TO PROVIDE THE DESIRED SHAPE FACTOR.



**8 JOINT DOGLEG**  
N.T.S.

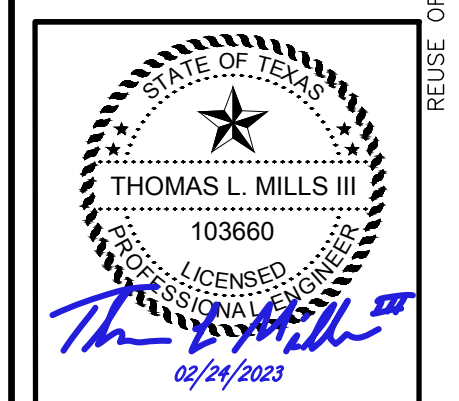


**9 1/4 - INCH CHAMFER**  
N.T.S.

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FAA NON-STANDARD TAXIWAYS PROJECT  
PAVEMENT JOINTING DETAILS

PROJECT MGR:	S. CHILDERS
DESIGNER:	D. CRAWFORD
DRAWN BY:	C. MCCLAIN
CHECKED BY:	R. EHTESHAM
SCALE:	AS SHOWN
DATE:	02/24/2023



APPROVED BY: \_\_\_\_\_ DATE: \_\_\_\_\_

PROJECT NO:	770
C.I.P. NO:	3-48-0110-044
H.A.S. NO:	N/A
SHEET NO:	CP502



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0	ISSUED FOR BID	02/24/2023	SC

FAA NON-STANDARD TAXIWAYS PROJECT

PAVEMENT JOINTING DETAILS

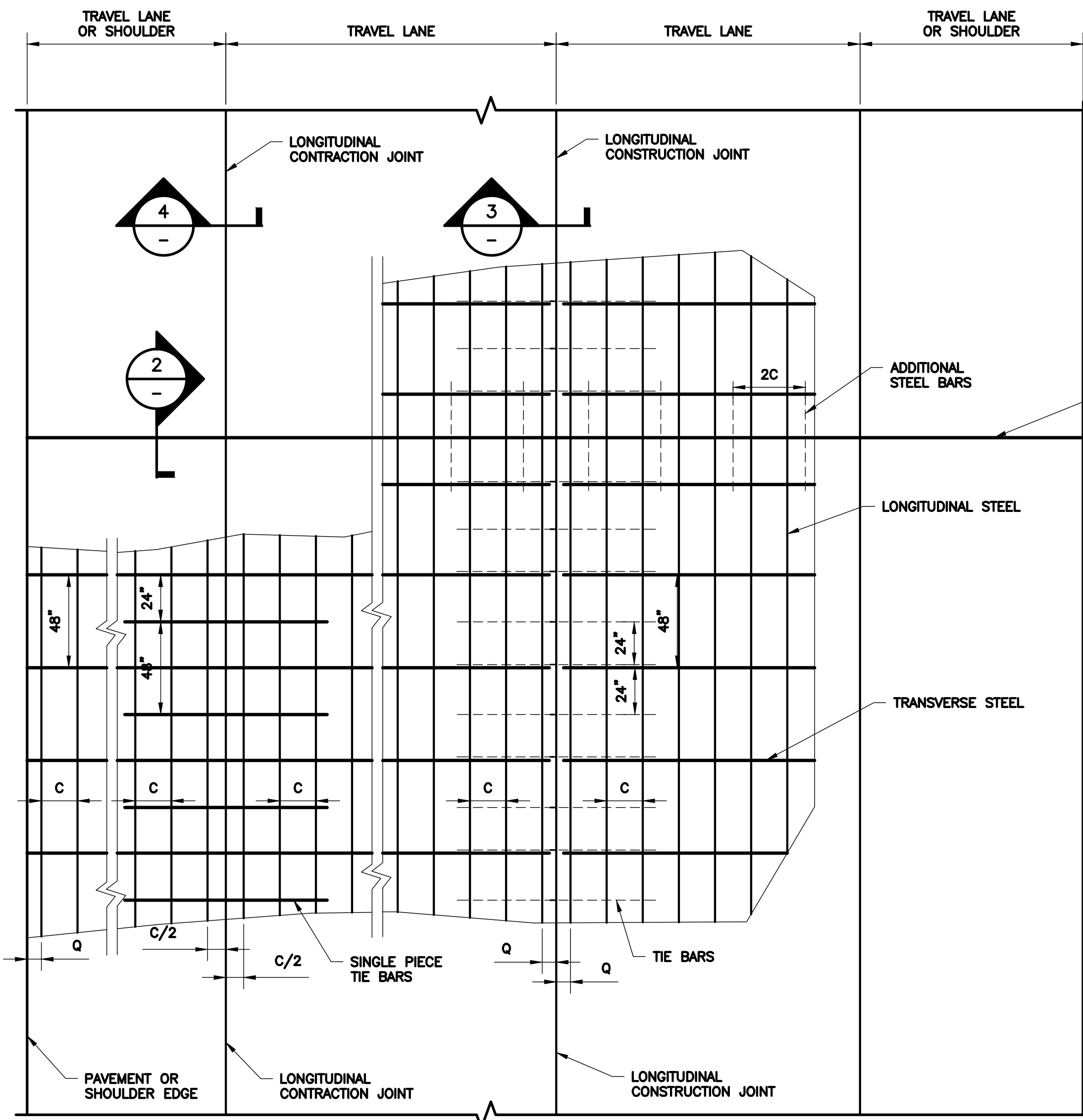
PROJECT MGR: S. CHILDERS  
 DESIGNER: D. CRAWFORD  
 DRAWN BY: C. MCLAIN  
 CHECKED BY: R. EHTESHAM  
 SCALE: AS SHOWN  
 DATE: 02/24/2023



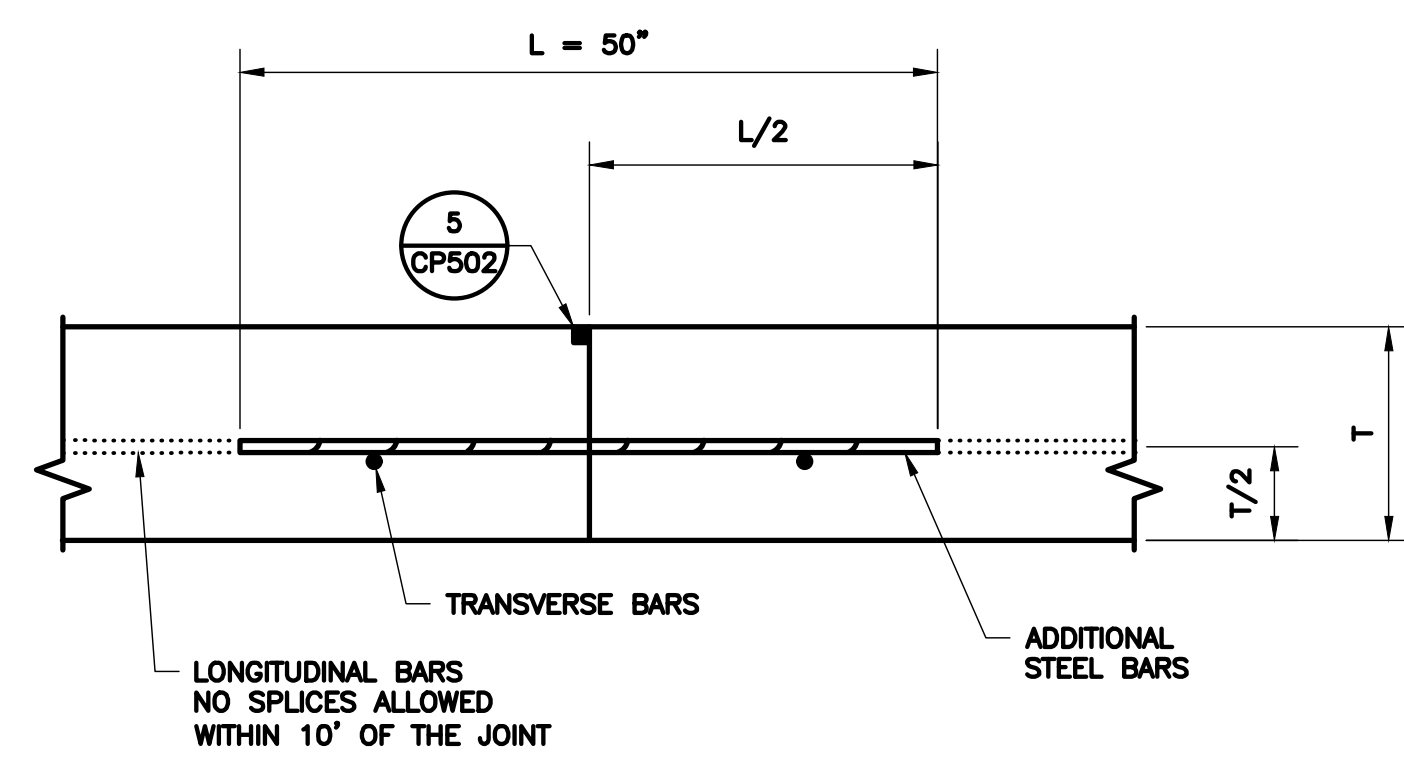
APPROVED BY: \_\_\_\_\_ DATE: \_\_\_\_\_

DIRECTOR  
 HOUSTON AIRPORT SYSTEM

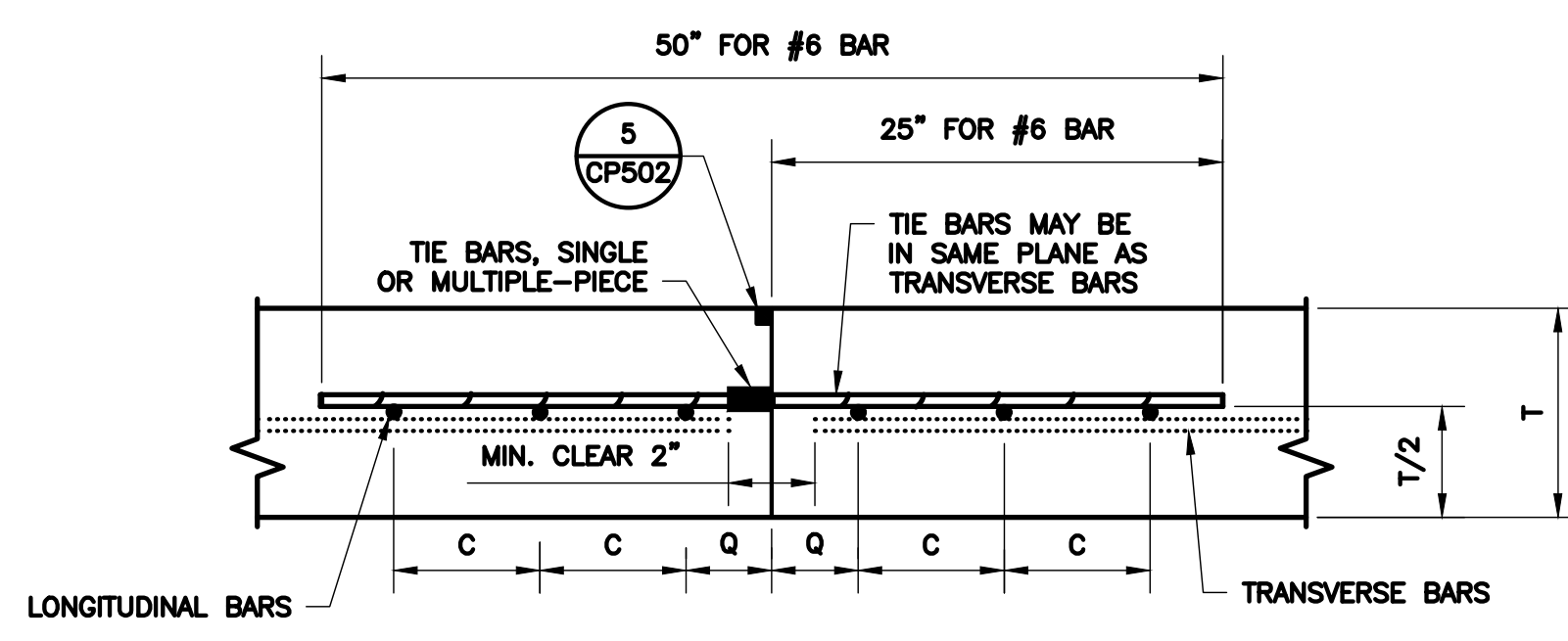
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 C.I.P. NO: 3-48-0110-044  
 H.A.S. NO: N/A  
 SHEET NO: CP503  
 of



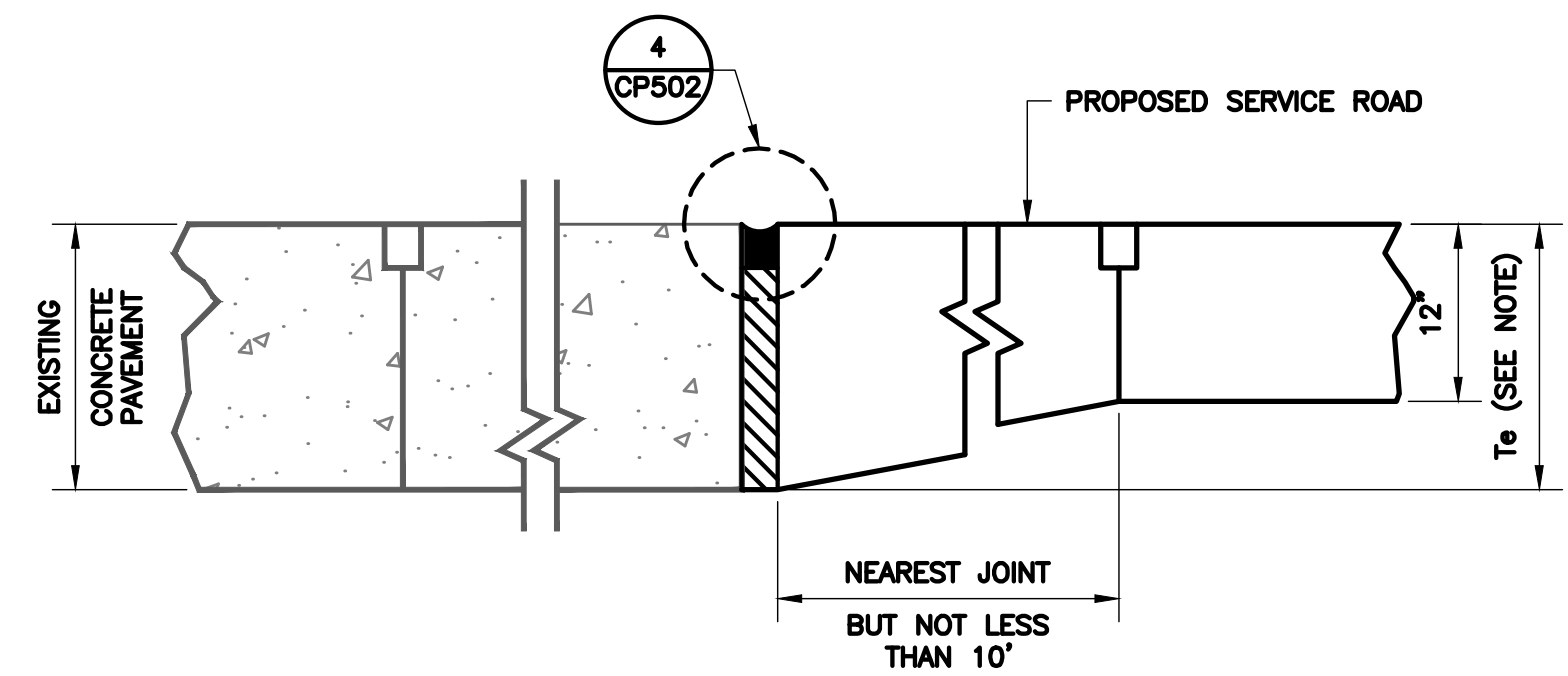
**1** TYPICAL PAVEMENT LAYOUT  
 N.T.S.



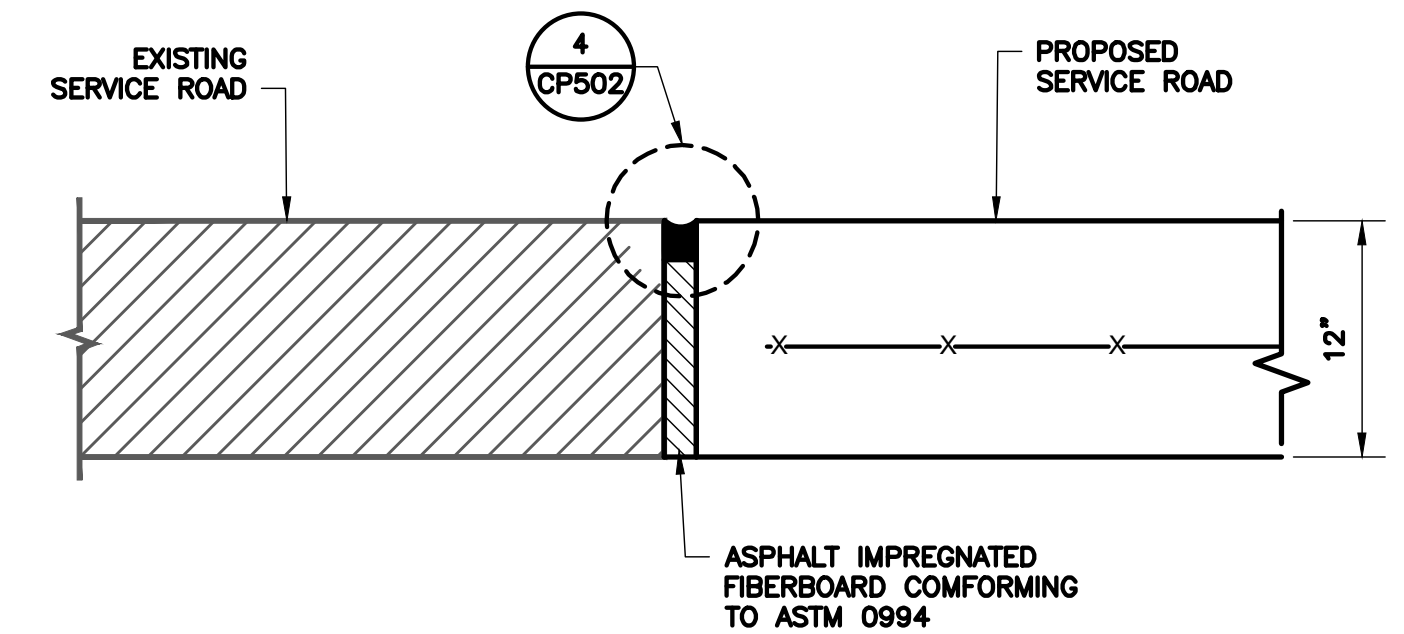
**2** TRANSVERSE CONSTRUCTION JOINT (X-X)  
 N.T.S.



**3** TYPE 'E1' - LONGITUDINAL CONSTRUCTION JOINT (Y-Y)  
 N.T.S.



**5** TYPE 'A2' ROADWAY THICKENED EDGE ISOLATION JOINT  
 N.T.S.



**6** TYPE 'A3' ROADWAY ISOLATION JOINT  
 N.T.S.

**GENERAL NOTES:**

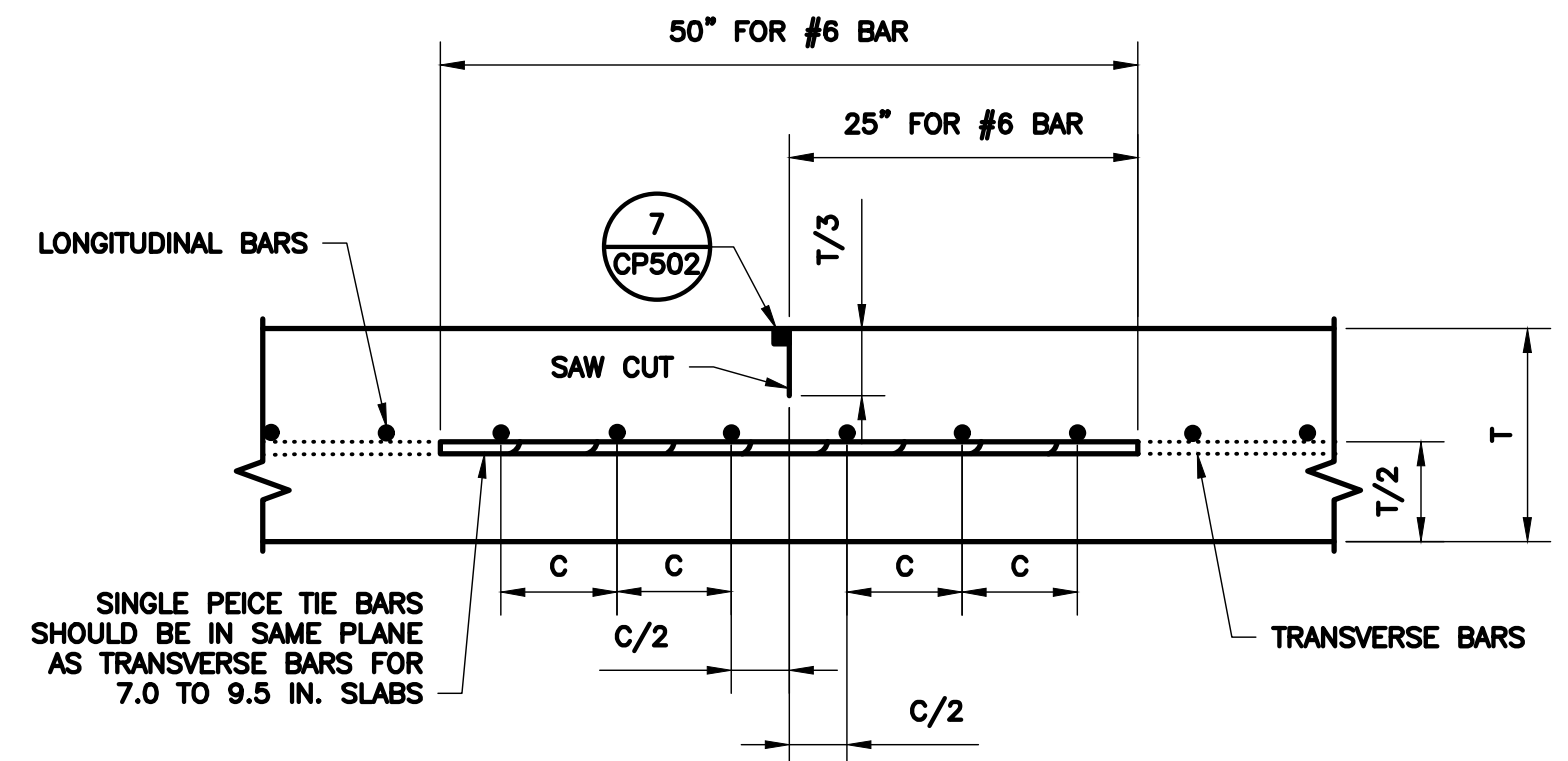
- ALL THE REINFORCING STEEL AND TIE BARS SHALL BE DEFORMED STEEL BARS CONFORMING TO ASTM A 615 (GRADE 60) OR ASTM A 996 (GRADE 60) OR ABOVE. STEEL BAR SIZES AND SPACINGS SHALL CONFORM TO TABLE NO.1 AND TABLE NO.2.
- STEEL BAR PLACEMENT TOLERANCE SHALL BE +/- 1 IN. HORIZONTALLY AND +/- 0.5 IN. VERTICALLY. CALCULATED AVERAGE BAR SPACING (CONCRETE PLACEMENT WIDTH / NUMBER OF LONGITUDINAL BARS) SHALL CONFORM TO TABLE NO.1.
- PAVEMENT WIDTHS OF MORE THAN 15 FT. SHALL HAVE A LONGITUDINAL JOINT (SECTION Z-Z OR SECTION Y-Y). THESE JOINTS SHALL BE LOCATED WITHIN 6 IN. OF THE LANE LINE UNLESS THE JOINT LOCATION IS SHOWN ELSEWHERE ON THE PLANS.
- REPLACE MISSING OR DAMAGED TIE BARS WITHOUT ADDITIONAL COMPENSATION BY DRILLING MIN.10 IN. DEEP AND GROUTING THE BARS WITH TYPE III, CLASS C EPOXY. MEET THE PULL-OUT TEST REQUIREMENTS IN ITEM 361.
- OMIT TIE BARS LOCATED WITHIN 18-IN. OF THE TRANSVERSE CONSTRUCTION JOINTS (SECTION X-X). USE HAND-OPERATED IMMERSION VIBRATORS TO CONSOLIDATE THE CONCRETE ADJACENT TO ALL FORMED JOINTS.
- LONGITUDINAL REINFORCING STEEL SPLICES SHALL BE A MINIMUM OF 25 IN. STAGGER THE LAP LOCATIONS SO THAT NO MORE THAN 1/3 OF THE LONGITUDINAL STEEL IS SPLICED IN ANY GIVEN 12-FT. WIDTH AND 2-FT. LENGTH OF THE PAVEMENT.

**TABLE NO.1**  
 LONGITUDINAL STEEL

SLAB THICKNESS AND BAR SIZE		REGULAR STEEL BARS	FIRST SPACING AT EDGE OR JOINT	ADDITIONAL STEEL BARS AT TRANSVERSE CONSTRUCTION JOINT (SECTION X-X)	
T (IN.)	BAR SIZE	SPACING C (IN.)	SPACING Q (IN.)	SPACING 2 X C (IN.)	LENGTH L (IN.)
12.0	#6	6.0	3 TO 4	12	50

**TABLE NO.2**  
 TRANSVERSE STEEL & TIE BARS

SLAB THICKNESS (IN.)	TRANSVERSE STEEL		TIE BARS AT LONGITUDINAL CONTRACTION JOINT (SECTION Z-Z)		TIE BARS AT LONGITUDINAL CONSTRUCTION JOINT (SECTION Y-Y)	
	BAR SIZE	SPACING (IN.)	BAR SIZE	SPACING (IN.)	BAR SIZE	SPACING (IN.)
8.0 - 13.0	#5	48	#6	48	#6	24



**4** TYPE 'C1' - LONGITUDINAL CONTRACTION JOINT (Z-Z)  
 N.T.S.



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NO.	DESCRIPTION	DATE	BY

NO.	DESCRIPTION	DATE	BY

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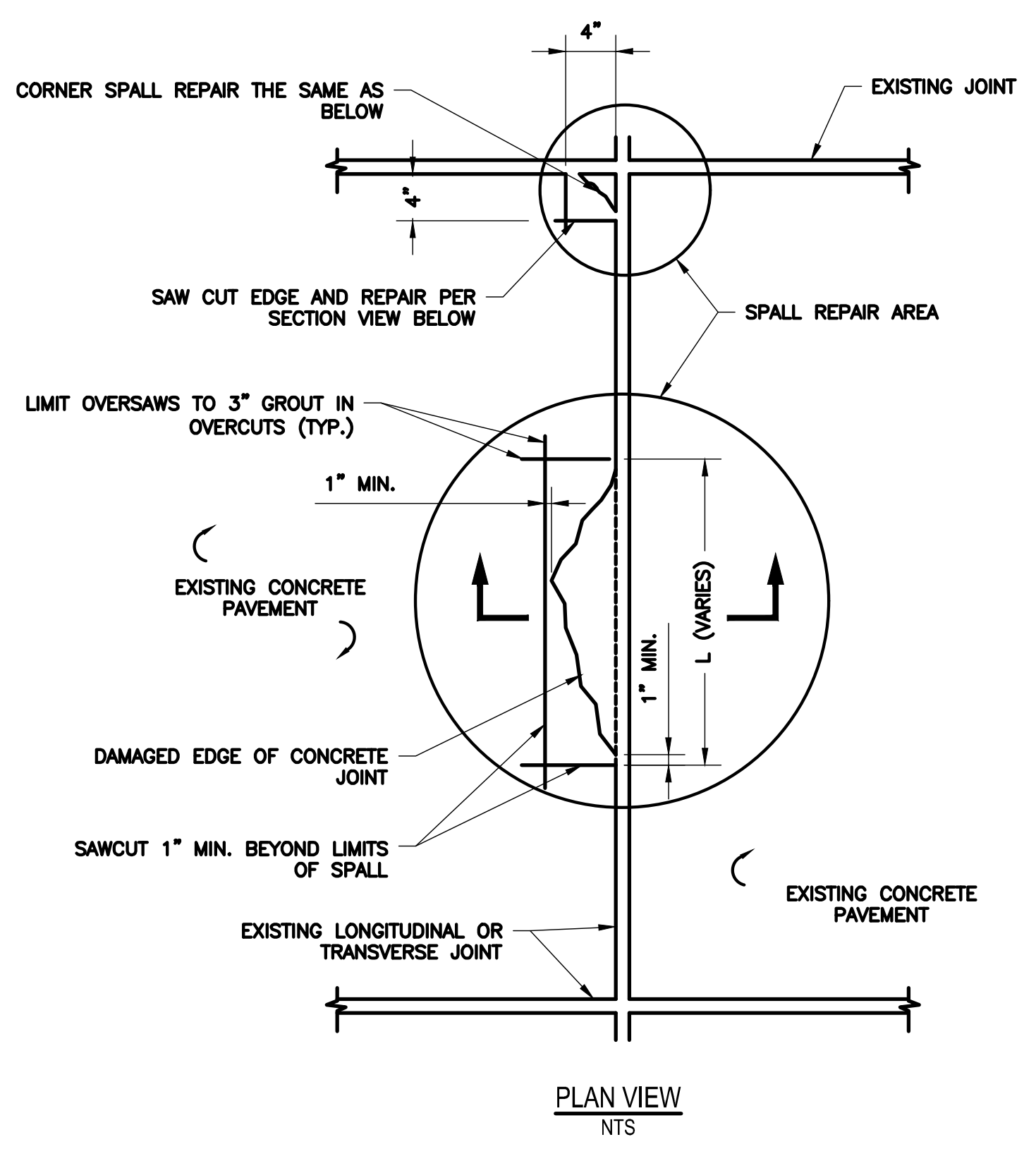
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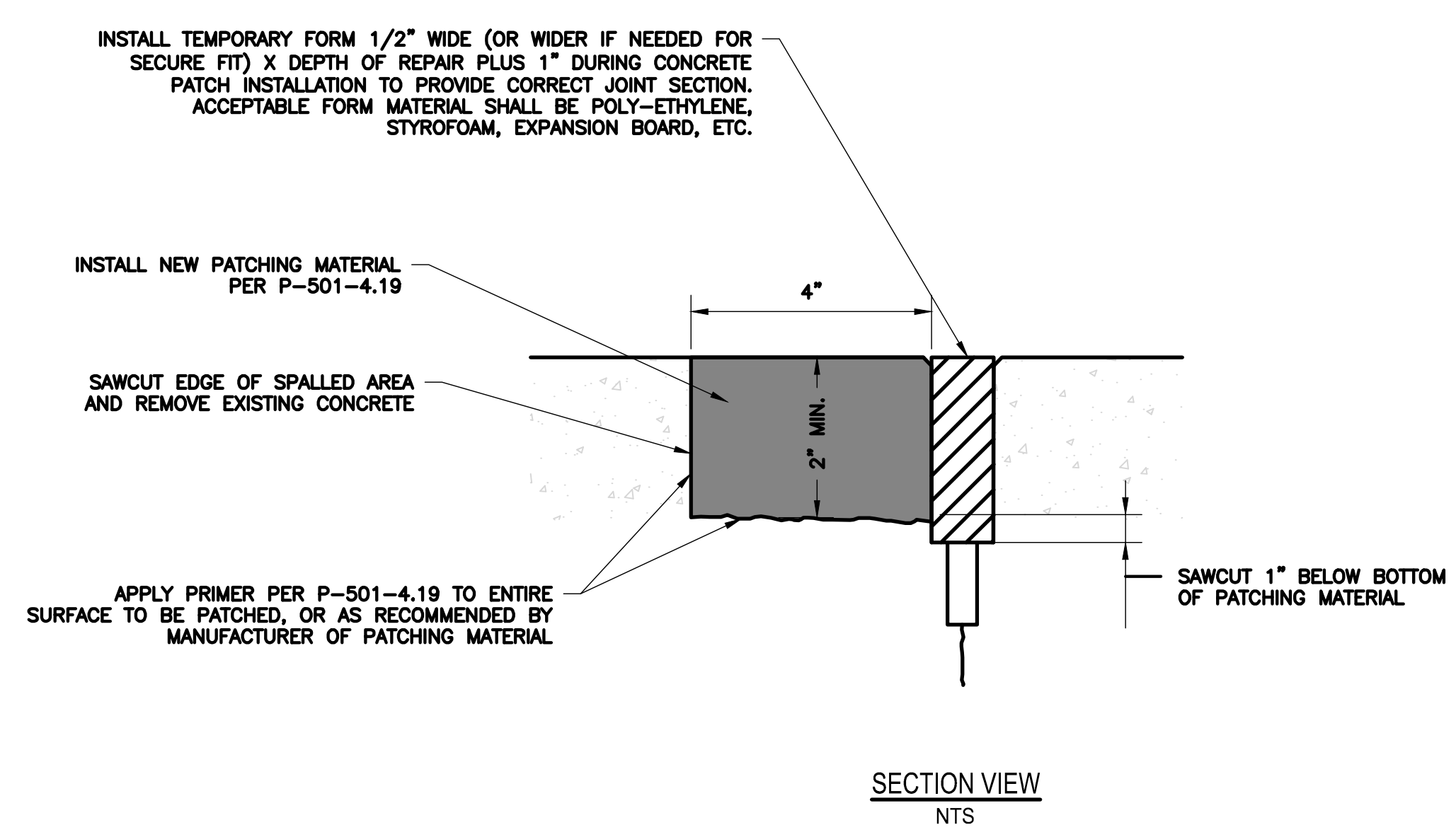
NO.	DESCRIPTION	DATE	BY

NO.	DESCRIPTION	DATE	BY

NO.	DESCRIPTION	DATE	BY



- NOTES**
- SAW CUT MINIMUM 1" CLEAR OF EXISTING SPALL, 2" MINIMUM DEPTH, REMOVE SPALLED CONCRETE AND EXISTING SEALANT WITH PATCH BOUNDARY.
  - SAW CUTS SHALL BE STRAIGHT LINES FORMING RECTANGULAR PATCHES SURROUNDING THE SPALLED AREA.
  - REMOVE UNSOUND CONCRETE AND AT LEAST 1/2" OF VISUALLY SOUND CONCRETE BETWEEN SAW CUT AND THE JOINT. MAXIMUM ALLOWABLE PNEUMATIC HAMMER SIZE IS 30 POUNDS.
  - CLEAN AND DRY AREA TO BE PATCHED BY AIR BLASTING. JOINTS AND EXPOSED CONCRETE SURFACE SHALL BE DUST-FREE PRIOR TO INSTALLATION OF SEALER.
  - INSTALL NON ABSORBENT BOARDS WITHIN THE LIMITS OF THE JOINT SEAL ALONG THE LENGTH OF THE ADJACENT CONCRETE PANEL. PROVIDE A SECURE SEAL BETWEEN BOARDS AND EXISTING CONCRETE TO ENSURE NEW PATCHING MATERIAL DOES NOT PENETRATE INTO THE JOINT.
  - PATCH SHALL BE TROWELED TOWARD THE PATCH PERIMETER, NOT AWAY FROM PATCHED PERIMETER.
  - SPALLS DEEPER THAN 1/2 THE SLAB DEPTH OR SPALLS LONGER THAN 25% OF THE ADJACENT JOINT REQUIRE REPLACEMENT OF THE ENTIRE SLAB.
  - SPALLS LESS THAN ONE INCH WIDE AND LESS THAN THE DEPTH OF THE JOINT SEALANT RESERVOIR, SHALL BE FILLED WITH JOINT SEALANT MATERIAL.
  - SECTION SHOWS SPALL REPAIR ON ONE SIDE OF THE JOINT. SOME CASES WILL REQUIRE REPAIR ON BOTH SIDES OF THE JOINT, HOWEVER THE METHODOLOGY SHALL BE FOLLOWED TO REPAIR BOTH SIDES.
  - JOINT SEALANT SHALL BE RE-ESTABLISHED PER SPECIFICATION P-605.



**1 SPALL REPAIR DETAIL**  
N.T.S.

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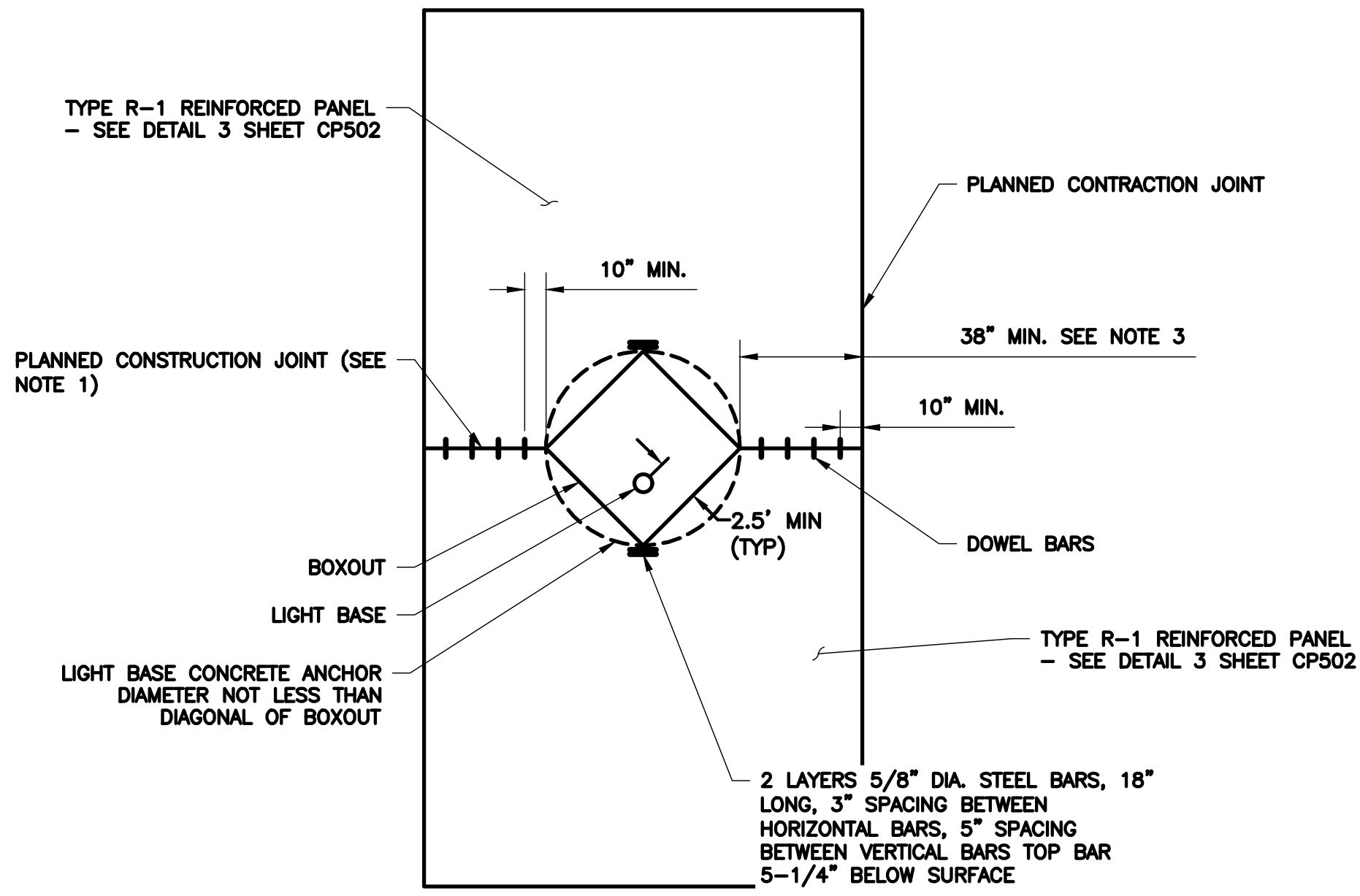
FAA NON-STANDARD TAXIWAYS PROJECT  
 PAVEMENT JOINTING DETAILS

PROJECT MGR:	S. CHILDERS
DESIGNER:	D. CRAWFORD
DRAWN BY:	D. CRAWFORD
CHECKED BY:	R. EHTESHAM
SCALE:	AS SHOWN
DATE:	02/24/2023

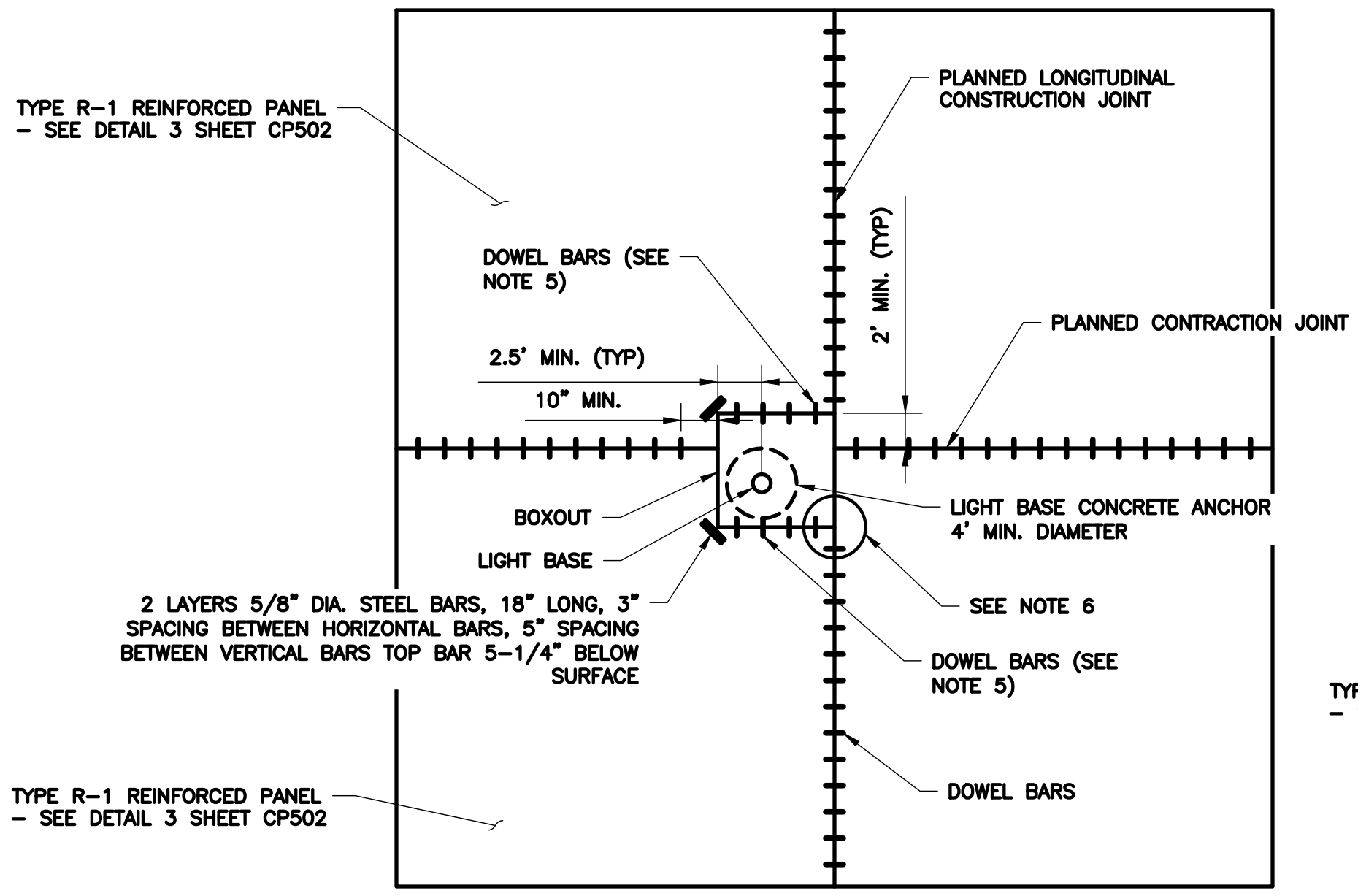


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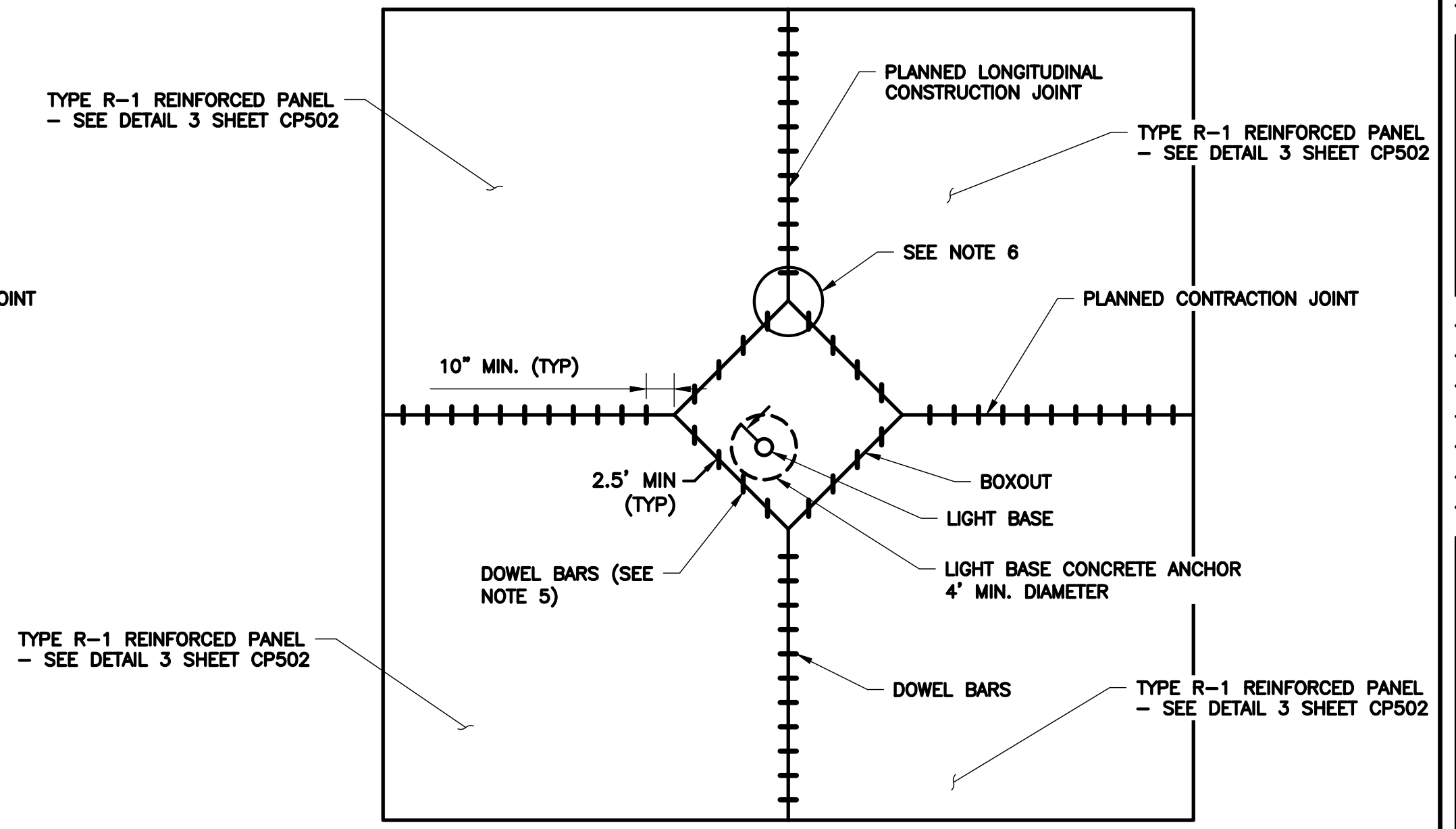
PROJECT NO:	770
C.I.P. NO:	3-48-0110-044
H.A.S. NO:	N/A
SHEET NO:	CP505
	of



**1** INTERIOR SLAB BOXOUT  
 N.T.S.



**2** EDGE SLAB BOXOUT - 2 PANELS  
 N.T.S.

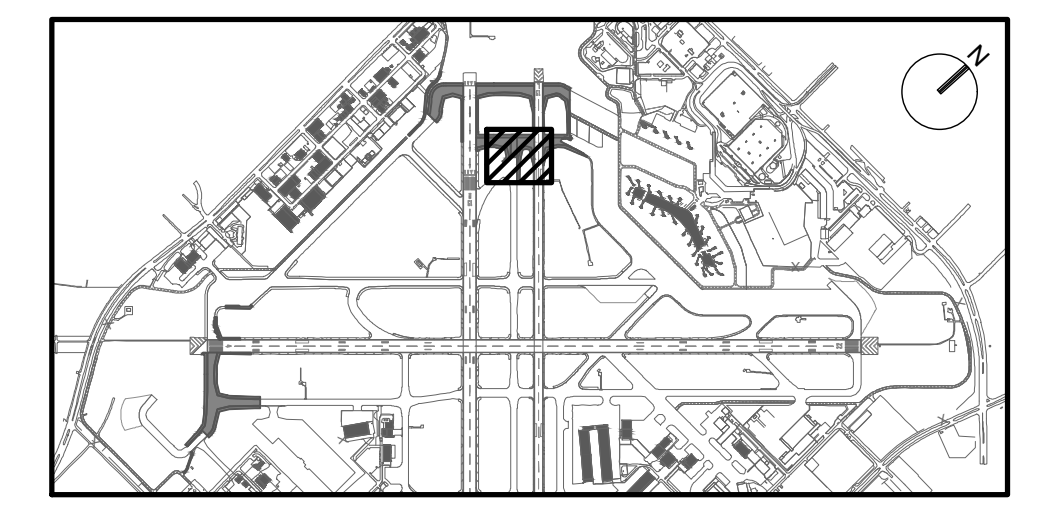
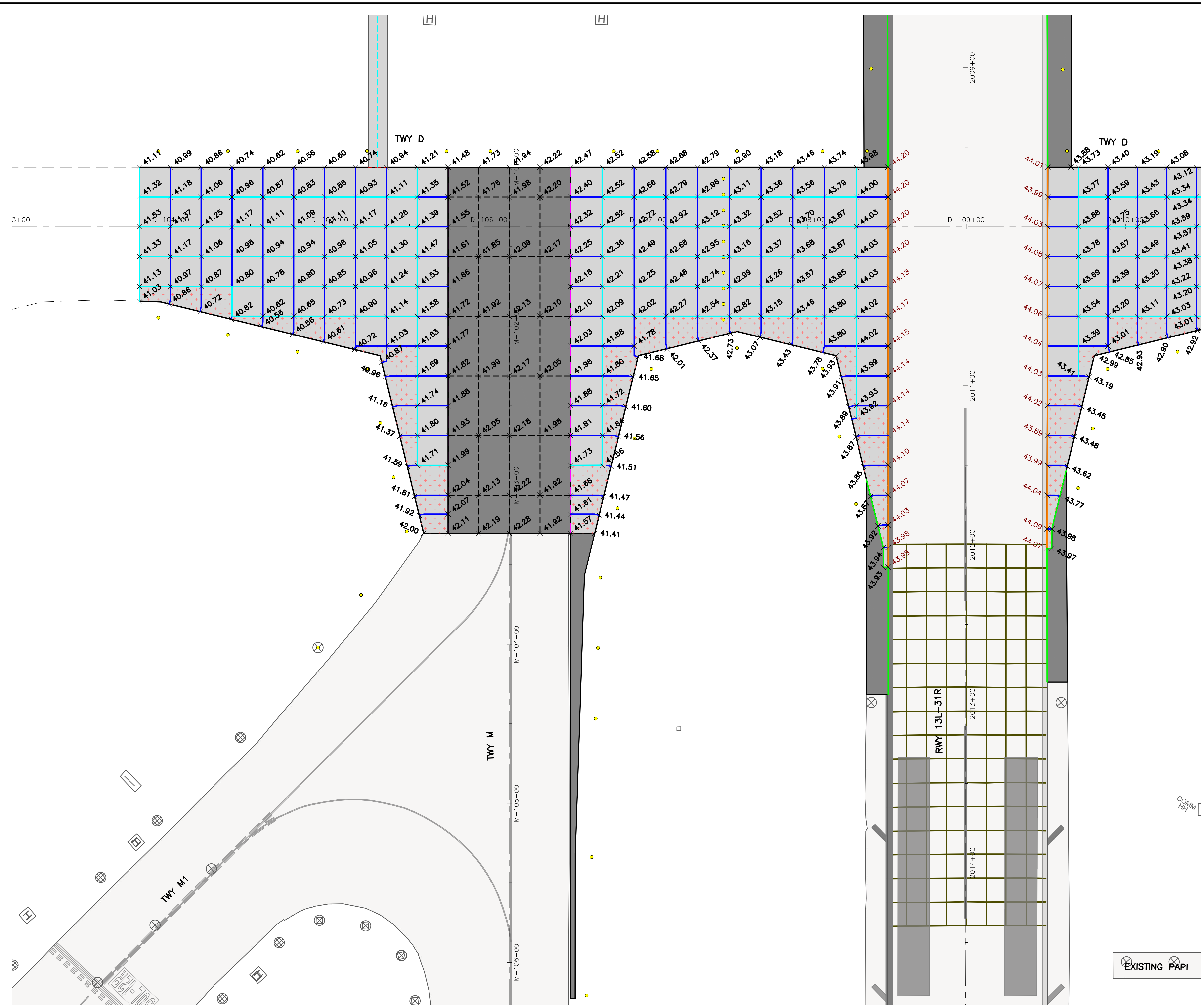


**3** EDGE SLAB BOXOUT - 4 PANELS  
 N.T.S.

**BOXOUT NOTES:**

- WHEN A BOXOUT IS USED ON A PLANNED CONTRACTION JOINT, THE JOINT SHALL BE CHANGED TO A CONSTRUCTION JOINT.
- A BOXOUT IS NOT REQUIRED WHEN AN IN-PAVEMENT LIGHT BASE IS INSTALLED AT OR MORE THAN TWO FEET FROM A PLANNED JOINT. A BOXOUT MUST BE USED WHEN A LIGHT BASE MUST BE LOCATED CLOSER THAN TWO FEET TO A PLANNED CONSTRUCTION JOINT.
- WHEN MINIMUM CLEARANCE FROM THE INTERIOR BOXOUT TO A PLANNED JOINT CAN NOT BE ACHIEVED, AN EDGE BOXOUT SHALL BE UTILIZED.
- AN EDGE BOXOUT MUST INCORPORATE ALL OF THE PANEL DISTANCE BETWEEN A PLANNED JOINT AND THE LIGHT BASE. DO NOT CROSS A PLANNED JOINT WITH THE BOXOUT IF THERE IS AT LEAST 2' FROM THE LIGHT BASE TO THE PLANNED JOINT. BOXOUTS WHICH ARE REQUIRED TO CROSS A PLANNED JOINT MUST EXTEND A MINIMUM OF 2' BEYOND THE PLANNED JOINT.
- DOWEL BARS SHALL BE ALIGNED PARALLEL WITH THE DIRECTION OF TRAFFIC ONLY. DOWELS SHALL BE SPACED AT 18" AND PLACED NO CLOSER THAN 10" FROM THE EDGE OF THE BOXOUT OR PLANNED JOINT.
- DOWEL BAR SPACING ALONG A PLANNED PAVEMENT JOINT HAS PRECEDENCE OVER DOWEL BAR SPACING ALONG THE BOXOUT JOINT. A DOWEL PLACED ALONG A BOXOUT JOINT MUST BE LOCATED AT LEAST 12" FROM THE END OF A DOWEL BAR PLACED ALONG THE INTERSECTING PAVEMENT JOINT.





**KEY MAP**  
NTS

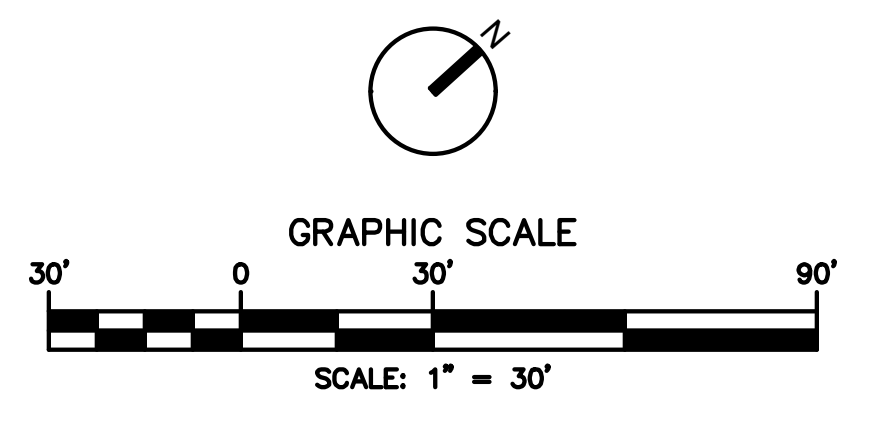
**NOTES:**

- FOR PAVEMENT JOINT TYPES AND REINFORCEMENT PLAN, SEE SHEETS CP101-P1 THROUGH CP108-P5.
- FOR PAVEMENT TYPICAL SECTIONS, SEE SHEETS CS001 THROUGH CS003.
- FOR GEOMETRIC LAYOUT, SEE SHEETS CS101-P1 THROUGH CS106-P6.
- FOR GRADING PLANS, SEE SHEETS CG101-P1 THROUGH CG109-P6. CONTRACTOR SHALL MEET ALL ELEVATIONS IDENTIFIED ON THE GRADING SHEETS IN ADDITION TO WHAT IS SHOWN ON THE SPOT ELEVATION PLANS.
- ALL PAVEMENT JOINTS SHALL MATCH INTO EXISTING PAVEMENT JOINTS AT TIE-IN LOCATIONS.

**LEGEND:**

- PROPOSED CONCRETE PAVEMENT (PCC)
- PROPOSED ASPHALT PAVEMENT
- EXISTING PAVEMENT
- EXISTING PAVEMENT JOINT
- PROPOSED GEOMETRY OUTSIDE OF CURRENT PHASE (FOR REFERENCE ONLY)
- + 0.00 EXISTING SPOT ELEVATION
- + 0.00 PROPOSED SPOT ELEVATION

MATCHLINE, SEE SHEET CP702-P1



**Jacobs**  
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HOUSTON, TEXAS 77072  
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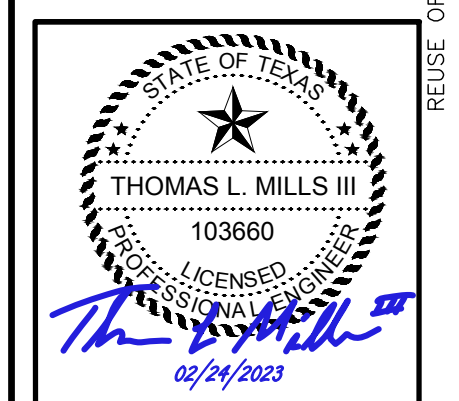
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FAA NON-STANDARD TAXIWAYS PROJECT

**PAVEMENT SPOT ELEVATION PLAN  
— PHASE 1**

PROJECT MGR: \_\_\_\_\_  
DESIGNER: \_\_\_\_\_  
DRAWN BY: \_\_\_\_\_  
CHECKED BY: \_\_\_\_\_  
SCALE: AS SHOWN  
DATE: 02/24/2023



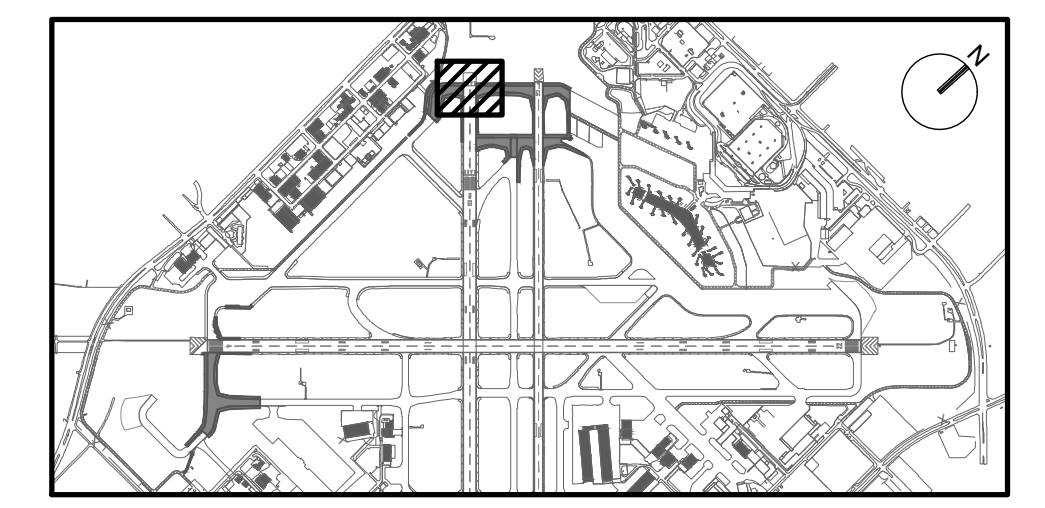
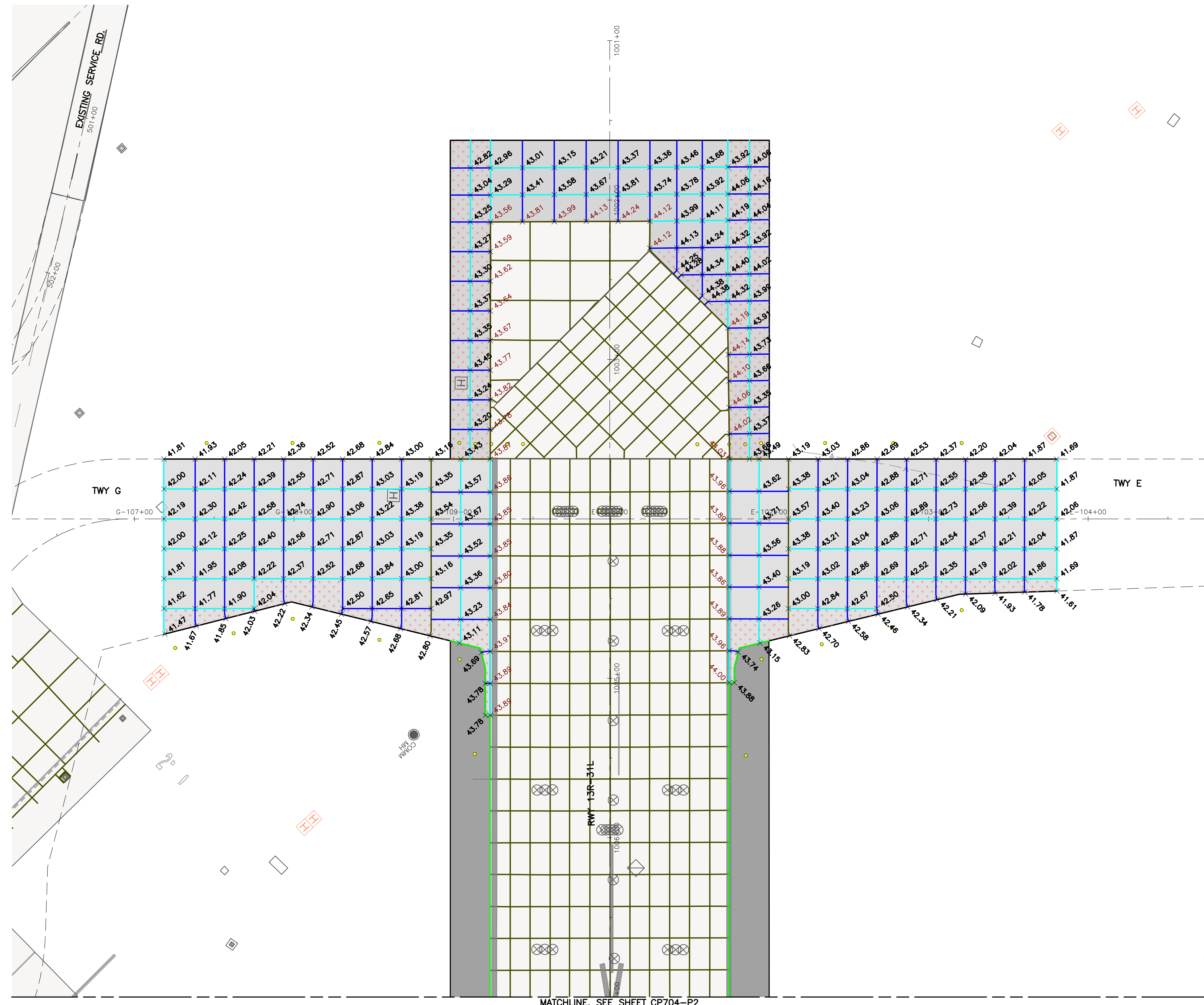
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DIRECTOR  
HOUSTON AIRPORT SYSTEM

PROJECT NO: 770  
C.I.P. NO: 3-48-0110-044  
H.A.S. NO: N/A  
SHEET NO: CP701-P1  
of









**KEY MAP**  
NTS

**NOTES:**

- FOR PAVEMENT JOINT TYPES AND REINFORCEMENT PLAN, SEE SHEETS CP101-P1 THROUGH CP108-P5.
- FOR PAVEMENT TYPICAL SECTIONS, SEE SHEETS CS001 THROUGH CS003.
- FOR GEOMETRIC LAYOUT, SEE SHEETS CS101-P1 THROUGH CS106-P6.
- FOR GRADING PLANS, SEE SHEETS CG101-P1 THROUGH CG109-P6. CONTRACTOR SHALL MEET ALL ELEVATIONS IDENTIFIED ON THE GRADING SHEETS IN ADDITION TO WHAT IS SHOWN ON THE SPOT ELEVATION PLANS.
- ALL PAVEMENT JOINTS SHALL MATCH INTO EXISTING PAVEMENT JOINTS AT TIE-IN LOCATIONS.

**LEGEND:**

- PROPOSED CONCRETE PAVEMENT (PCC)
- PROPOSED ASPHALT PAVEMENT
- EXISTING PAVEMENT
- EXISTING PAVEMENT JOINT
- PROPOSED GEOMETRY OUTSIDE OF CURRENT PHASE (FOR REFERENCE ONLY)
- +0.00 EXISTING SPOT ELEVATION
- +0.00 PROPOSED SPOT ELEVATION

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TEXAS P.E. FIRM F-2966

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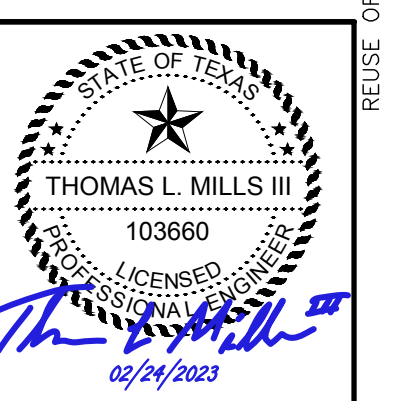
**REVISIONS**

NO.	DESCRIPTION	DATE	BY
0	ISSUED FOR BID	02/24/2023	SC

FAA NON-STANDARD TAXIWAYS PROJECT

**PAVEMENT SPOT ELEVATION PLAN**  
**- PHASE 2**

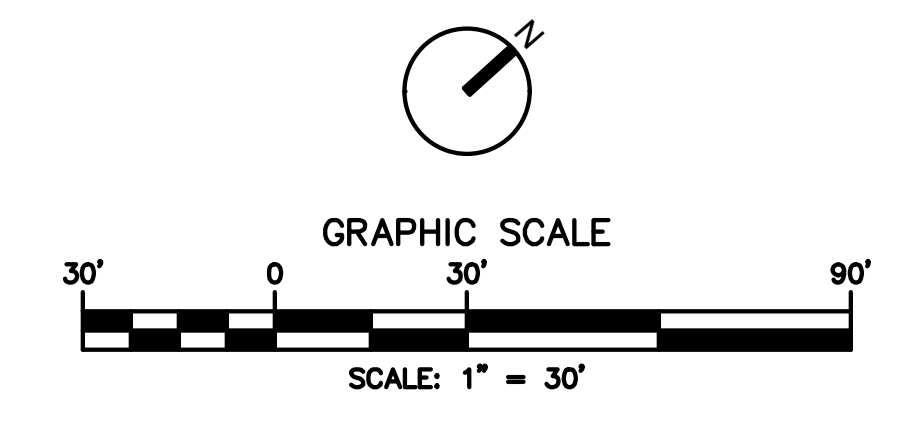
PROJECT MGR:	S. CHILDERS
DESIGNER:	D. CRAWFORD
DRAWN BY:	C. MCLAIN
CHECKED BY:	R. EHTESHAM
SCALE:	AS SHOWN
DATE:	02/24/2023



**APPROVED BY:** \_\_\_\_\_ **DATE:** \_\_\_\_\_

DIRECTOR  
HOUSTON AIRPORT SYSTEM

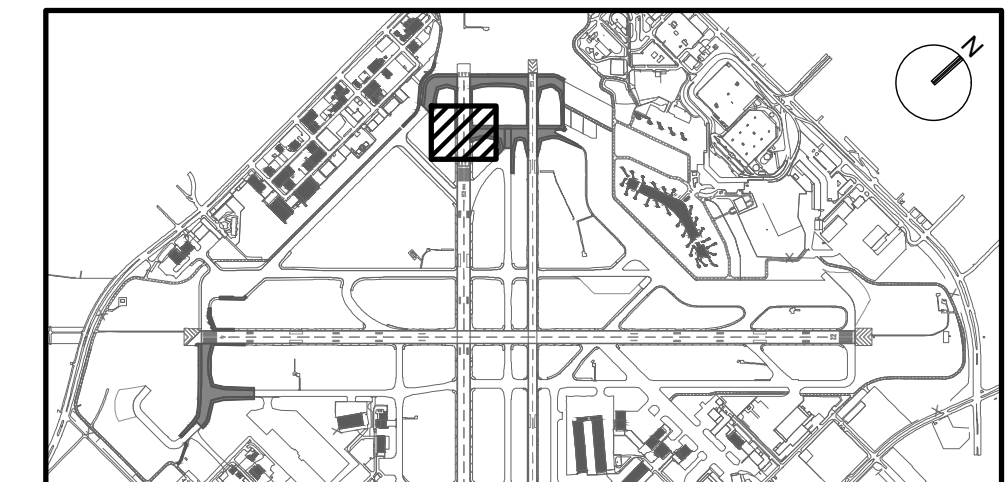
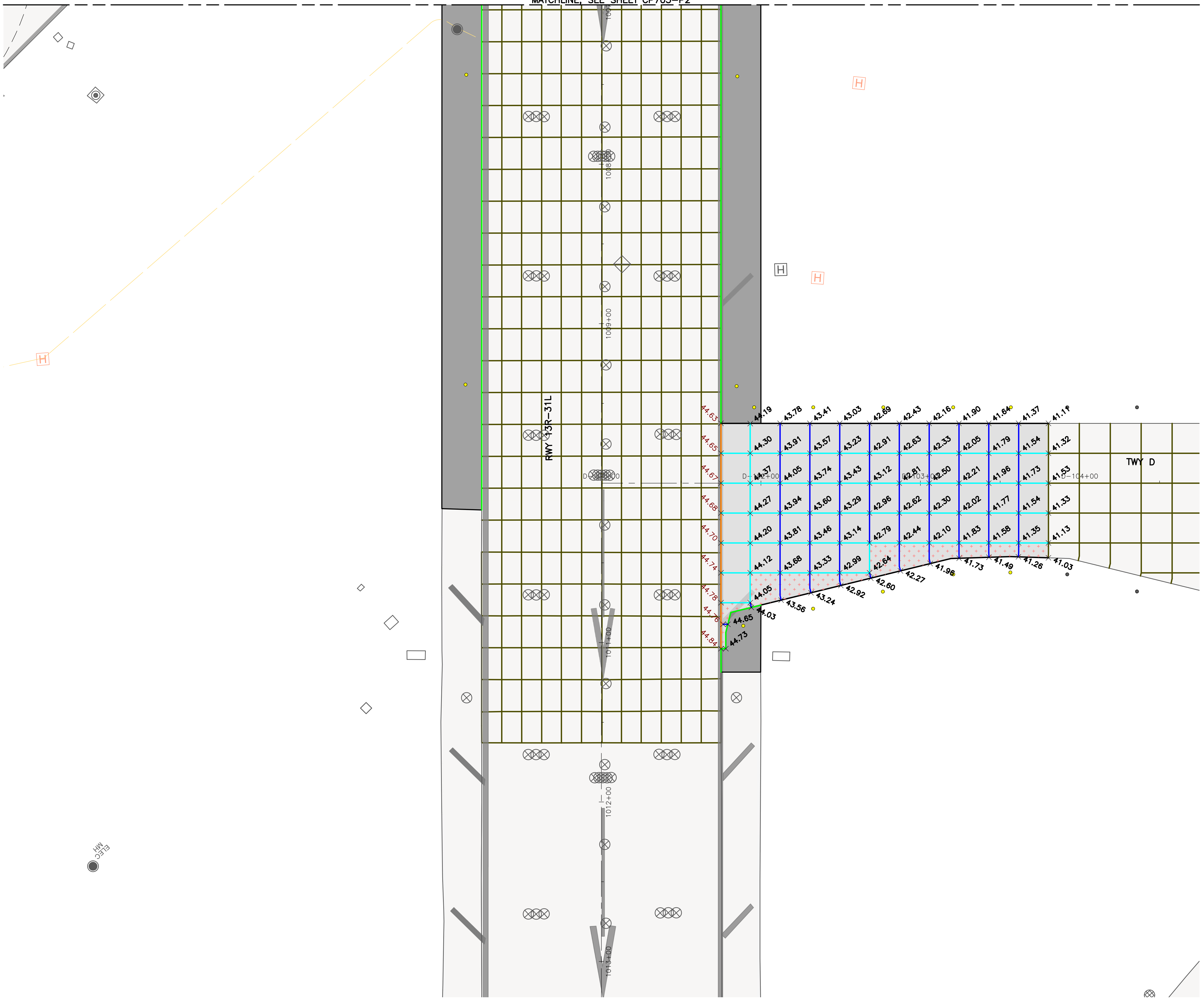
PROJECT NO:	770
C.I.P. NO:	3-48-0110-044
H.A.S. NO:	N/A
SHEET NO:	CP703-P2



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MATCHLINE, SEE SHEET CP703-P2



**KEY MAP**  
NTS

**NOTES:**

- FOR PAVEMENT JOINT TYPES AND REINFORCEMENT PLAN, SEE SHEETS CP101-P1 THROUGH CP108-P5.
- FOR PAVEMENT TYPICAL SECTIONS, SEE SHEETS CS001 THROUGH CS003.
- FOR GEOMETRIC LAYOUT, SEE SHEETS CS101-P1 THROUGH CS106-P6.
- FOR GRADING PLANS, SEE SHEETS CG101-P1 THROUGH CG109-P6. CONTRACTOR SHALL MEET ALL ELEVATIONS IDENTIFIED ON THE GRADING SHEETS IN ADDITION TO WHAT IS SHOWN ON THE SPOT ELEVATION PLANS.
- ALL PAVEMENT JOINTS SHALL MATCH INTO EXISTING PAVEMENT JOINTS AT TIE-IN LOCATIONS.

**LEGEND:**

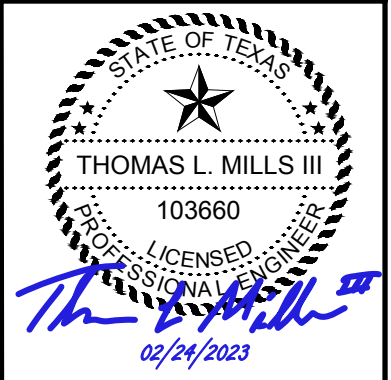
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- PROPOSED ASPHALT PAVEMENT
- EXISTING PAVEMENT
- EXISTING PAVEMENT JOINT
- PROPOSED GEOMETRY OUTSIDE OF CURRENT PHASE (FOR REFERENCE ONLY)
- EXISTING SPOT ELEVATION
- PROPOSED SPOT ELEVATION

REVISIONS

NO.	DESCRIPTION	DATE	BY
0	ISSUED FOR BID	02/24/2023	SC

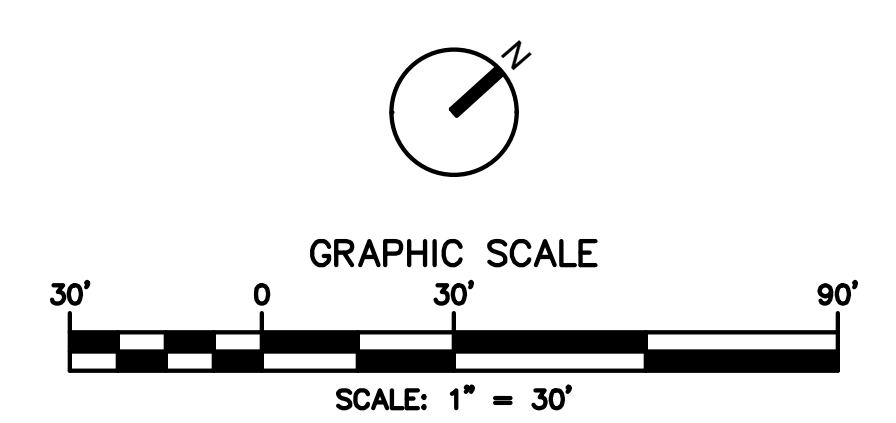
FAA NON-STANDARD TAXIWAYS PROJECT  
 PAVEMENT SPOT ELEVATION PLAN  
 - PHASE 2

PROJECT MGR: S. CHILDERS  
 DESIGNER: D. CRAWFORD  
 DRAWN BY: C. MCCLAIN  
 CHECKED BY: R. EHTESHAM  
 SCALE: AS SHOWN  
 DATE: 02/24/2023

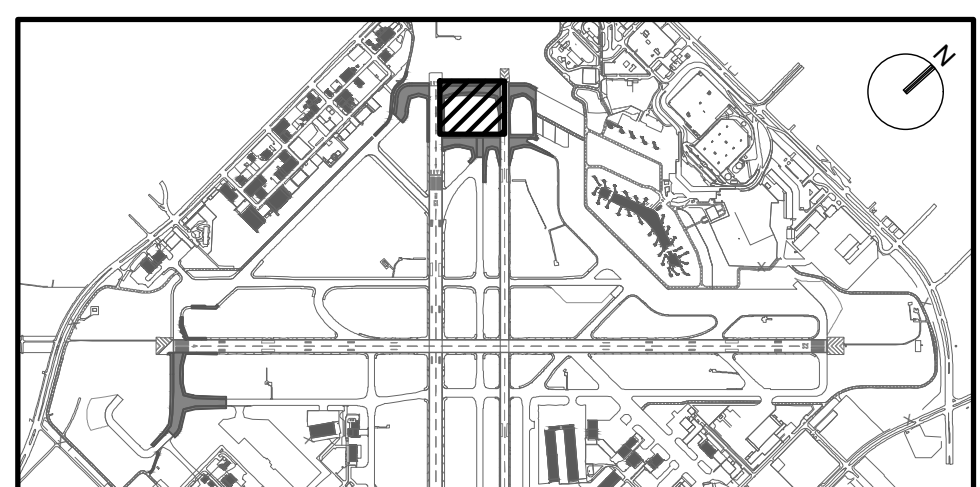
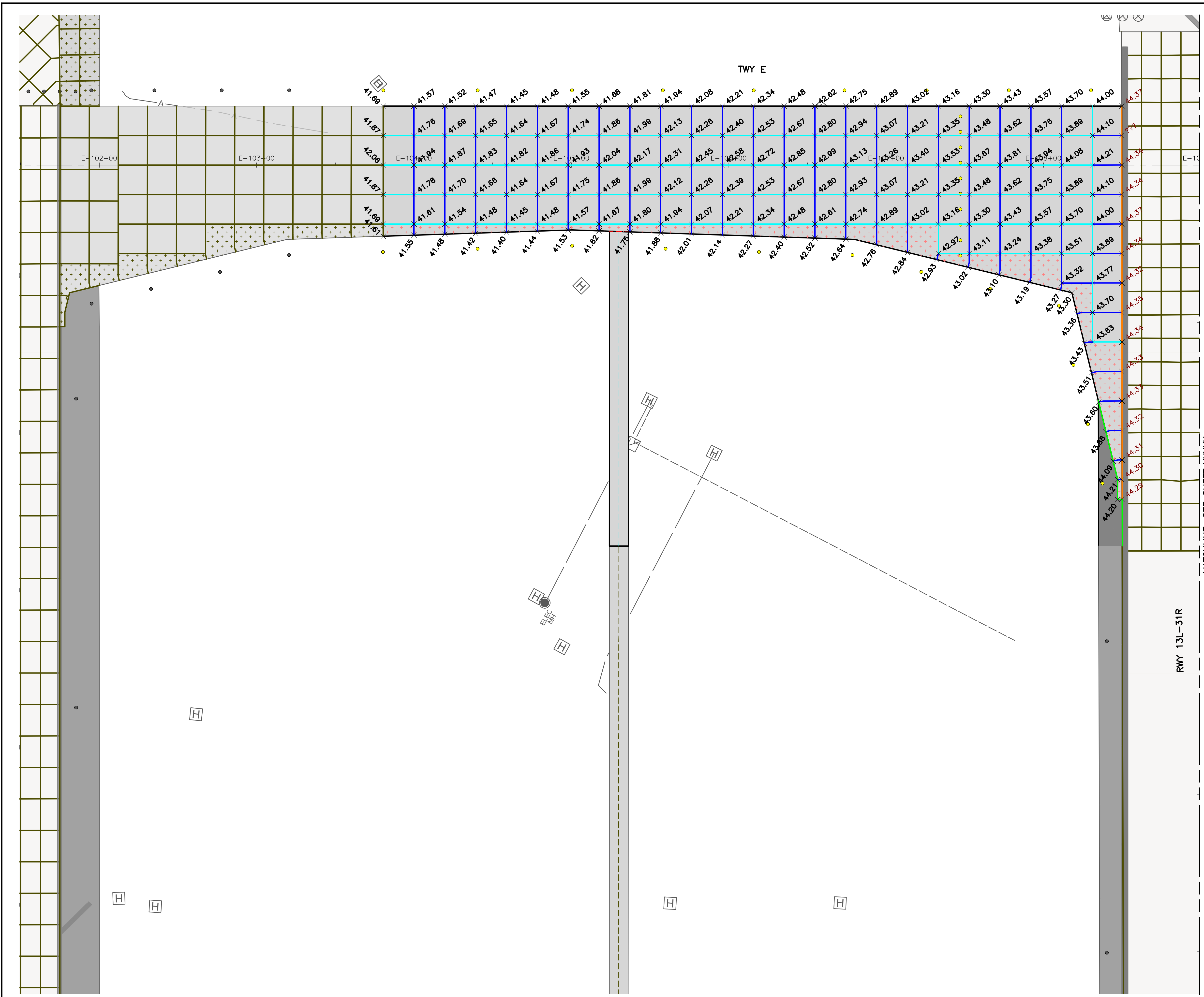


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 DIRECTOR  
 HOUSTON AIRPORT SYSTEM

PROJECT NO: 770  
 C.I.P. NO: 3-48-0110-044  
 H.A.S. NO: N/A  
 SHEET NO: CP704-P2  
 of







**KEY MAP**  
NTS

**NOTES:**

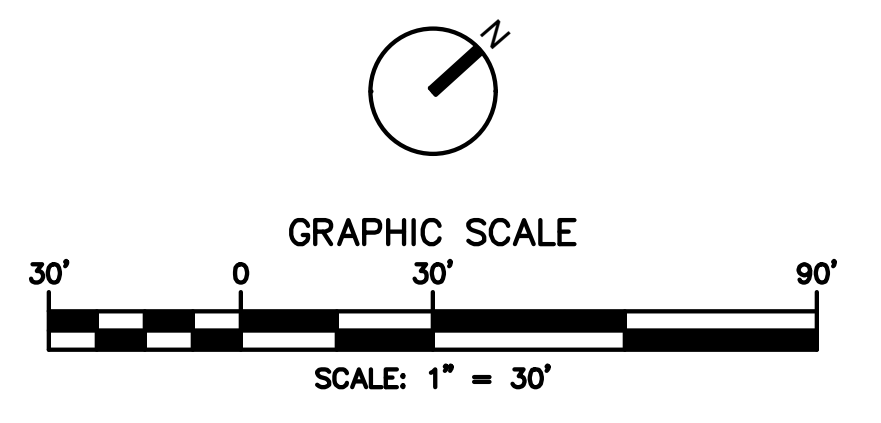
- FOR PAVEMENT JOINT TYPES AND REINFORCEMENT PLAN, SEE SHEETS CP101-P1 THROUGH CP108-P5.
- FOR PAVEMENT TYPICAL SECTIONS, SEE SHEETS CS001 THROUGH CS003.
- FOR GEOMETRIC LAYOUT, SEE SHEETS CS101-P1 THROUGH CS106-P6.
- FOR GRADING PLANS, SEE SHEETS CG101-P1 THROUGH CG109-P6. CONTRACTOR SHALL MEET ALL ELEVATIONS IDENTIFIED ON THE GRADING SHEETS IN ADDITION TO WHAT IS SHOWN ON THE SPOT ELEVATION PLANS.
- ALL PAVEMENT JOINTS SHALL MATCH INTO EXISTING PAVEMENT JOINTS AT TIE-IN LOCATIONS.

**LEGEND:**

- PROPOSED CONCRETE PAVEMENT (PCC)
- PROPOSED ASPHALT PAVEMENT
- EXISTING PAVEMENT
- EXISTING PAVEMENT JOINT
- PROPOSED GEOMETRY OUTSIDE OF CURRENT PHASE (FOR REFERENCE ONLY)
- EXISTING SPOT ELEVATION
- PROPOSED SPOT ELEVATION

MATCHLINE, SEE SHEET XXXX

RWY 13L-31R

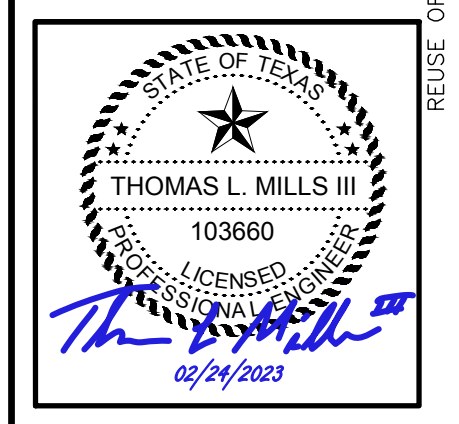


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FAA NON-STANDARD TAXIWAYS PROJECT

PAVEMENT SPOT ELEVATION PLAN  
- PHASE 3

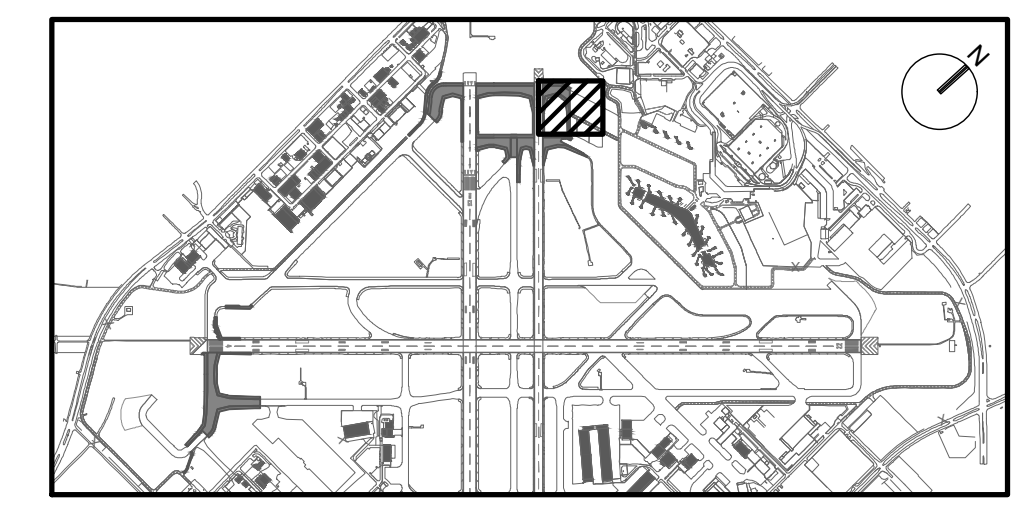
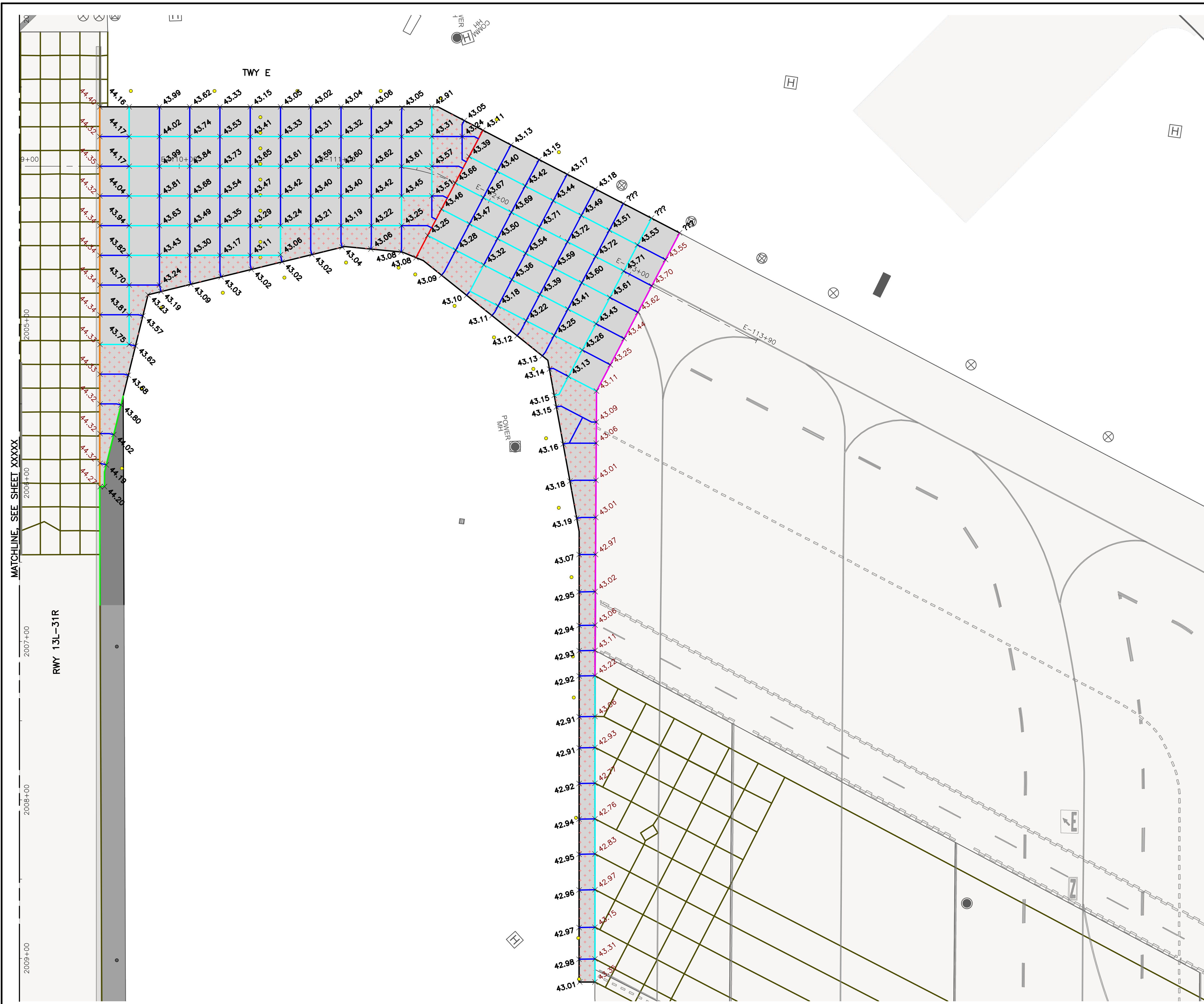
PROJECT MGR:	S. CHILDERS
DESIGNER:	D. CRAWFORD
DRAWN BY:	C. MCLAIN
CHECKED BY:	R. EHTESHAM
SCALE:	AS SHOWN
DATE:	02/24/2023



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PROJECT NO:	770
C.I.P. NO:	3-48-0110-044
H.A.S. NO:	N/A
SHEET NO:	CP705-P3
of	





KEY MAP  
NTS

NOTES:

- FOR PAVEMENT JOINT TYPES AND REINFORCEMENT PLAN, SEE SHEETS CP101-P1 THROUGH CP108-P5.
- FOR PAVEMENT TYPICAL SECTIONS, SEE SHEETS CS001 THROUGH CS003.
- FOR GEOMETRIC LAYOUT, SEE SHEETS CS101-P1 THROUGH CS106-P6.
- FOR GRADING PLANS, SEE SHEETS CG101-P1 THROUGH CG109-P6. CONTRACTOR SHALL MEET ALL ELEVATIONS IDENTIFIED ON THE GRADING SHEETS IN ADDITION TO WHAT IS SHOWN ON THE SPOT ELEVATION PLANS.
- ALL PAVEMENT JOINTS SHALL MATCH INTO EXISTING PAVEMENT JOINTS AT TIE-IN LOCATIONS.

LEGEND:

- PROPOSED CONCRETE PAVEMENT (PCC)
- PROPOSED ASPHALT PAVEMENT
- EXISTING PAVEMENT
- EXISTING PAVEMENT JOINT
- PROPOSED GEOMETRY OUTSIDE OF CURRENT PHASE (FOR REFERENCE ONLY)
- EXISTING SPOT ELEVATION
- PROPOSED SPOT ELEVATION

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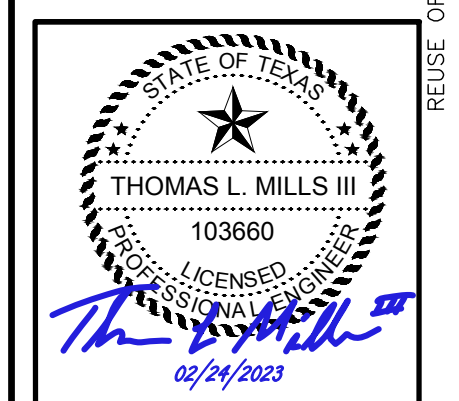
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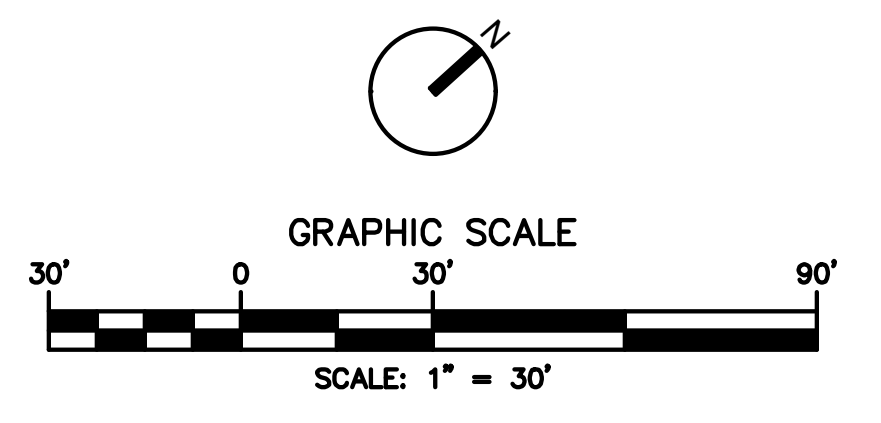
PAVEMENT SPOT ELEVATION PLAN  
- PHASE 3

PROJECT MGR:	S. CHILDERS
DESIGNER:	D. CRAWFORD
DRAWN BY:	C. MCLAIN
CHECKED BY:	R. EHTESHAM
SCALE:	AS SHOWN
DATE:	02/24/2023

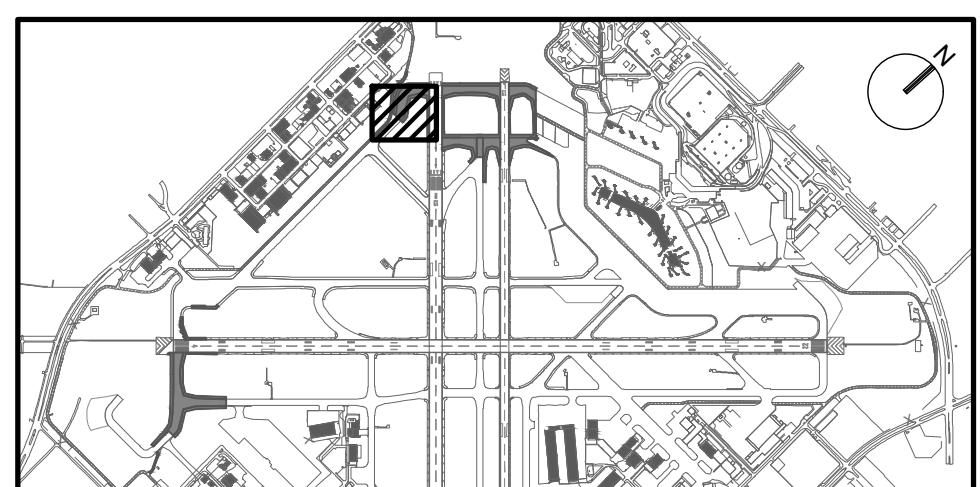
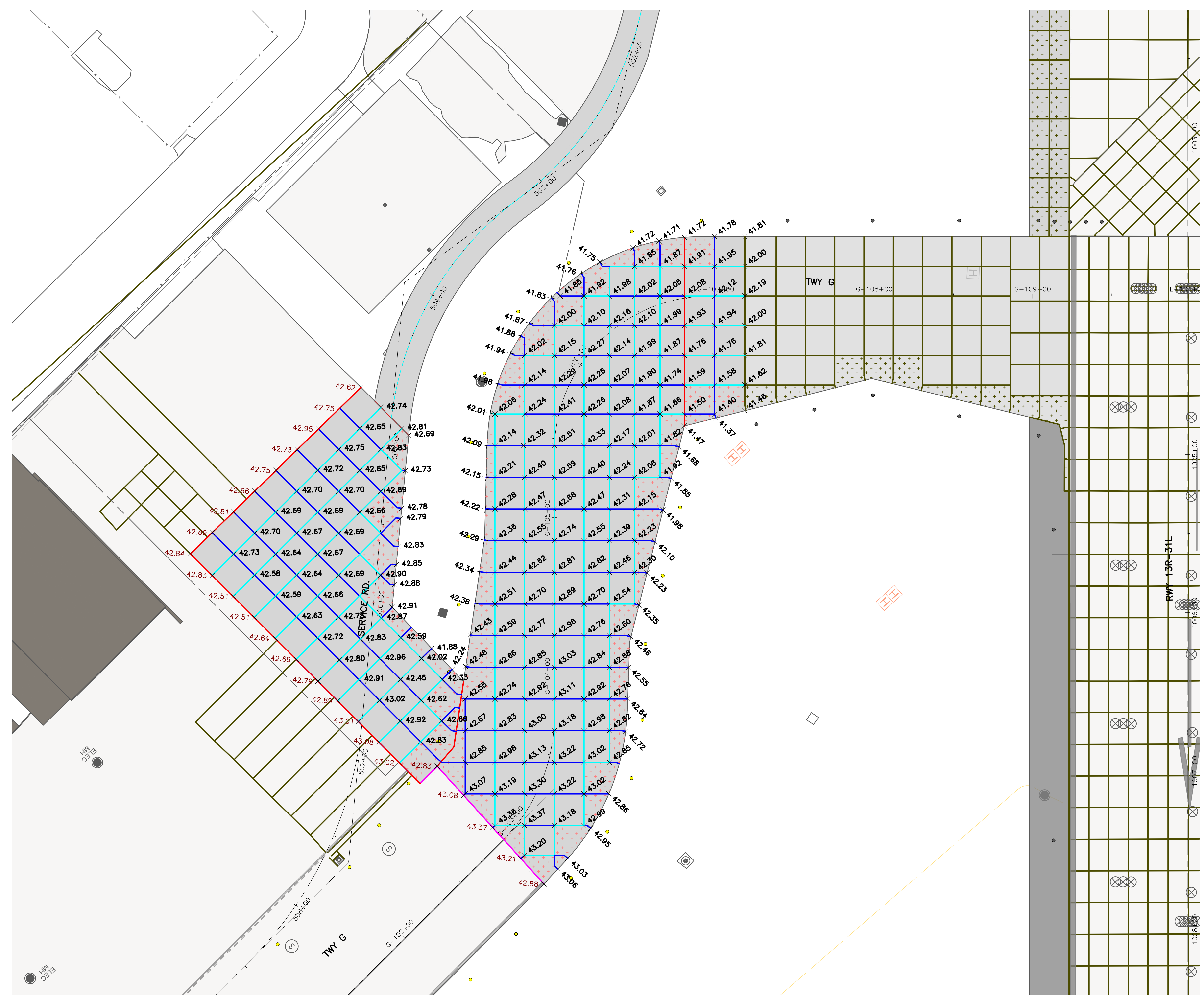


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PROJECT NO:	770
C.I.P. NO:	3-48-0110-044
H.A.S. NO:	N/A
SHEET NO:	CP706-P3
of	







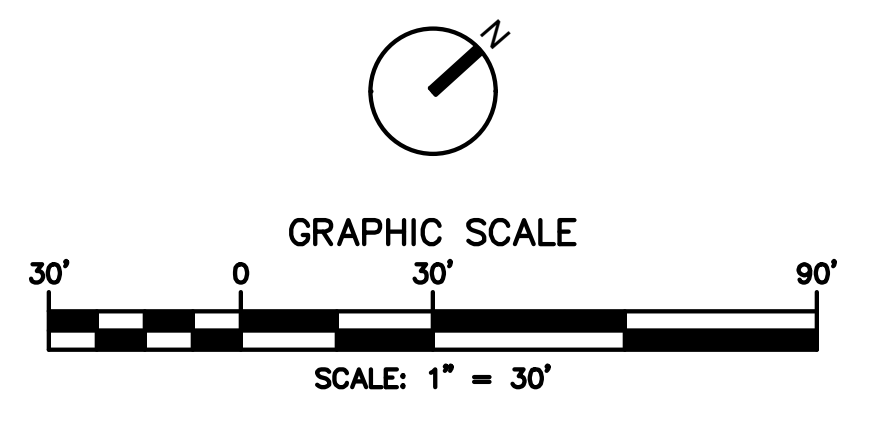
**KEY MAP**  
NTS

**NOTES:**

- FOR PAVEMENT JOINT TYPES AND REINFORCEMENT PLAN, SEE SHEETS CP101-P1 THROUGH CP108-P5.
- FOR PAVEMENT TYPICAL SECTIONS, SEE SHEETS CS001 THROUGH CS003.
- FOR GEOMETRIC LAYOUT, SEE SHEETS CS101-P1 THROUGH CS106-P6.
- FOR GRADING PLANS, SEE SHEETS CG101-P1 THROUGH CG109-P6. CONTRACTOR SHALL MEET ALL ELEVATIONS IDENTIFIED ON THE GRADING SHEETS IN ADDITION TO WHAT IS SHOWN ON THE SPOT ELEVATION PLANS.
- ALL PAVEMENT JOINTS SHALL MATCH INTO EXISTING PAVEMENT JOINTS AT TIE-IN LOCATIONS.

**LEGEND:**

- PROPOSED CONCRETE PAVEMENT (PCC)
- PROPOSED ASPHALT PAVEMENT
- EXISTING PAVEMENT
- EXISTING PAVEMENT JOINT
- PROPOSED GEOMETRY OUTSIDE OF CURRENT PHASE (FOR REFERENCE ONLY)
- +0.00 EXISTING SPOT ELEVATION
- +0.00 PROPOSED SPOT ELEVATION



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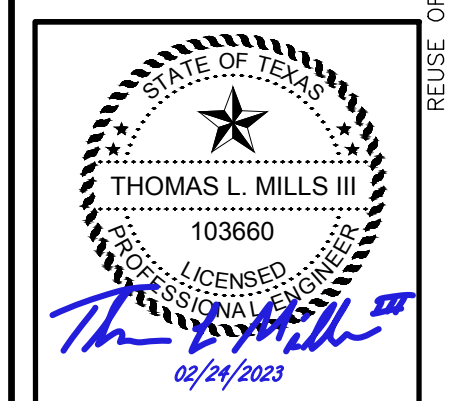
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NO.	DESCRIPTION	DATE	BY
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FAA NON-STANDARD TAXIWAYS PROJECT

**PAVEMENT SPOT ELEVATION PLAN - PHASE 4**

PROJECT MGR:	S. CHILDERS
DESIGNER:	D. CRAWFORD
DRAWN BY:	C. MCLAIN
CHECKED BY:	R. EHTESHAM
SCALE:	AS SHOWN
DATE:	02/24/2023



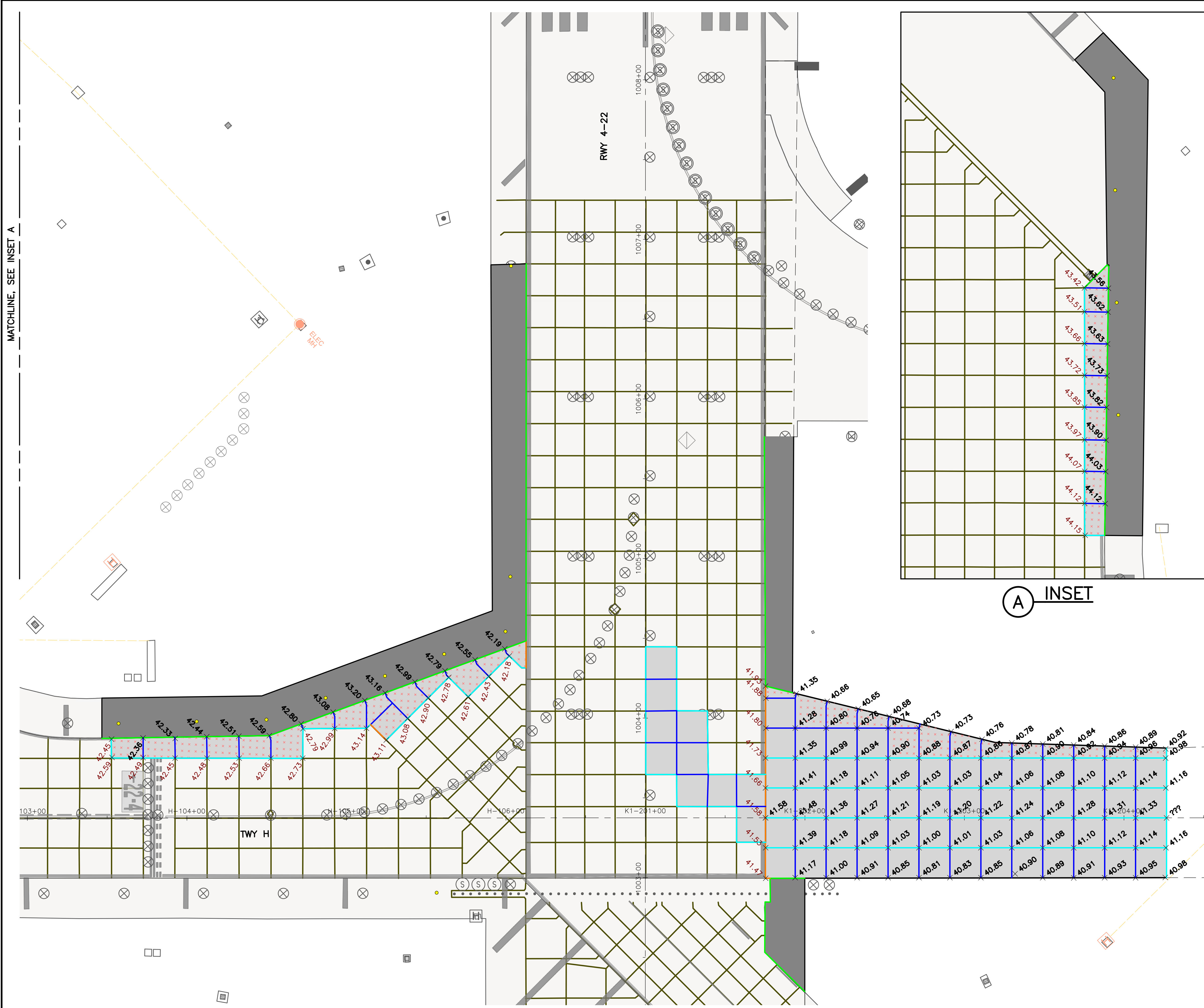
APPROVED BY: \_\_\_\_\_ DATE: \_\_\_\_\_

\_\_\_\_\_  
DIRECTOR  
HOUSTON AIRPORT SYSTEM

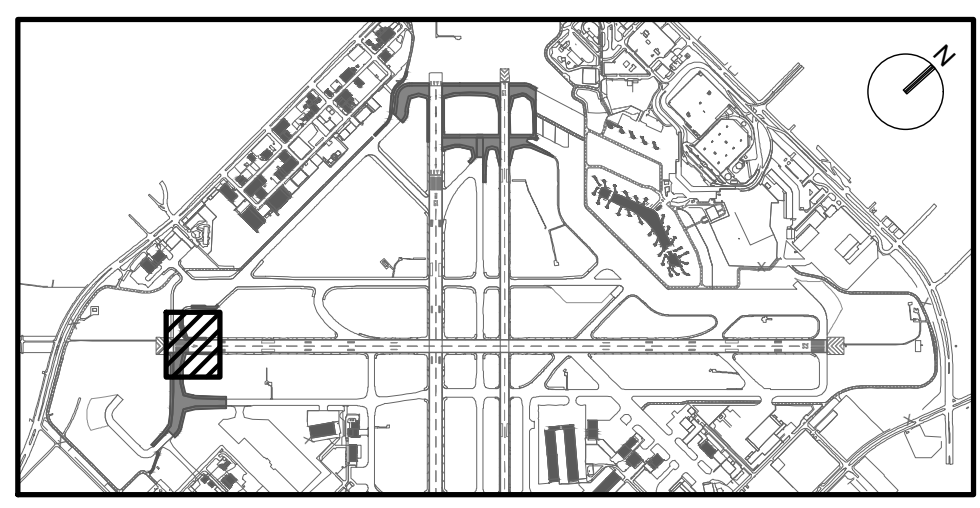
PROJECT NO:	770
C.I.P. NO:	3-48-0110-044
H.A.S. NO:	N/A
SHEET NO:	CP707-P4
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MATCHLINE, SEE INSET A



KEY MAP  
NTS

NOTES:

- FOR PAVEMENT JOINT TYPES AND REINFORCEMENT PLAN, SEE SHEETS CP101-P1 THROUGH CP108-P5.
- FOR PAVEMENT TYPICAL SECTIONS, SEE SHEETS CS001 THROUGH CS003.
- FOR GEOMETRIC LAYOUT, SEE SHEETS CS101-P1 THROUGH CS106-P6.
- FOR GRADING PLANS, SEE SHEETS CG101-P1 THROUGH CG109-P6. CONTRACTOR SHALL MEET ALL ELEVATIONS IDENTIFIED ON THE GRADING SHEETS IN ADDITION TO WHAT IS SHOWN ON THE SPOT ELEVATION PLANS.
- ALL PAVEMENT JOINTS SHALL MATCH INTO EXISTING PAVEMENT JOINTS AT TIE-IN LOCATIONS.

LEGEND:

- PROPOSED CONCRETE PAVEMENT (PCC)
- PROPOSED ASPHALT PAVEMENT
- EXISTING PAVEMENT
- EXISTING PAVEMENT JOINT
- PROPOSED GEOMETRY OUTSIDE OF CURRENT PHASE (FOR REFERENCE ONLY)
- EXISTING SPOT ELEVATION
- PROPOSED SPOT ELEVATION

A INSET

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TEXAS P.E. FIRM F-2966

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REVISIONS			
NO.	DESCRIPTION	DATE	BY
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FAA NON-STANDARD TAXIWAYS PROJECT

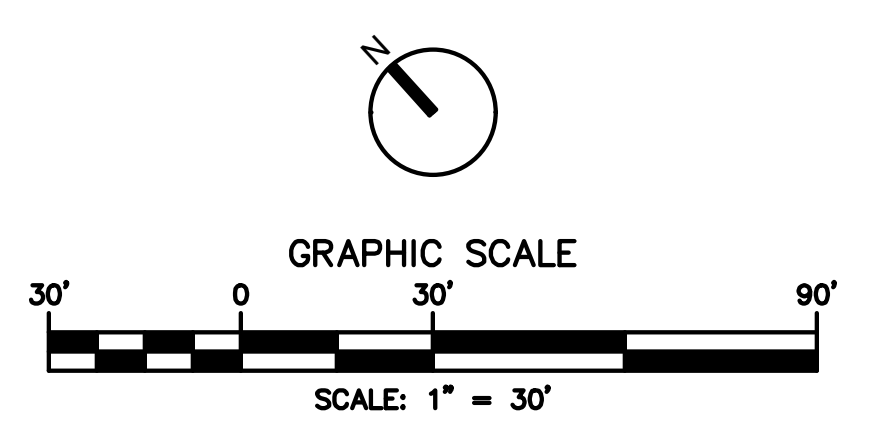
PAVEMENT SPOT ELEVATION PLAN  
- PHASE 5

PROJECT MGR:	S. CHILDERS
DESIGNER:	D. CRAWFORD
DRAWN BY:	C. MCLAIN
CHECKED BY:	R. EHTESHAM
SCALE:	AS SHOWN
DATE:	02/24/2023

THOMAS L. MILLS III  
103660  
PROFESSIONAL ENGINEER  
STATE OF TEXAS  
02/24/2023

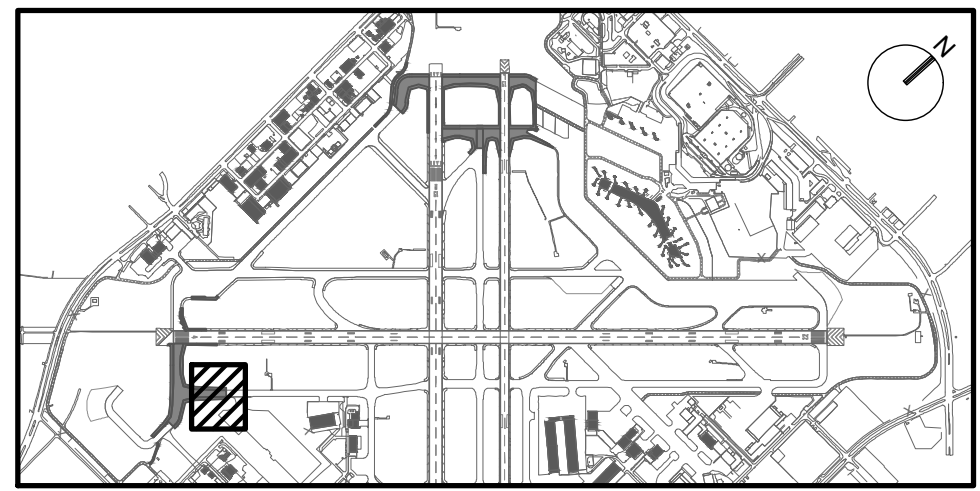
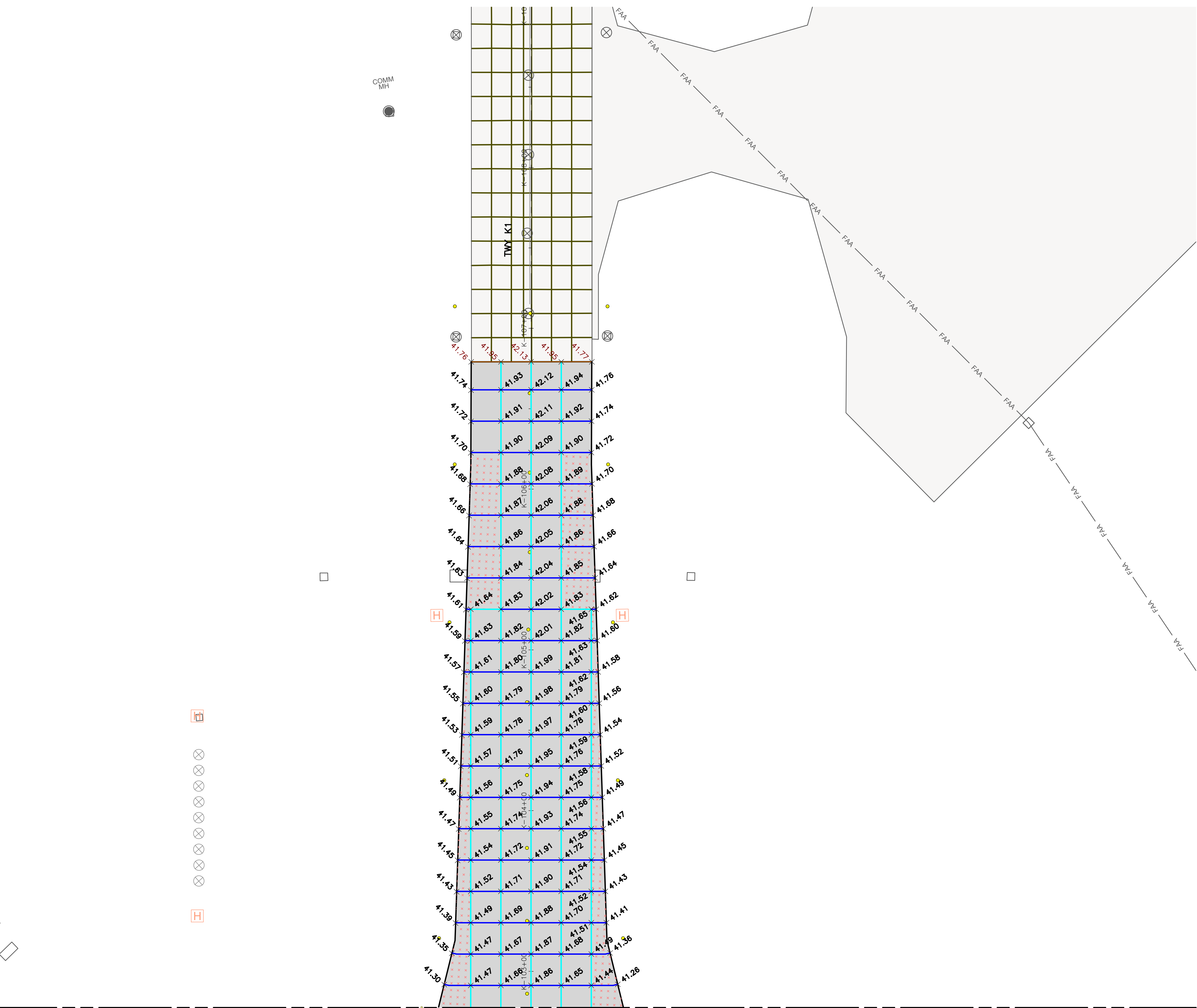
APPROVED BY:	DATE:
DIRECTOR	HOUSTON AIRPORT SYSTEM

PROJECT NO:	770
C.I.P. NO:	3-48-0110-044
H.A.S. NO:	N/A
SHEET NO:	CP708-P5
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**KEY MAP**  
NTS

**NOTES:**

- FOR PAVEMENT JOINT TYPES AND REINFORCEMENT PLAN, SEE SHEETS CP101-P1 THROUGH CP108-P5.
- FOR PAVEMENT TYPICAL SECTIONS, SEE SHEETS CS001 THROUGH CS003.
- FOR GEOMETRIC LAYOUT, SEE SHEETS CS101-P1 THROUGH CS106-P6.
- FOR GRADING PLANS, SEE SHEETS CG101-P1 THROUGH CG109-P6. CONTRACTOR SHALL MEET ALL ELEVATIONS IDENTIFIED ON THE GRADING SHEETS IN ADDITION TO WHAT IS SHOWN ON THE SPOT ELEVATION PLANS.
- ALL PAVEMENT JOINTS SHALL MATCH INTO EXISTING PAVEMENT JOINTS AT TIE-IN LOCATIONS.

**LEGEND:**

- PROPOSED CONCRETE PAVEMENT (PCC)
- PROPOSED ASPHALT PAVEMENT
- EXISTING PAVEMENT
- EXISTING PAVEMENT JOINT
- PROPOSED GEOMETRY OUTSIDE OF CURRENT PHASE (FOR REFERENCE ONLY)
- EXISTING SPOT ELEVATION
- PROPOSED SPOT ELEVATION

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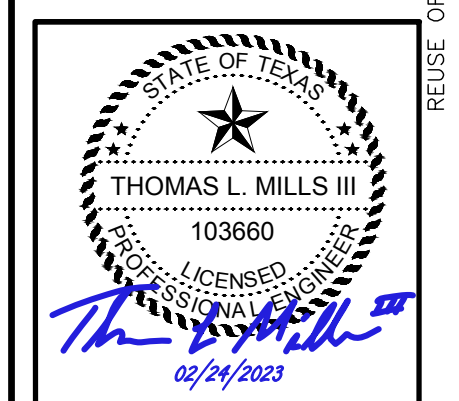
**REVISIONS**

NO.	DESCRIPTION	DATE	BY
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FAA NON-STANDARD TAXIWAYS PROJECT

PAVEMENT SPOT ELEVATION PLAN  
- PHASE 6

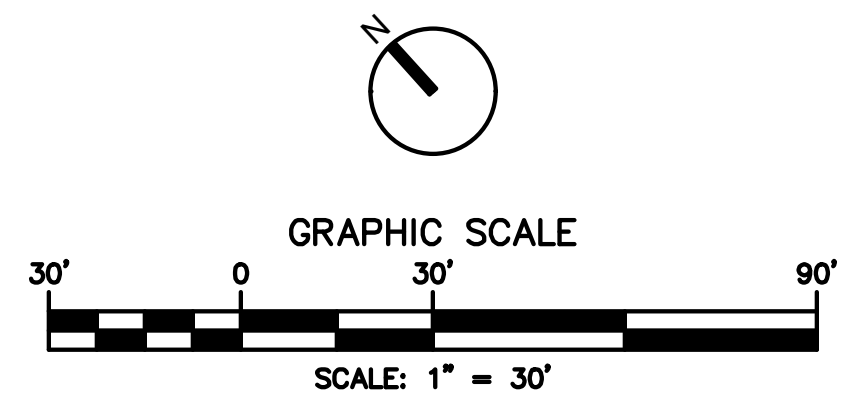
PROJECT MGR:	S. CHILDERS
DESIGNER:	D. CRAWFORD
DRAWN BY:	C. MCLAIN
CHECKED BY:	R. EHTESHAM
SCALE:	AS SHOWN
DATE:	02/24/2023



APPROVED BY: \_\_\_\_\_ DATE: \_\_\_\_\_

DIRECTOR  
HOUSTON AIRPORT SYSTEM

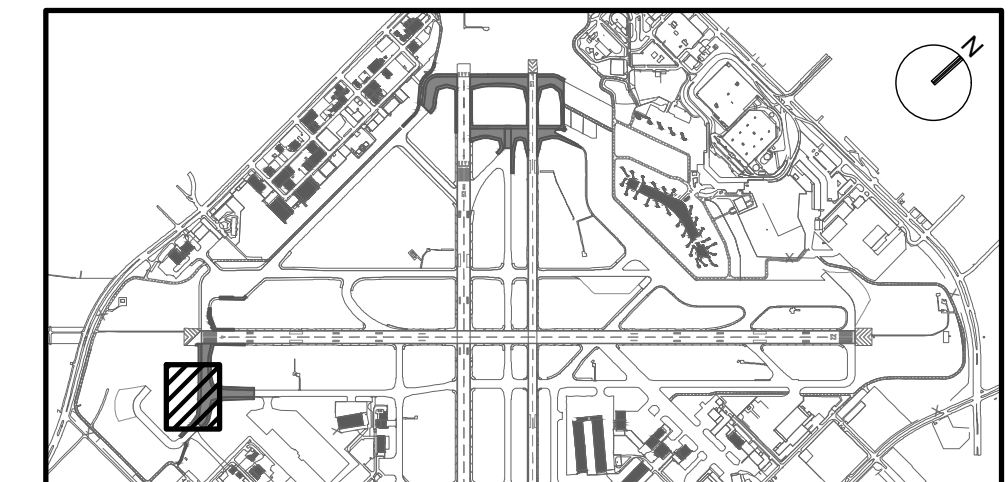
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C.I.P. NO:	3-48-0110-044
H.A.S. NO:	N/A
SHEET NO:	CP709-P6



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MATCHLINE, SEE SHEET CP109-P6



KEY MAP  
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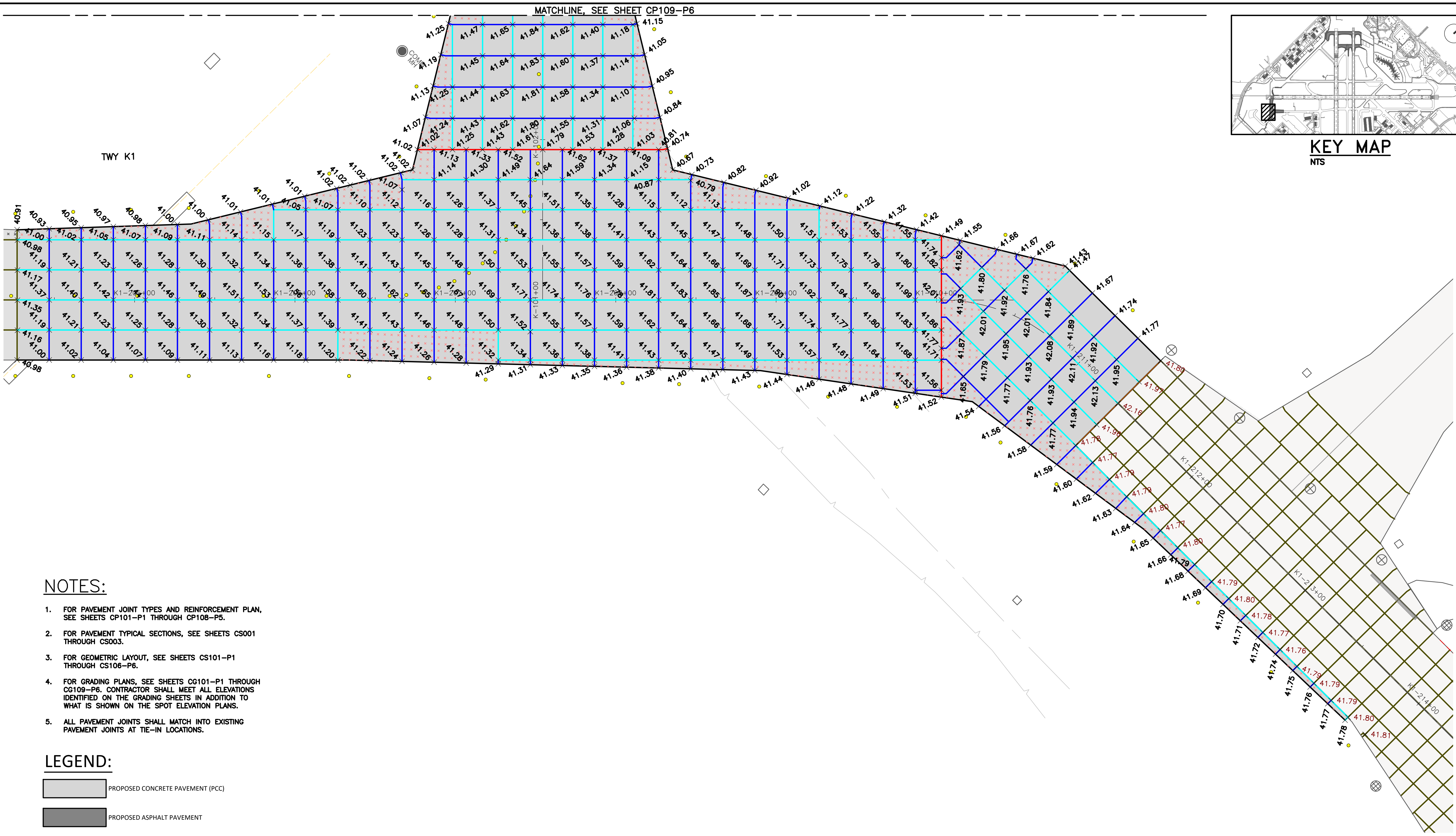
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**NOTES:**

- FOR PAVEMENT JOINT TYPES AND REINFORCEMENT PLAN, SEE SHEETS CP101-P1 THROUGH CP108-P5.
- FOR PAVEMENT TYPICAL SECTIONS, SEE SHEETS CS001 THROUGH CS003.
- FOR GEOMETRIC LAYOUT, SEE SHEETS CS101-P1 THROUGH CS106-P6.
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- ALL PAVEMENT JOINTS SHALL MATCH INTO EXISTING PAVEMENT JOINTS AT TIE-IN LOCATIONS.

**LEGEND:**

- PROPOSED CONCRETE PAVEMENT (PCC)
- PROPOSED ASPHALT PAVEMENT
- EXISTING PAVEMENT
- EXISTING PAVEMENT JOINT
- PROPOSED GEOMETRY OUTSIDE OF CURRENT PHASE (FOR REFERENCE ONLY)
- EXISTING SPOT ELEVATION
- PROPOSED SPOT ELEVATION

FAA NON-STANDARD TAXIWAYS PROJECT

PAVEMENT SPOT ELEVATION PLAN  
- PHASE 6

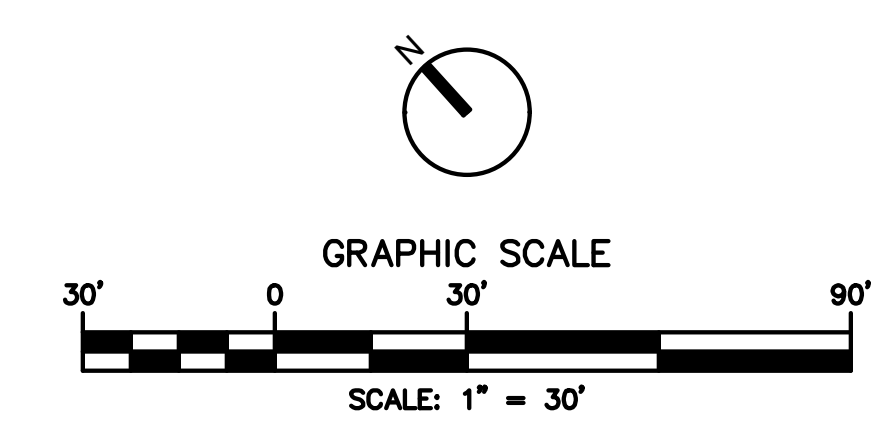
PROJECT MGR: S. CHILDERS  
DESIGNER: D. CRAWFORD  
DRAWN BY: C. MCCLAIN  
CHECKED BY: R. EHTESHAM  
SCALE: AS SHOWN  
DATE: 02/24/2023



APPROVED BY: DATE:

DIRECTOR  
HOUSTON AIRPORT SYSTEM

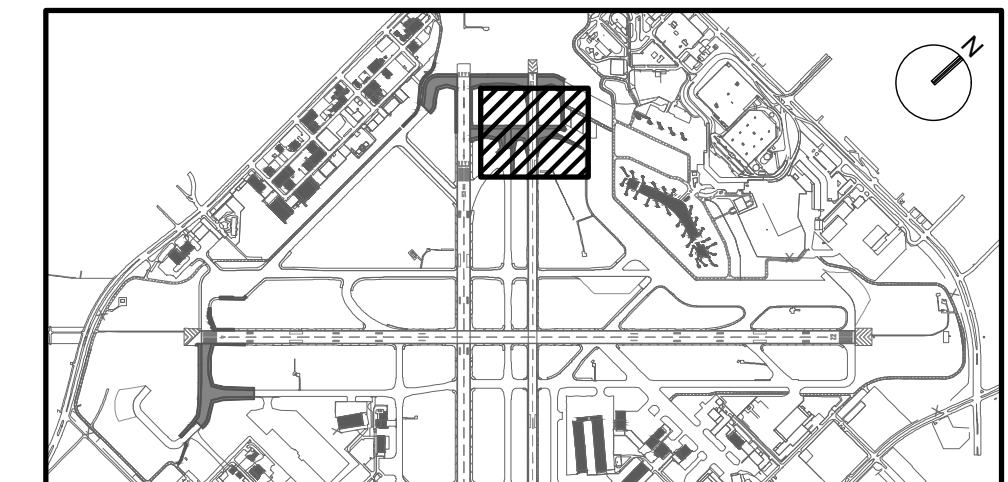
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C.I.P. NO: 3-48-0110-044  
H.A.S. NO: N/A  
SHEET NO: CP710-P6  
of





PHASE 1 MARKING POINTS TABLE		
POINT ID	NORTHING	EASTING
P1-C-101	13804573.74	3149462.62
P1-C-102	13804640.45	3149387.19
P1-C-103	13804770.40	3149380.67
P1-C-104	13804843.00	3149446.33
P1-C-105	13804986.51	3149576.13
P1-C-106	13805133.87	3149709.41
P1-C-107	13805162.96	3149752.07
P1-C-108	13805184.17	3149809.43
P1-C-109	13804849.53	3149576.26
P1-C-110	13804715.41	3149724.62
P1-C-111	13804856.55	3149582.66
P1-E-201	13804405.28	3149704.57
P1-E-202	13804590.17	3149513.26
P1-E-203	13804762.31	3149587.69

PHASE 1 MARKING POINTS TABLE		
POINT ID	NORTHING	EASTING
P1-E-204	13804735.91	3149636.66
P1-E-205	13805159.02	3149681.60
P1-E-206	13805198.13	3149739.05
P1-E-207	13805226.39	3149708.47
P1-E-208	13805241.39	3149691.85
P1-E-209	13804823.09	3149691.50
P1-E-210	13804858.68	3149667.40
P1-E-211	13805161.01	3149868.76
P1-E-212	13805199.79	3149967.21



KEY MAP  
NTS

**HOUSTON AIRPORT SYSTEM**  
WILLIAM P. HOBBY AIRPORT  
HOUSTON TEXAS

**Jacobs**  
JACOBS ENGINEERING GROUP INC.  
5995 ROGERDALE ROAD  
HOUSTON, TEXAS 77072  
+1-832-351-6000  
WWW.JACOBS.COM  
TEXAS P.E. FIRM F-2966

VERIFY SCALE  
BAR IS ONE INCH ON  
ORIGINAL DRAWING.  
0 1"

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- BLACKOUT EXISTING MARKING

REVISIONS			
NO.	DESCRIPTION	DATE	BY
0	ISSUED FOR BID	02/24/2023	SC

FAA NON-STANDARD TAXIWAYS PROJECT  
 PAVEMENT MARKING PLAN  
 - PHASE 1

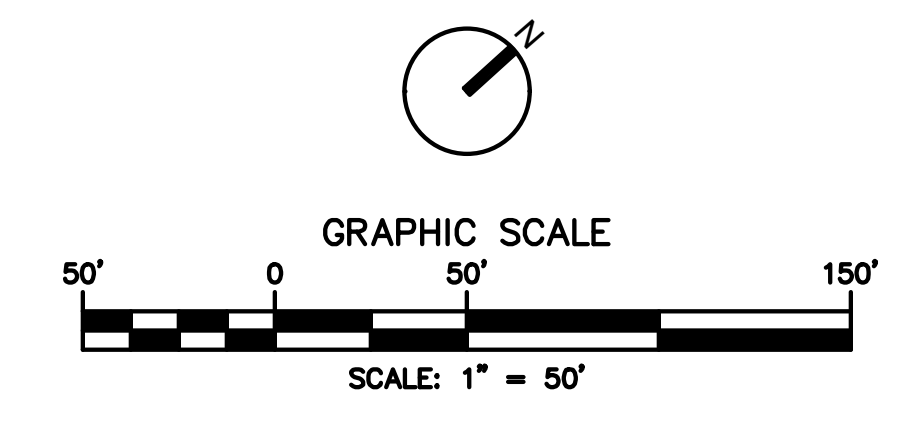
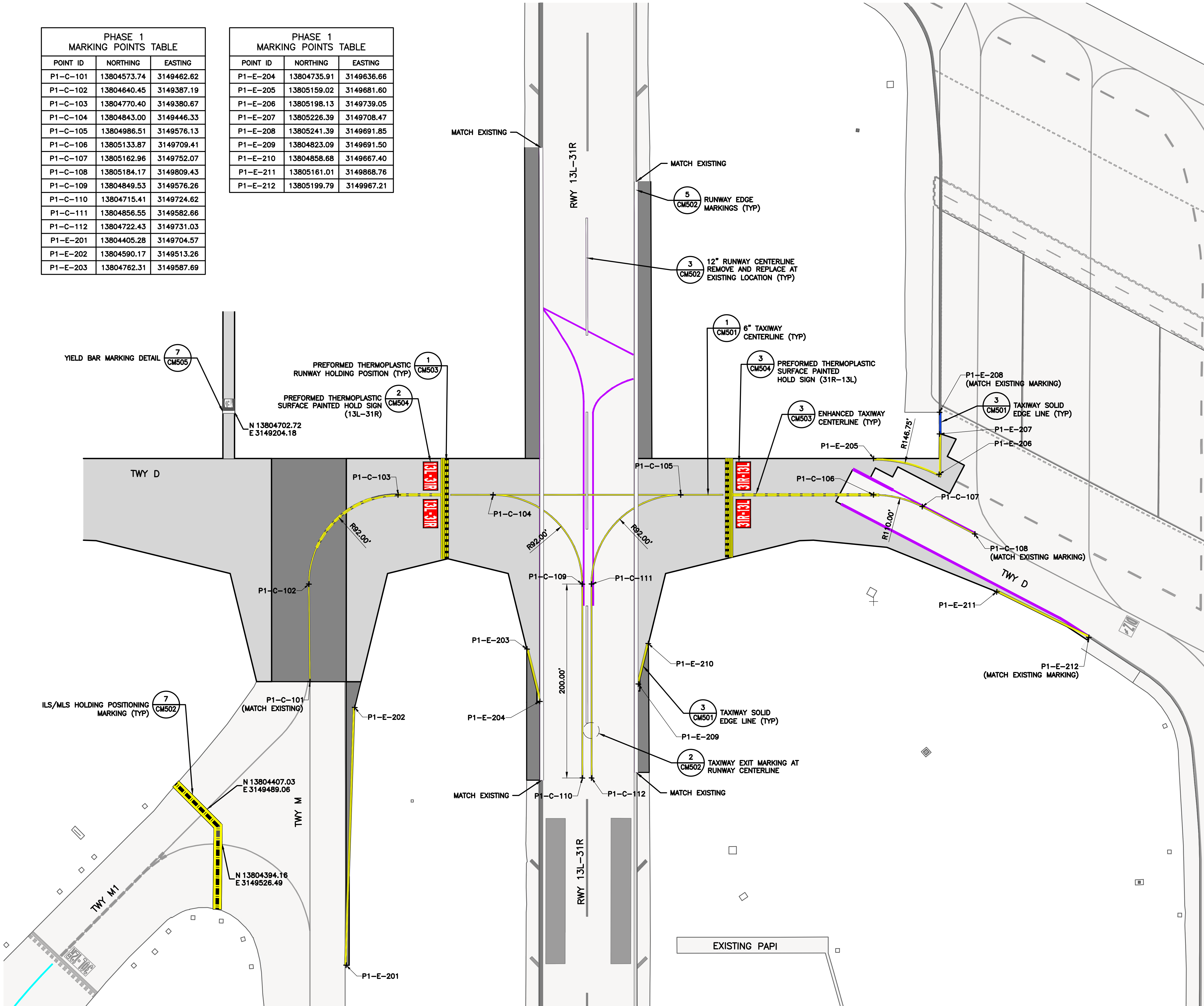
PROJECT MGR:	S. CHILDERS
DESIGNER:	D. CRAWFORD
DRAWN BY:	B. BARTLETT
CHECKED BY:	R. EHTESHAM
SCALE:	AS SHOWN
DATE:	02/24/2023



APPROVED BY: \_\_\_\_\_ DATE: \_\_\_\_\_

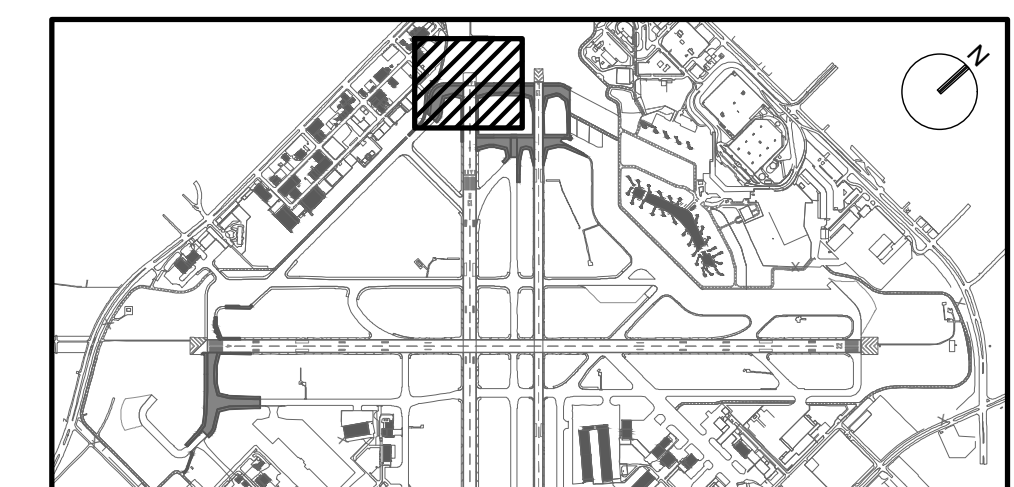
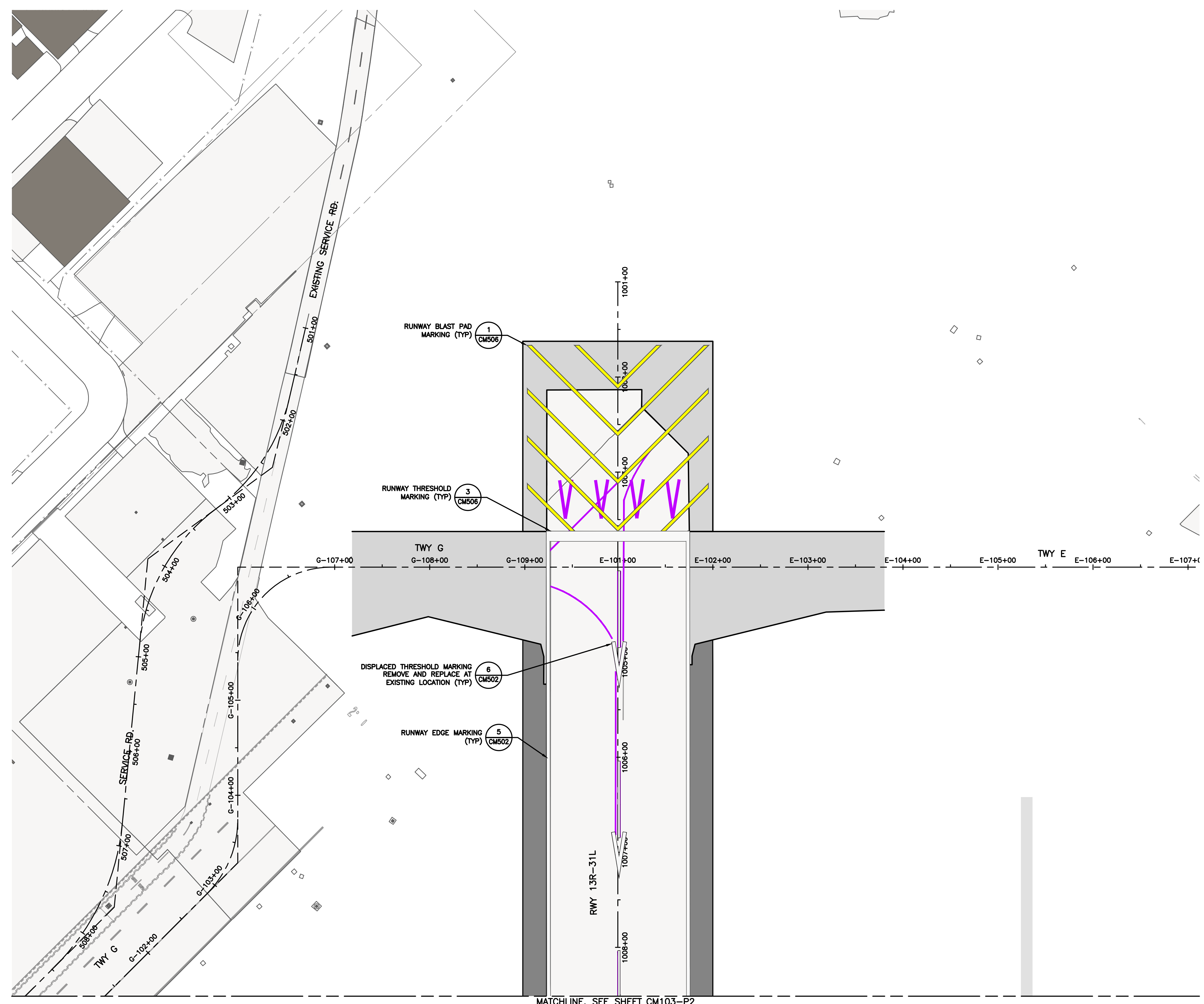
\_\_\_\_\_  
DIRECTOR  
HOUSTON AIRPORT SYSTEM

PROJECT NO:	770
C.I.P. NO:	3-48-0110-044
H.A.S. NO:	N/A
SHEET NO:	CM101-P1



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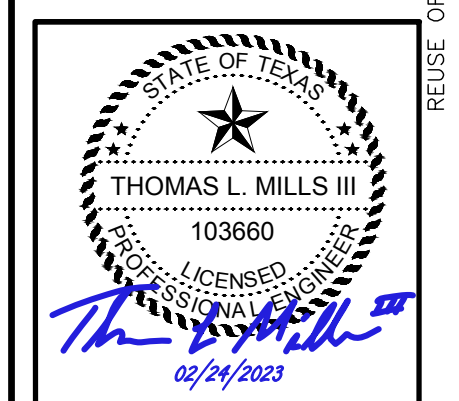
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- TEMPORARY AIRFIELD PAVEMENT MARKING
- PAVEMENT MARKING OBLITERATION
- BLACKOUT EXISTING MARKING

REVISIONS

NO.	DESCRIPTION	DATE	BY
0	ISSUED FOR BID	02/24/2023	SC

FAA NON-STANDARD TAXIWAYS PROJECT  
 PAVEMENT MARKING PLAN  
 - PHASE 2

PROJECT MGR: S. CHILDERS  
 DESIGNER: D. CRAWFORD  
 DRAWN BY: B. BARTLETT  
 CHECKED BY: R. EHTESHAM  
 SCALE: AS SHOWN  
 DATE: 02/24/2023

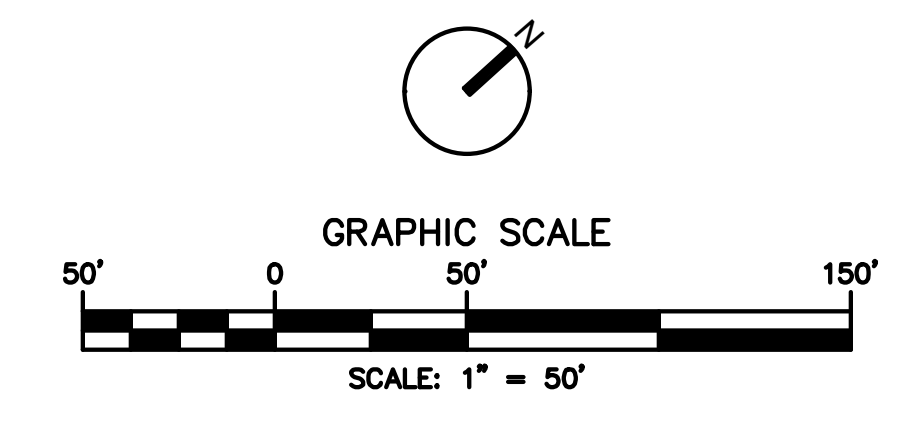


APPROVED BY: \_\_\_\_\_ DATE: \_\_\_\_\_

DIRECTOR  
HOUSTON AIRPORT SYSTEM

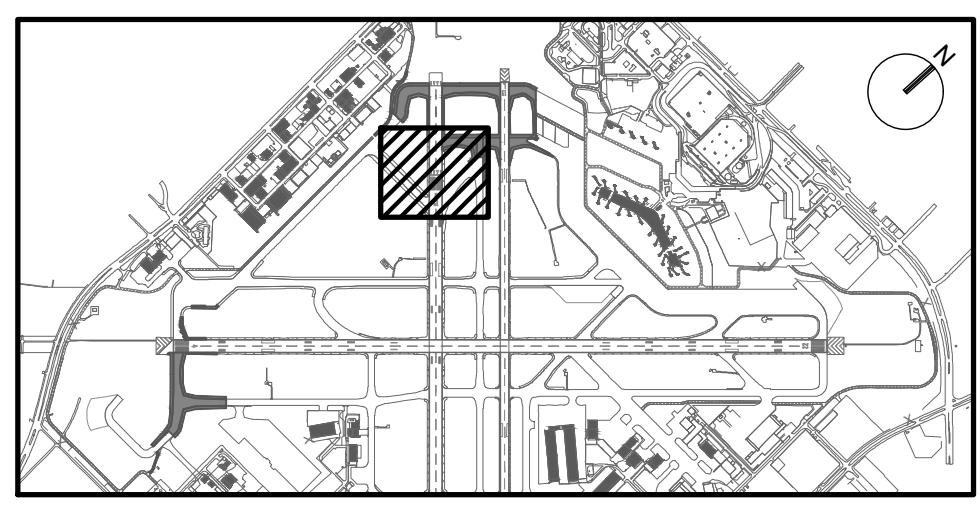
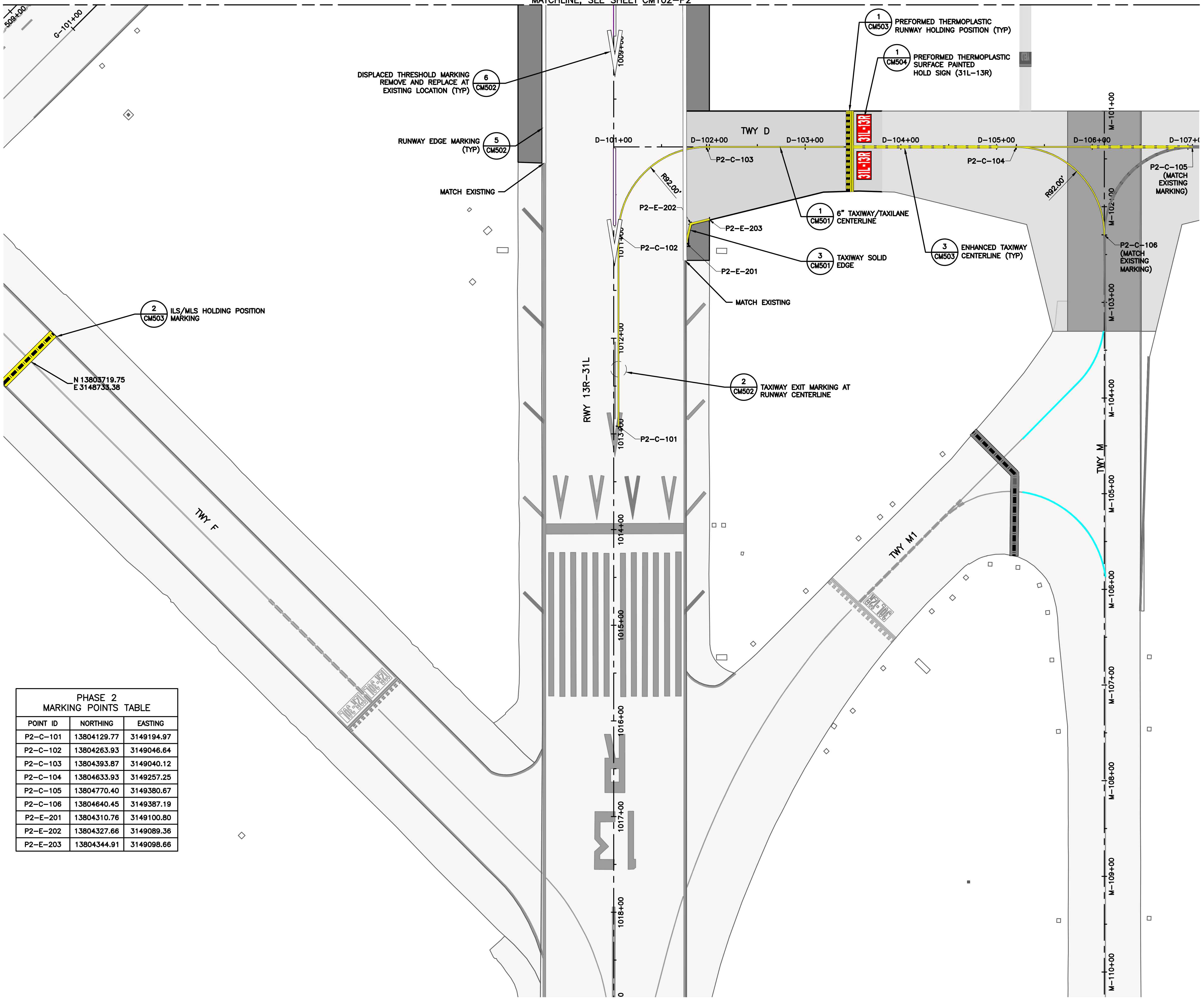
PROJECT NO: 770  
 C.I.P. NO: 3-48-0110-044  
 H.A.S. NO: N/A

SHEET NO: CM102-P2  
of





MATCHLINE, SEE SHEET CM102-P2



KEY MAP  
NTS

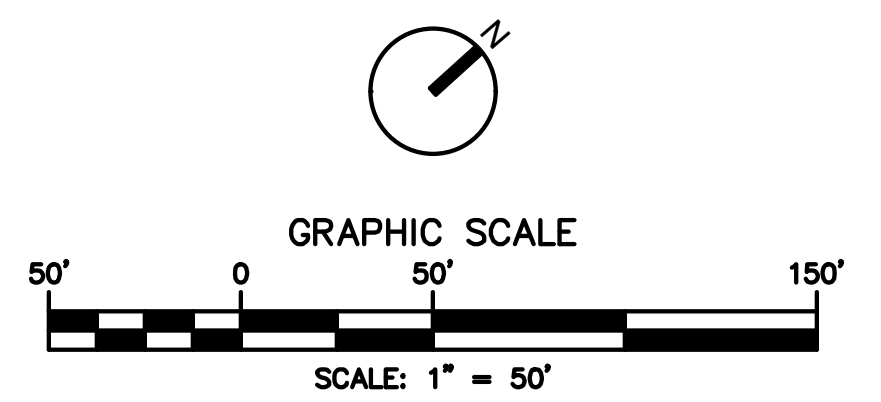
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PHASE 2 MARKING POINTS TABLE		
POINT ID	NORTHING	EASTING
P2-C-101	13804129.77	3149194.97
P2-C-102	13804263.93	3149046.64
P2-C-103	13804393.87	3149040.12
P2-C-104	13804633.93	3149257.25
P2-C-105	13804770.40	3149380.67
P2-C-106	13804640.45	3149387.19
P2-E-201	13804310.76	3149100.80
P2-E-202	13804327.66	3149089.36
P2-E-203	13804344.91	3149098.66

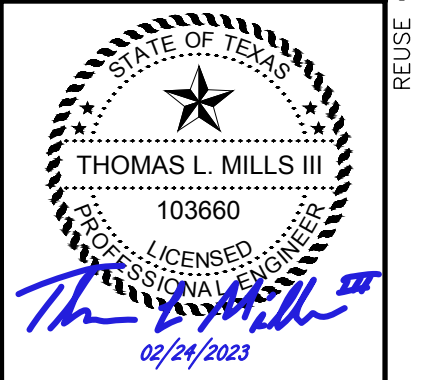


REVISIONS

NO.	DESCRIPTION	DATE	BY
0	ISSUED FOR BID	02/24/2023	SC

FAA NON-STANDARD TAXIWAYS PROJECT  
PAVEMENT MARKING PLAN  
- PHASE 2

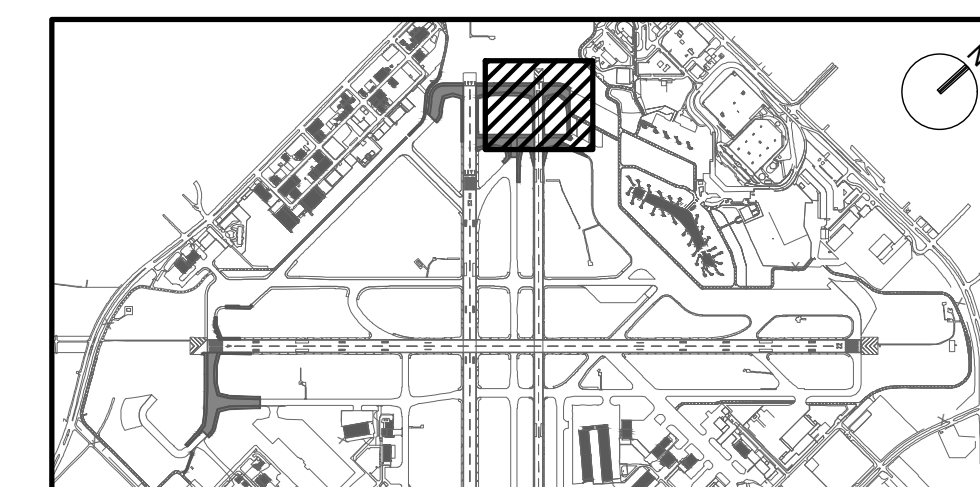
PROJECT MGR:  
DESIGNER:  
DRAWN BY:  
CHECKED BY:  
SCALE: AS SHOWN  
DATE: 02/24/2023



APPROVED BY: \_\_\_\_\_ DATE: \_\_\_\_\_  
DIRECTOR  
HOUSTON AIRPORT SYSTEM

PROJECT NO: 770  
C.I.P. NO: 3-48-0110-044  
H.A.S. NO: N/A  
SHEET NO: CM103-P2





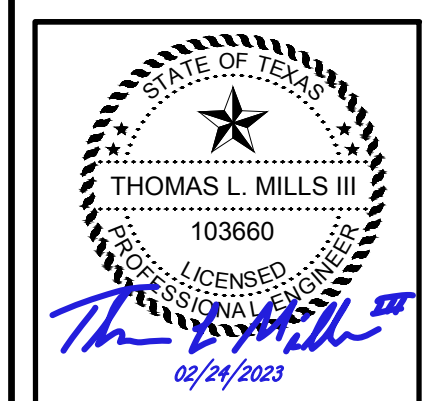
**KEY MAP**  
 NTS

REVISIONS

NO.	DESCRIPTION	DATE	BY
0	ISSUED FOR BID	02/24/2023	SC

FAA NON-STANDARD TAXIWAYS PROJECT  
 PAVEMENT MARKING PLAN -  
 PHASE 3

PROJECT MGR: S. CHILDERS  
 DESIGNER: D. CRAWFORD  
 DRAWN BY: B. BARTLETT  
 CHECKED BY: R. EHTESHAM  
 SCALE: AS SHOWN  
 DATE: 02/24/2023



APPROVED BY: \_\_\_\_\_ DATE: \_\_\_\_\_

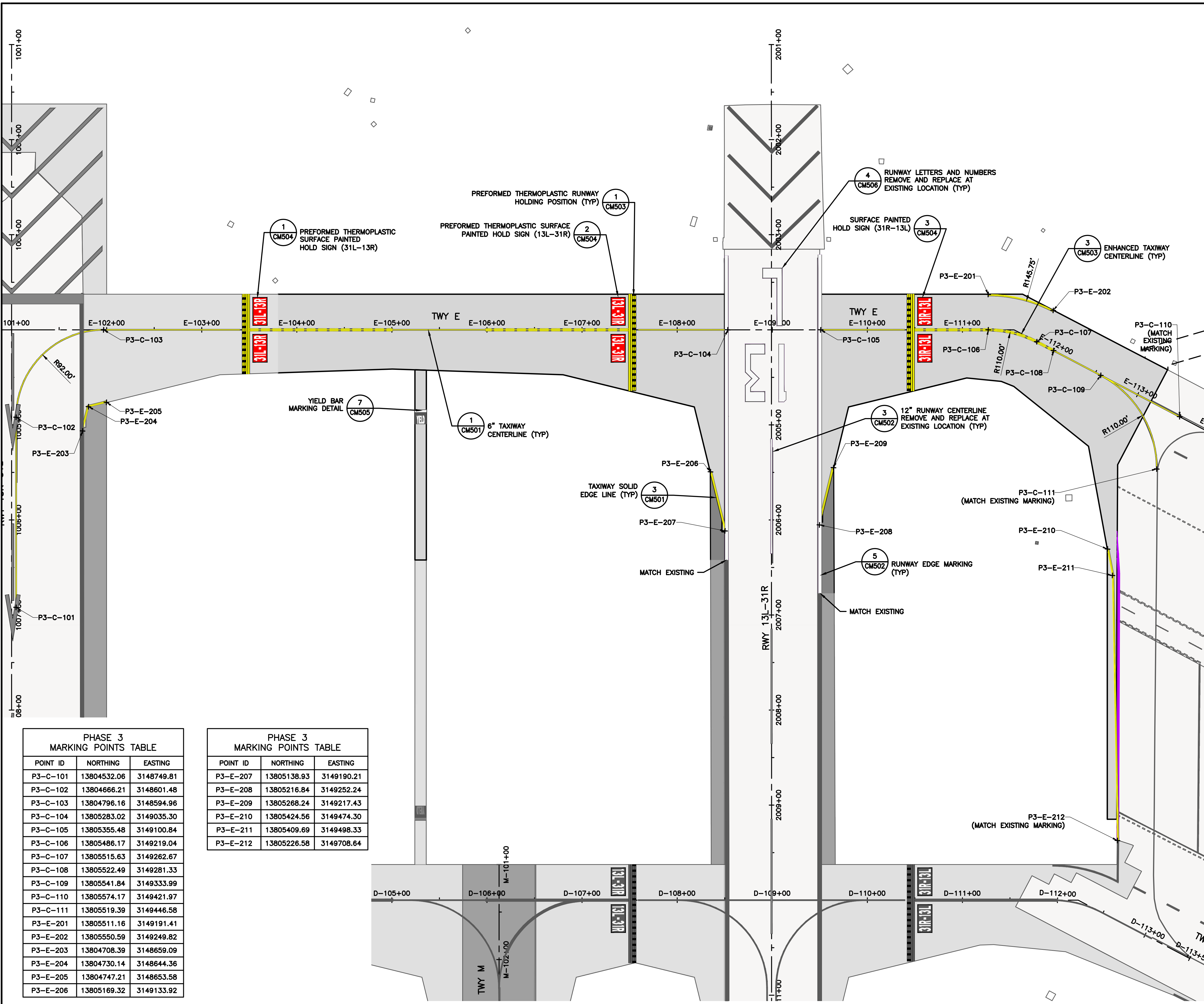
PROJECT NO: 770  
 C.I.P. NO: 3-48-0110-044  
 H.A.S. NO: N/A  
 SHEET NO: CM104-P3  
 of

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PHASE 3 MARKING POINTS TABLE

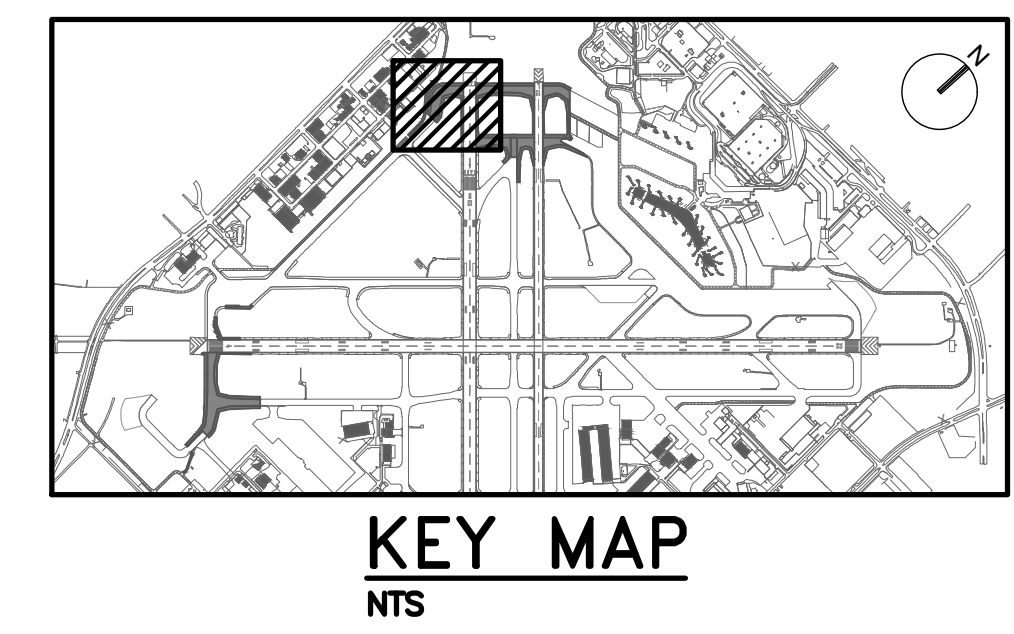
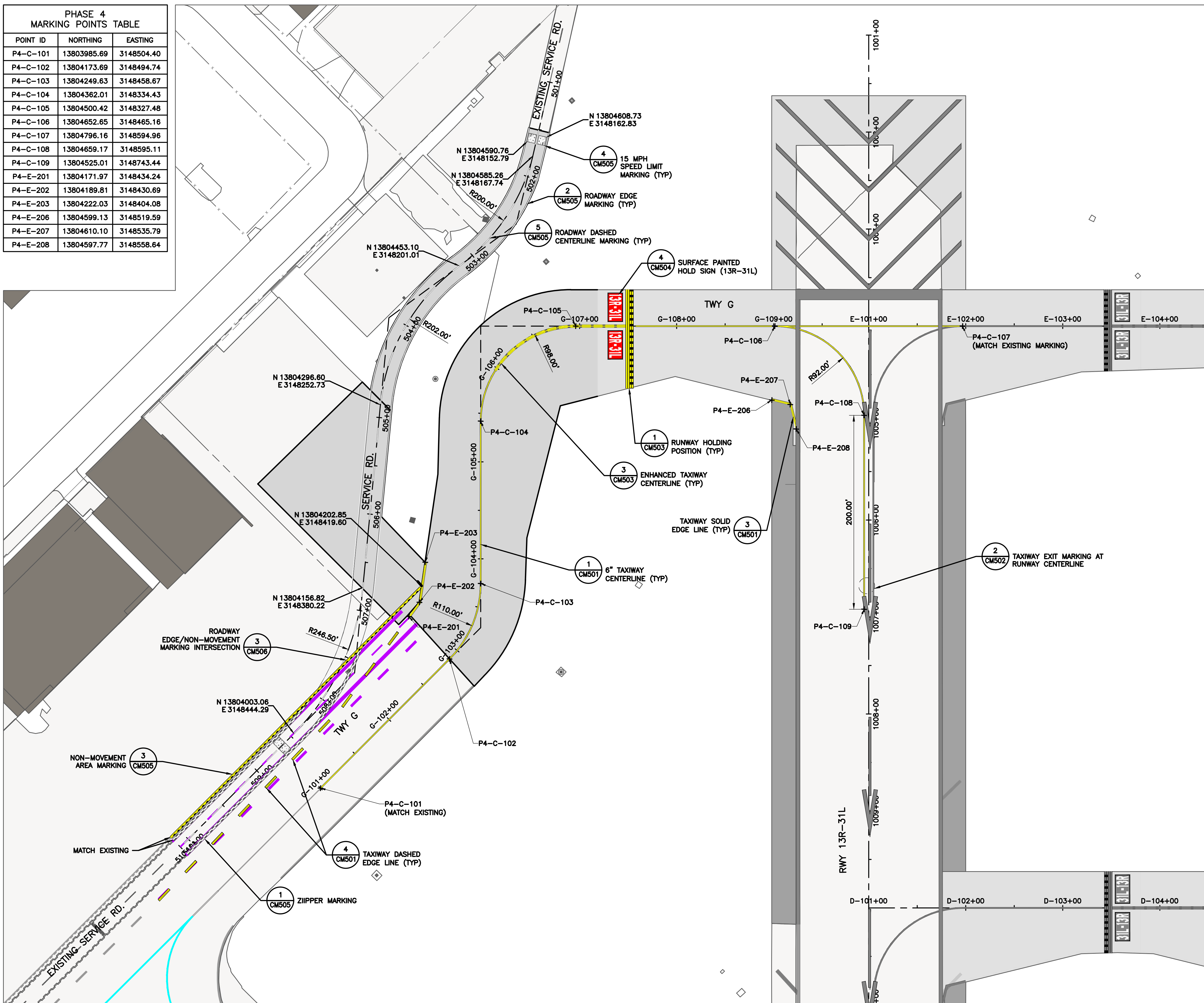
POINT ID	NORTHING	EASTING
P3-C-101	13804532.06	3148749.81
P3-C-102	13804666.21	3148601.48
P3-C-103	13804796.16	3148594.96
P3-C-104	13805283.02	3149035.30
P3-C-105	13805355.48	3149100.84
P3-C-106	13805486.17	3149219.04
P3-C-107	13805515.63	3149262.67
P3-C-108	13805522.49	3149281.33
P3-C-109	13805541.84	3149333.99
P3-C-110	13805574.17	3149421.97
P3-C-111	13805519.39	3149446.58
P3-E-201	13805511.16	3149191.41
P3-E-202	13805550.59	3149249.82
P3-E-203	13804708.39	3148659.09
P3-E-204	13804730.14	3148644.36
P3-E-205	13804747.21	3148653.58
P3-E-206	13805169.32	3149133.92

PHASE 3 MARKING POINTS TABLE

POINT ID	NORTHING	EASTING
P3-E-207	13805138.93	3149190.21
P3-E-208	13805216.84	3149252.24
P3-E-209	13805268.24	3149217.43
P3-E-210	13805424.56	3149474.30
P3-E-211	13805409.69	3149498.33
P3-E-212	13805226.58	3149708.64



PHASE 4 MARKING POINTS TABLE		
POINT ID	NORTHING	EASTING
P4-C-101	13803985.69	3148504.40
P4-C-102	13804173.69	3148494.74
P4-C-103	13804249.63	3148458.67
P4-C-104	13804362.01	3148334.43
P4-C-105	13804500.42	3148327.48
P4-C-106	13804652.65	3148465.16
P4-C-107	13804796.16	3148594.96
P4-C-108	13804659.17	3148595.11
P4-C-109	13804525.01	3148743.44
P4-E-201	13804171.97	3148434.24
P4-E-202	13804189.81	3148430.89
P4-E-203	13804222.03	3148404.08
P4-E-206	13804599.13	3148519.59
P4-E-207	13804610.10	3148535.79
P4-E-208	13804597.77	3148558.64



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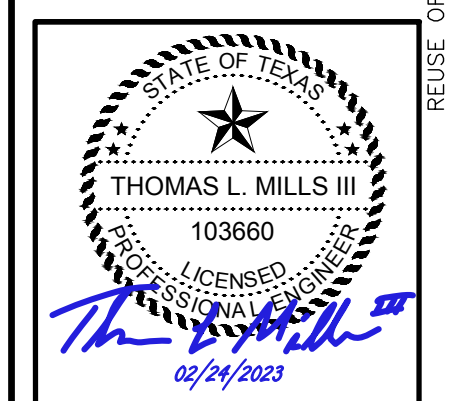
**REVISIONS**

NO.	DESCRIPTION	DATE	BY
0	ISSUED FOR BID	02/24/2023	SC

FAA NON-STANDARD TAXIWAYS PROJECT

PAVEMENT MARKING PLAN - PHASE 4

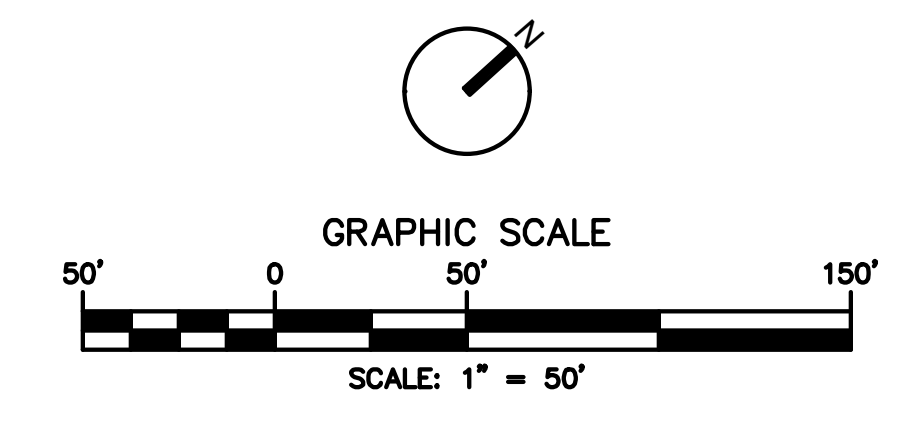
**PROJECT MGR:** S. CHILDERS  
**DESIGNER:** D. CRAWFORD  
**DRAWN BY:** B. BARTLETT  
**CHECKED BY:** R. EHTESHAM  
**SCALE:** AS SHOWN  
**DATE:** 02/24/2023



**APPROVED BY:** \_\_\_\_\_ **DATE:** \_\_\_\_\_

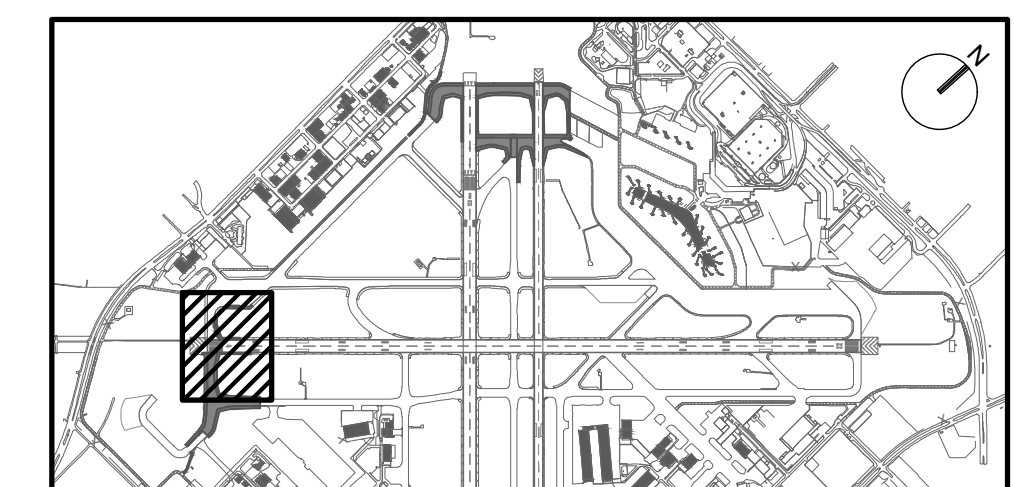
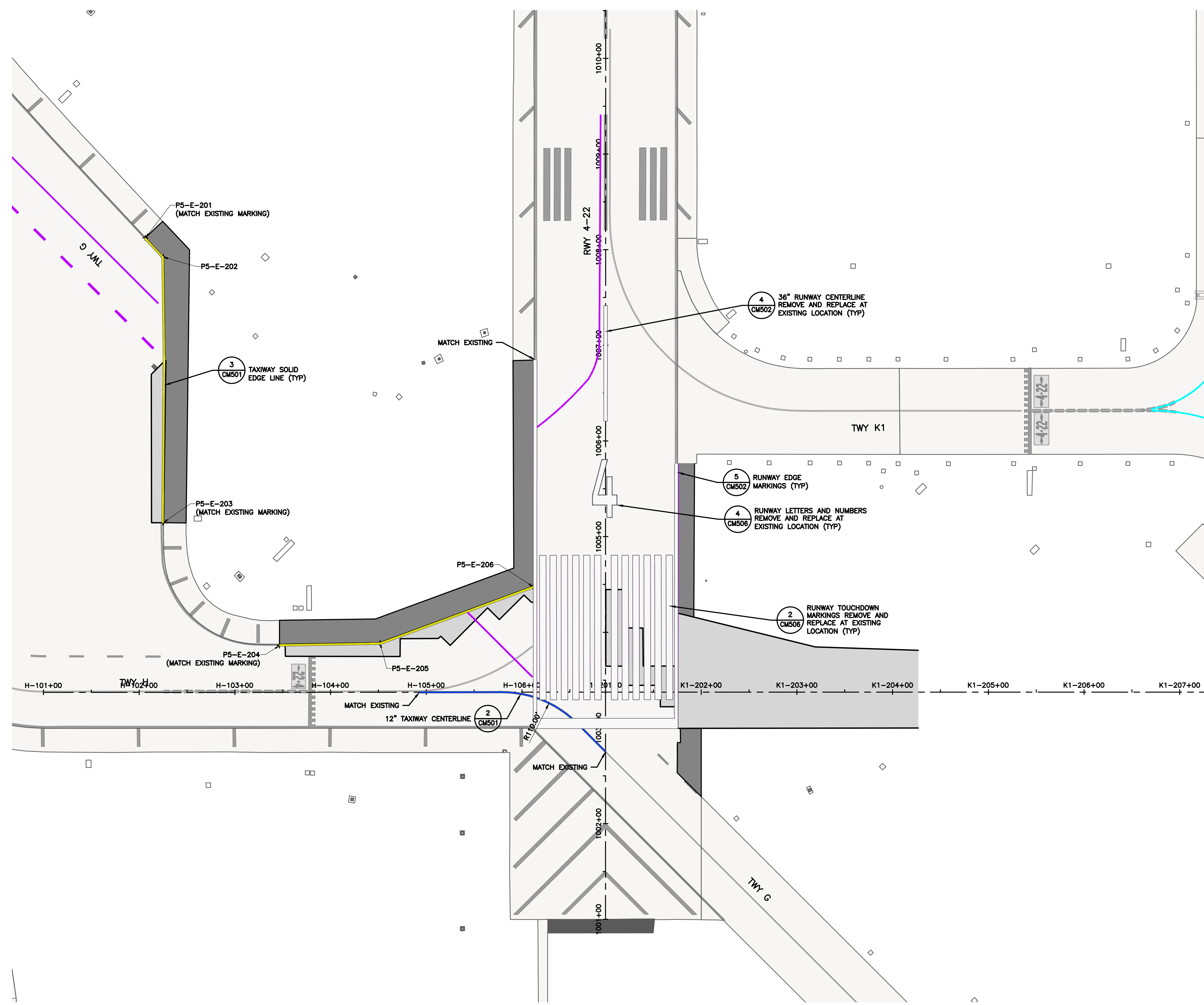
\_\_\_\_\_  
DIRECTOR  
HOUSTON AIRPORT SYSTEM

**PROJECT NO:** 770  
**C.I.P. NO:** 3-48-0110-044  
**H.A.S. NO:** N/A  
**SHEET NO:** CM105-P4  
of



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**KEY MAP**  
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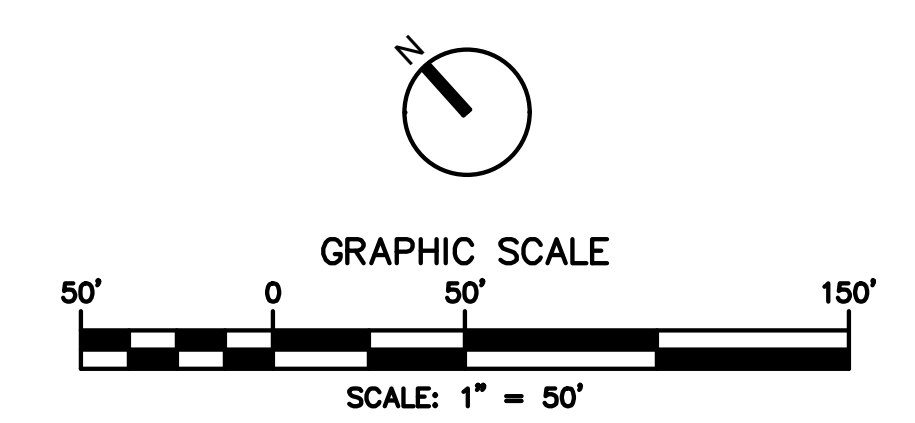
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**PHASE 5 MARKING POINTS TABLE**

POINT ID	NORTHING	EASTING
P5-E-201	13801156.84	3148685.73
P5-E-202	13801129.28	3148686.18
P5-E-203	13800923.47	3148499.85
P5-E-204	13800746.61	3148503.81
P5-E-205	13800678.12	3148583.52
P5-E-206	13800614.59	3148741.73

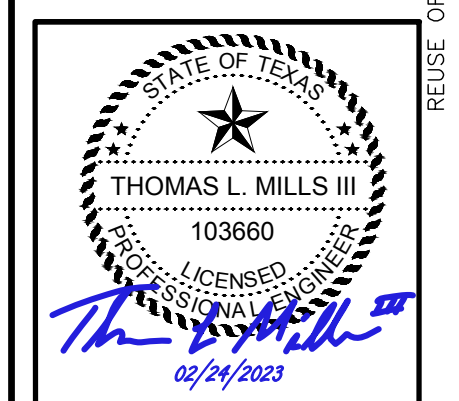


**REVISIONS**

NO.	DESCRIPTION	DATE	BY
0	ISSUED FOR BID	02/24/2023	SC

FAA NON-STANDARD TAXIWAYS PROJECT  
 PAVEMENT MARKING PLAN  
 - PHASE 5

PROJECT MGR: S. CHILDERS  
 DESIGNER: D. CRAWFORD  
 DRAWN BY: B. BARTLETT  
 CHECKED BY: R. EHTESHAM  
 SCALE: AS SHOWN  
 DATE: 02/24/2023

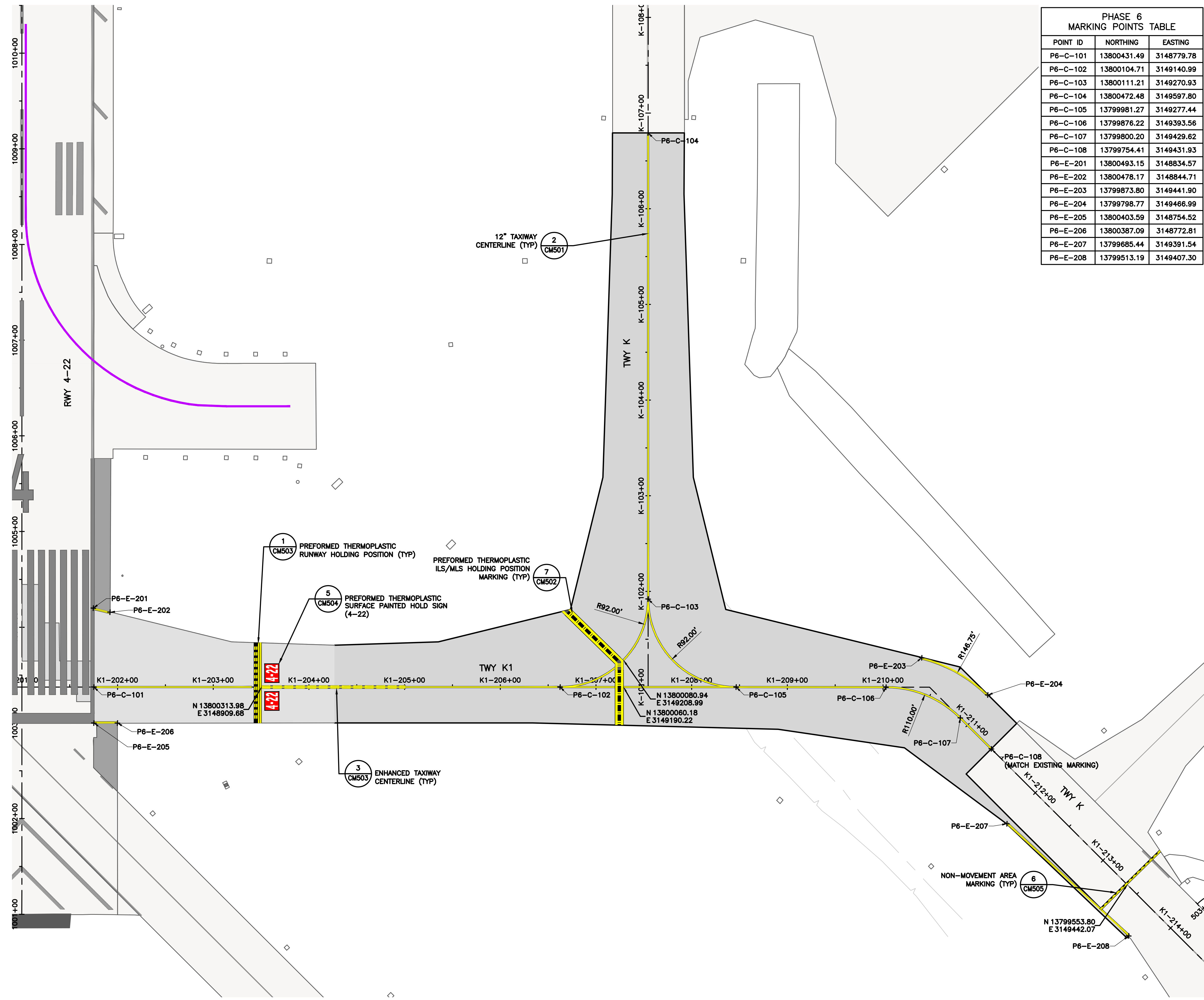


APPROVED BY: \_\_\_\_\_ DATE: \_\_\_\_\_  
 DIRECTOR  
 HOUSTON AIRPORT SYSTEM

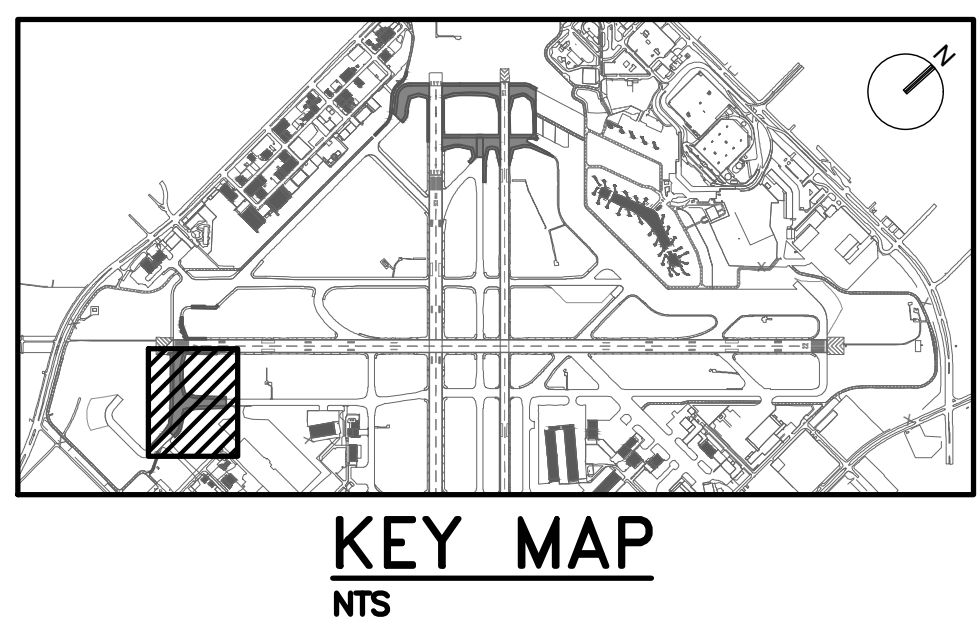
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 C.I.P. NO: 3-48-0110-044  
 H.A.S. NO: N/A

SHEET NO: CM106-P5  
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PHASE 6 MARKING POINTS TABLE		
POINT ID	NORTHING	EASTING
P6-C-101	13800431.49	3148779.78
P6-C-102	13800104.71	3149140.99
P6-C-103	13800111.21	3149270.93
P6-C-104	13800472.48	3149597.80
P6-C-105	13799981.27	3149277.44
P6-C-106	13799876.22	3149393.56
P6-C-107	13799800.20	3149429.62
P6-C-108	13799754.41	3149431.93
P6-E-201	13800493.15	3148834.57
P6-E-202	13800478.17	3148844.71
P6-E-203	13799873.80	3149441.90
P6-E-204	13799798.77	3149466.99
P6-E-205	13800403.59	3148754.52
P6-E-206	13800387.09	3148772.81
P6-E-207	13799685.44	3149391.54
P6-E-208	13799513.19	3149407.30



- NOTES:**
- THE CONTRACTOR SHALL PLACE PAVEMENT MARKINGS AS DIMENSIONED ON THE PLANS AND DETAILS.
  - OBLITERATION OF EXISTING MARKINGS SHOWN PER PHASE THROUGH THE FOLLOWING MARKING PLANS (CM100 SERIES SHEETS) MUST BE INSTALLED PRIOR TO ASSOCIATED PHASE WORK, UNLESS OTHERWISE NOTED.
  - ANY EXISTING MARKING TO REMAIN THAT IS DAMAGED BY THE CONTRACTOR MUST BE REPAINTED AT NO COST TO THE OWNER.
  - MARKINGS INADVERTENTLY AFFECTED BY MARKING OBLITERATION WORK MUST BE REPAINTED TO LOOK CONTINUOUS AT NO ADDITIONAL COST TO THE OWNER.
  - ALL MARKINGS PLACED ON PCC PAVEMENT MUST HAVE A 6-INCH BLACK BORDER, UNLESS OTHERWISE NOTED.
  - CONTRACTOR IS REQUIRED TO COORDINATE WITH THE AIRPORT FOR AVAILABLE MARKING STENCILS.
  - ALL FINAL MARKINGS MUST BE PAINTED AS SHOWN IN THE PAVEMENT MARKING DETAILS, SEE SHEETS CM501 THROUGH CM506.
  - IN PAVEMENT LIGHT FIXTURES SHALL BE COVERED SO THAT THEY ARE PROTECTED FROM PAINT. PAINT SHALL BE CLEANED FROM ALL LIGHT FIXTURES.
  - CONTRACTOR SHALL REMOVE PAVEMENT MARKINGS BY ROTARY GRINDING ON ASPHALT PAVEMENT AND BY WATER BLASTING ON CONCRETE PAVEMENT.
  - TEMPORARY MARKINGS DO NOT REQUIRE GLASS BEADS.

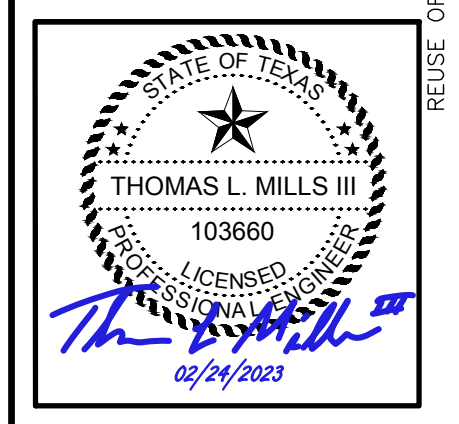
- LEGEND:**
- YELLOW AIRFIELD PAVEMENT MARKING (WITH BLACK BORDER)
  - WHITE AIRFIELD PAVEMENT MARKING (WITH BLACK BORDER)
  - RED AIRFIELD PAVEMENT MARKING (HOLDING POSITION MARKING)
  - TEMPORARY AIRFIELD PAVEMENT MARKING
  - PAVEMENT MARKING OBLITERATION
  - BLACKOUT EXISTING MARKING

**REVISIONS**

NO.	DESCRIPTION	DATE	BY
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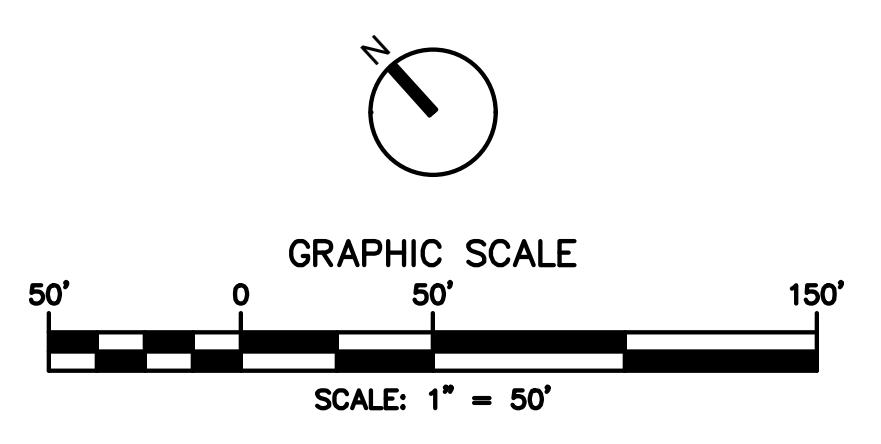
FAA NON-STANDARD TAXIWAYS PROJECT  
 PAVEMENT MARKING PLAN  
 PHASE 6A

PROJECT MGR: S. CHILDERS  
DESIGNER: D. CRAWFORD  
DRAWN BY: B. BARTLETT  
CHECKED BY: R. EHTESHAM  
SCALE: AS SHOWN  
DATE: 02/24/2023

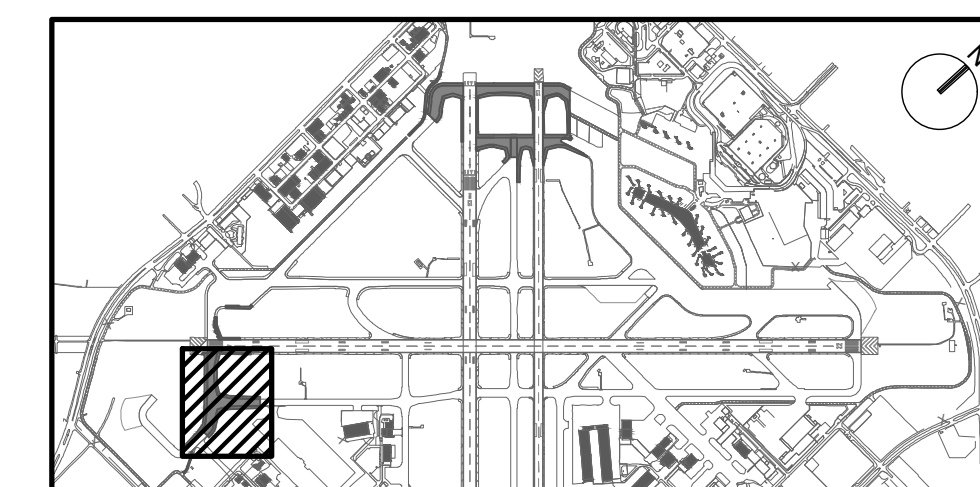
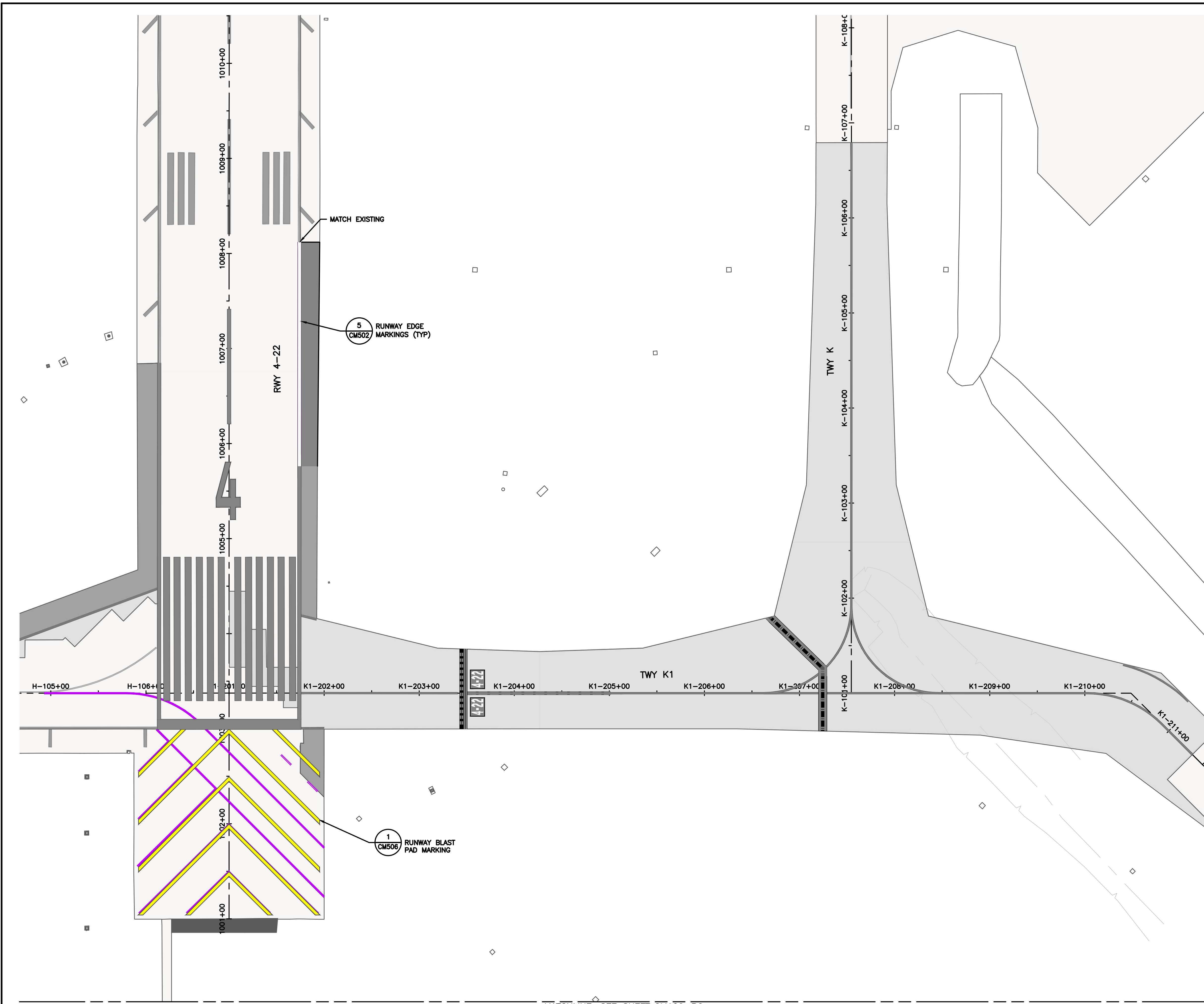


APPROVED BY: \_\_\_\_\_ DATE: \_\_\_\_\_  
DIRECTOR  
HOUSTON AIRPORT SYSTEM

PROJECT NO: 770  
C.I.P. NO: 3-48-0110-044  
H.A.S. NO: N/A  
SHEET NO: CM107-P6  
of







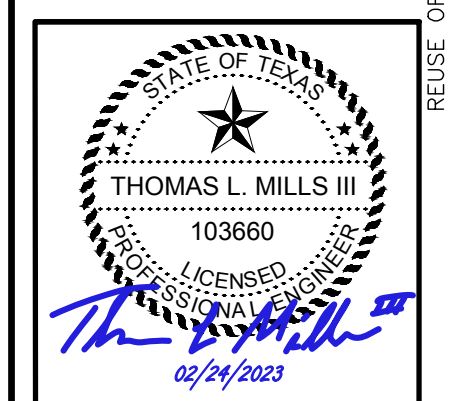
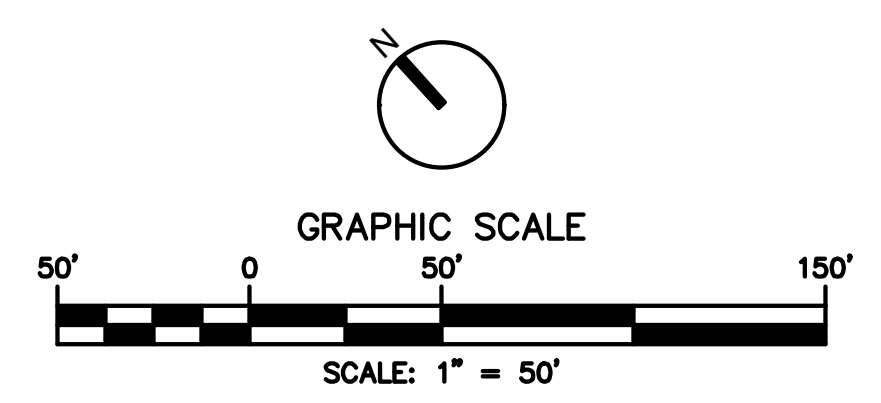
**KEY MAP**  
NTS

VERIFY SCALE  
BAR IS ONE INCH ON  
ORIGINAL DRAWING.  
0 1"

NO.	DESCRIPTION	DATE	BY
0	ISSUED FOR BID	02/24/2023	SC

FAA NON-STANDARD TAXIWAYS PROJECT  
**PAVEMENT MARKING PLAN**  
— PHASE 6B

PROJECT MGR:	S. CHILDERS
DESIGNER:	D. CRAWFORD
DRAWN BY:	B. BARTLETT
CHECKED BY:	R. EHTESHAM
SCALE:	AS SHOWN
DATE:	02/24/2023



APPROVED BY: \_\_\_\_\_ DATE: \_\_\_\_\_  
DIRECTOR  
HOUSTON AIRPORT SYSTEM

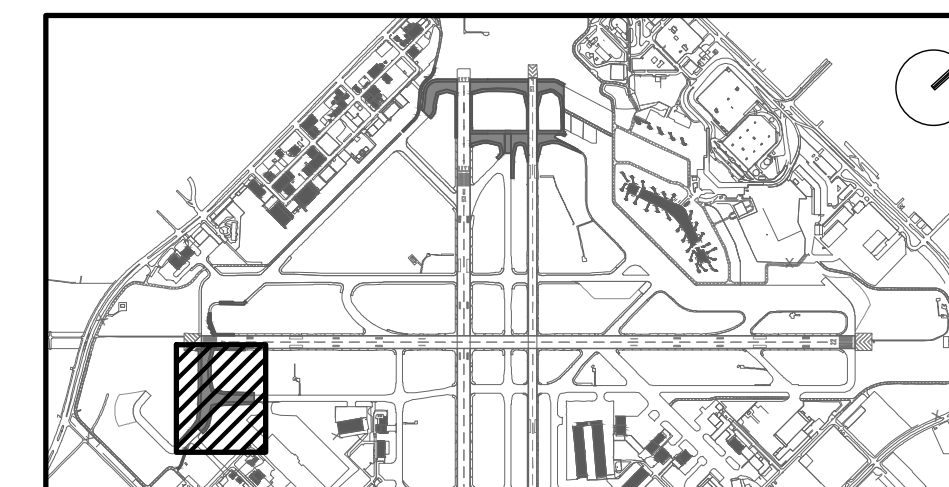
PROJECT NO: 770  
C.I.P. NO: 3-48-0110-044  
H.A.S. NO: N/A  
SHEET NO:

**CM108-P6**  
of

MATCHLINE, SEE SHEET CM109-P6

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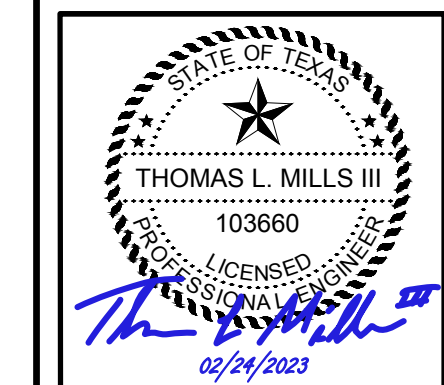
**KEY MAP**  
NTS

REVISIONS

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FAA NON-STANDARD TAXIWAYS PROJECT  
PAVEMENT MARKING PLAN  
— PHASE 6B

PROJECT MGR:	S. CHILDERS
DESIGNER:	D. CRAWFORD
DRAWN BY:	B. BARTLETT
CHECKED BY:	R. EHTESHAM
SCALE:	AS SHOWN
DATE:	02/24/2023

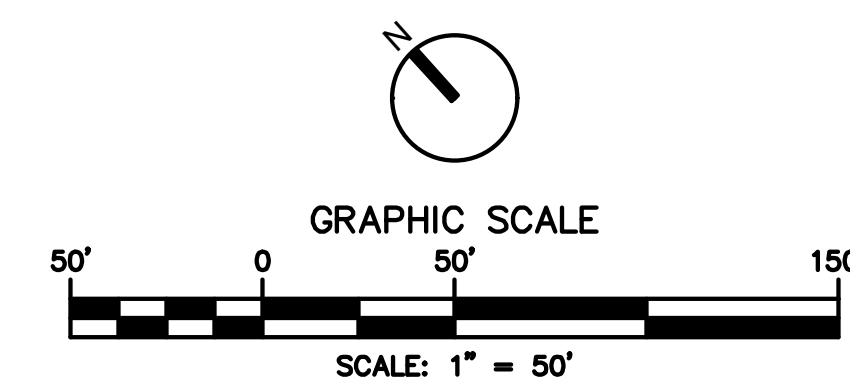
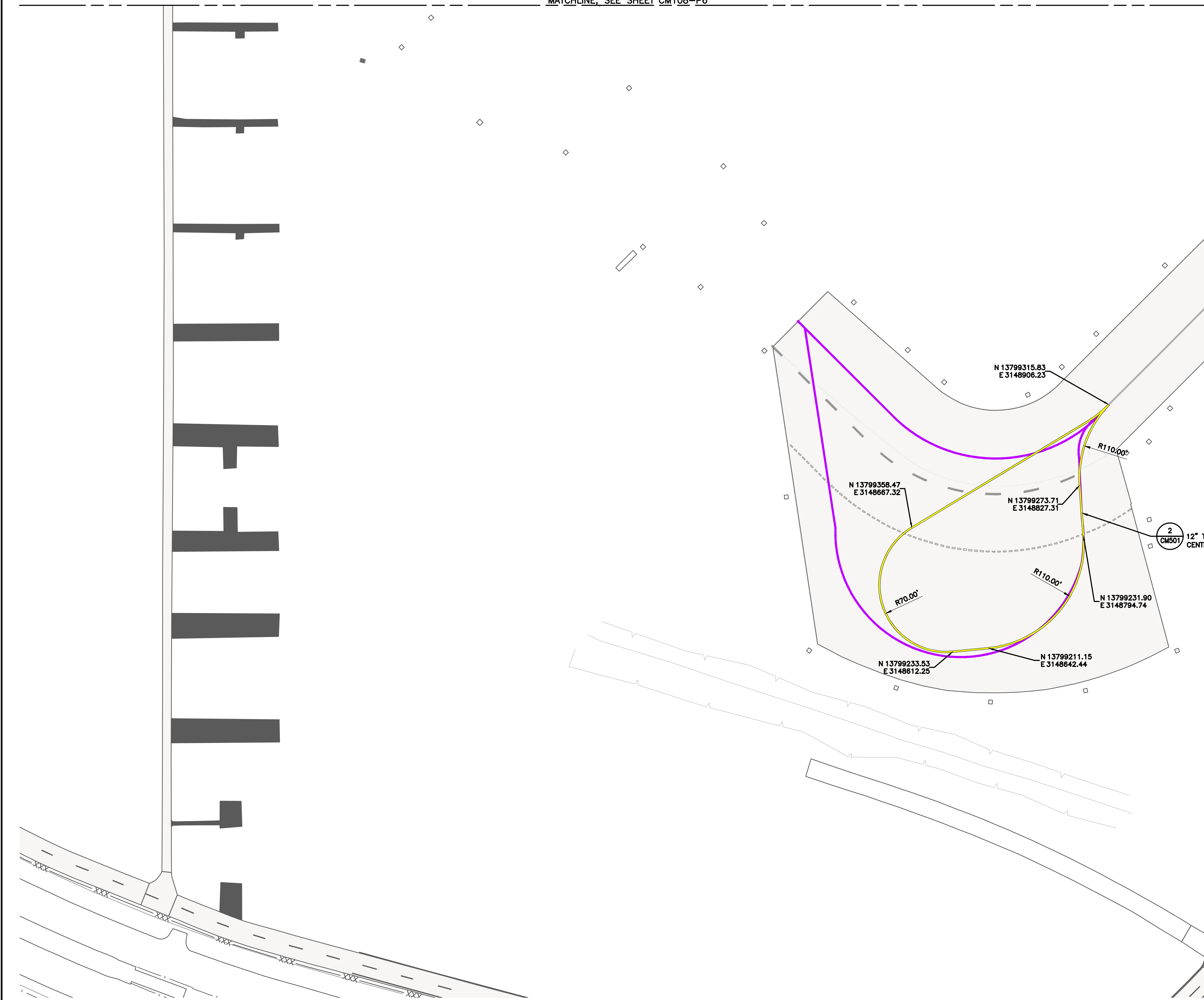


APPROVED BY: \_\_\_\_\_ DATE: \_\_\_\_\_

DIRECTOR  
HOUSTON AIRPORT SYSTEM

PROJECT NO: 770  
C.I.P. NO: 3-48-0110-044  
H.A.S. NO: N/A

SHEET NO: CM109-P6  
of





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FAA NON-STANDARD TAXIWAYS PROJECT  
 PAVEMENT MARKING DETAILS

PROJECT MGR:	S. CHILDERS
DESIGNER:	D. CRAWFORD
DRAWN BY:	B. BARTLETT
CHECKED BY:	R. EHTESHAM
SCALE:	AS SHOWN
DATE:	02/24/2023

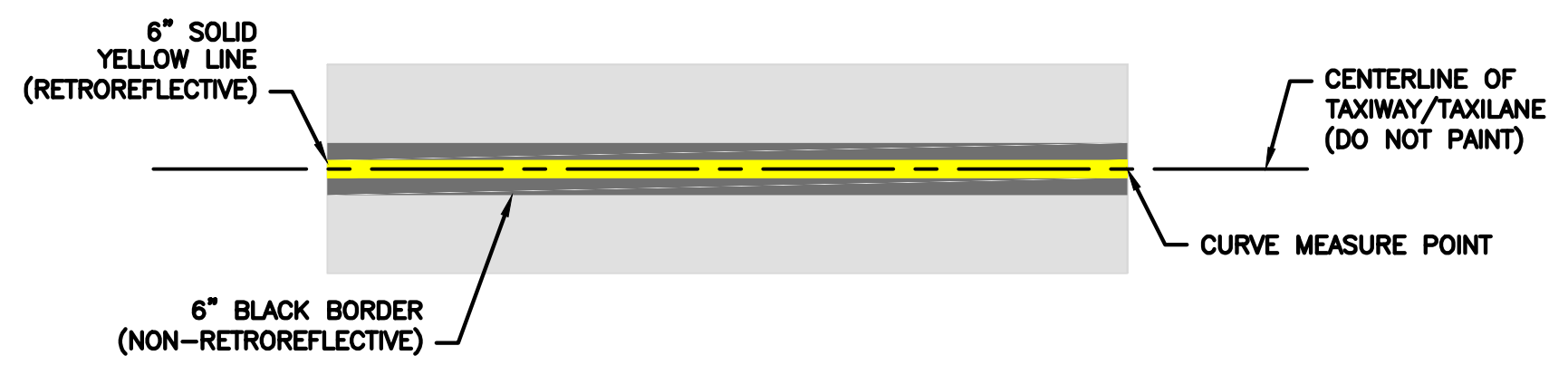


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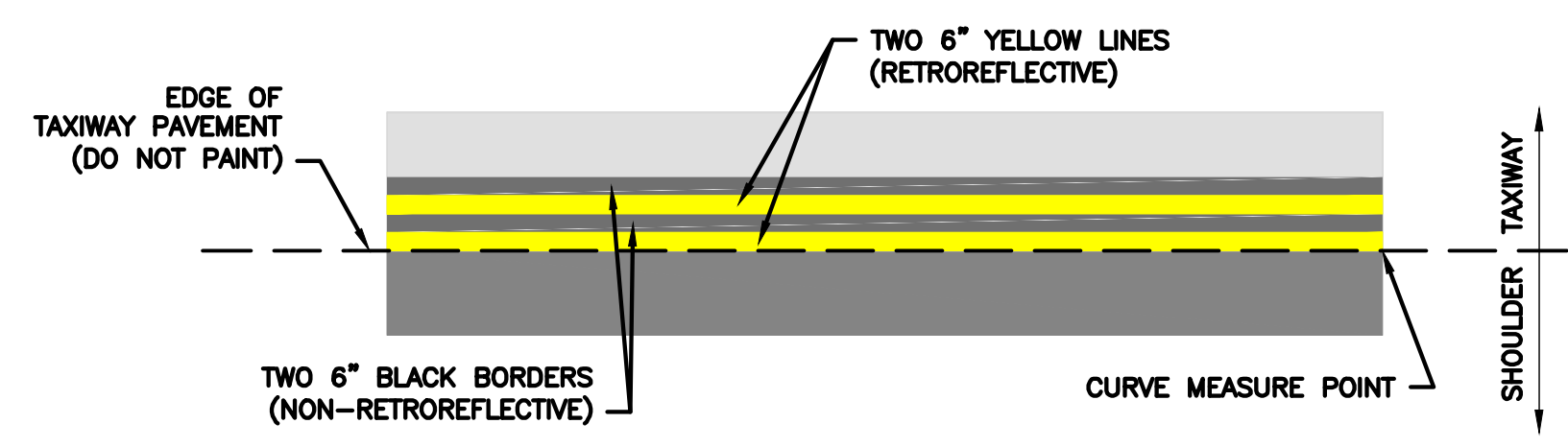
DIRECTOR  
HOUSTON AIRPORT SYSTEM

PROJECT NO:	770
C.I.P. NO:	3-48-0110-044
H.A.S. NO:	N/A
SHEET NO:	CM501

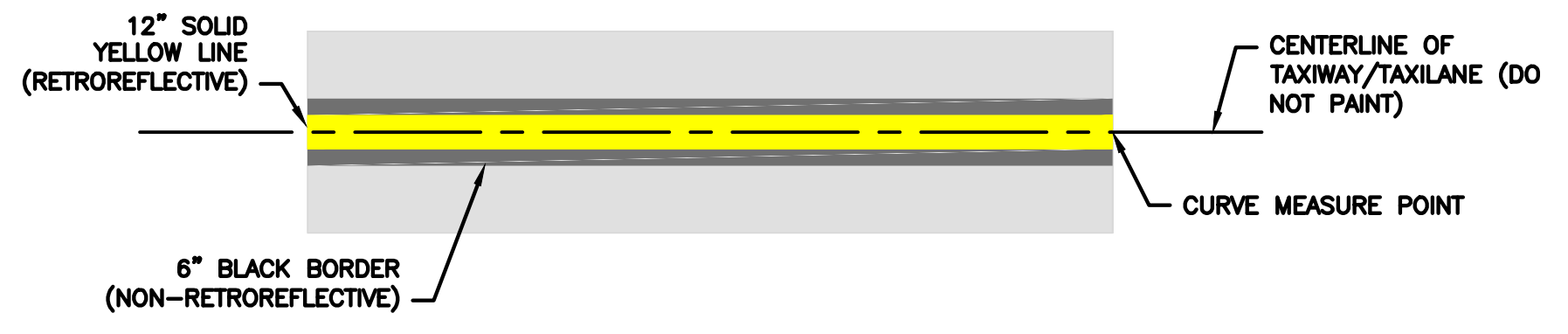
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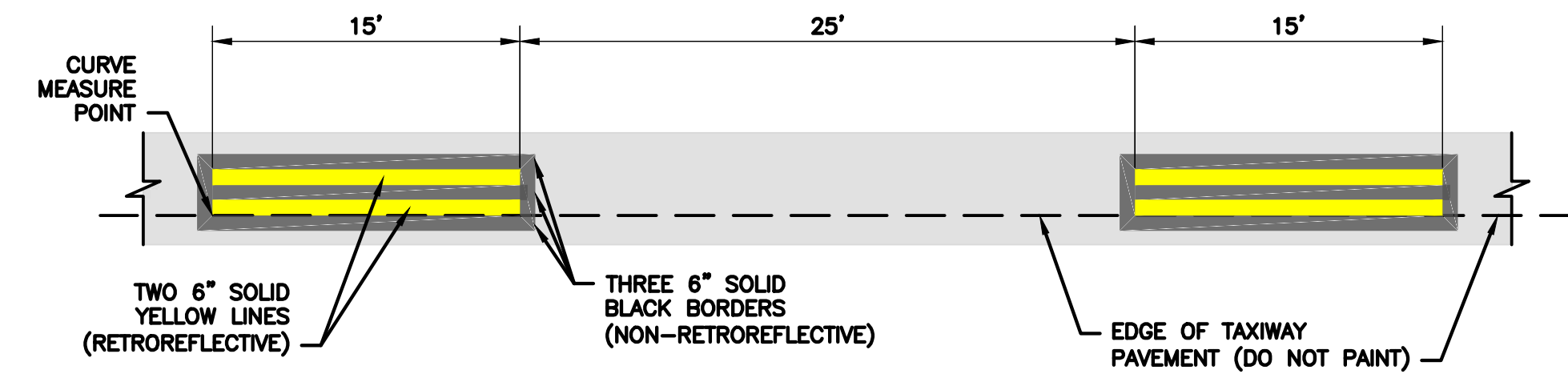
1 6" TAXIWAY/TAXILANE CENTERLINE  
 1" = 5'-0"



3 TAXIWAY SOLID EDGE  
 1" = 5'-0"



2 12" TAXIWAY/TAXILANE CENTERLINE  
 1" = 5'-0"



4 TAXIWAY EDGE DASHED MARKING  
 1" = 5'-0"

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FAA NON-STANDARD TAXIWAYS PROJECT  
 PAVEMENT MARKING DETAILS

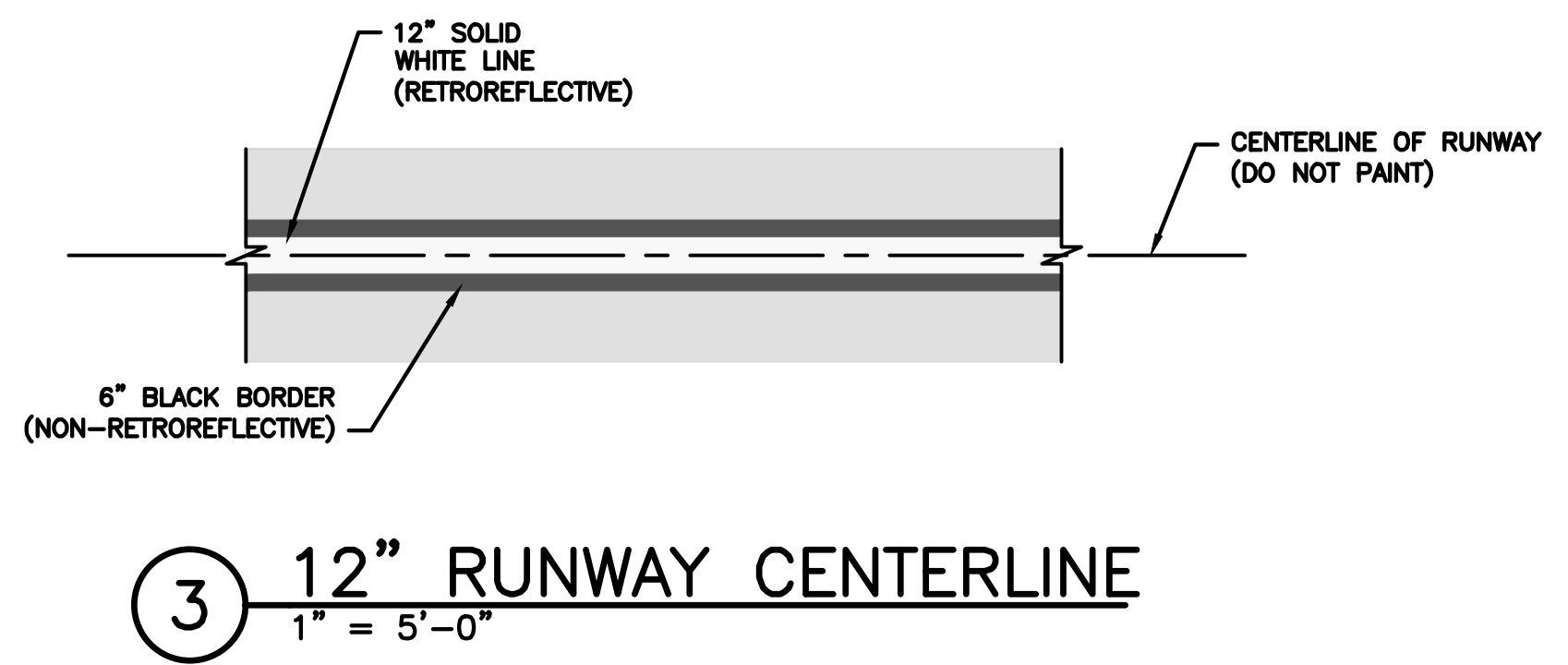
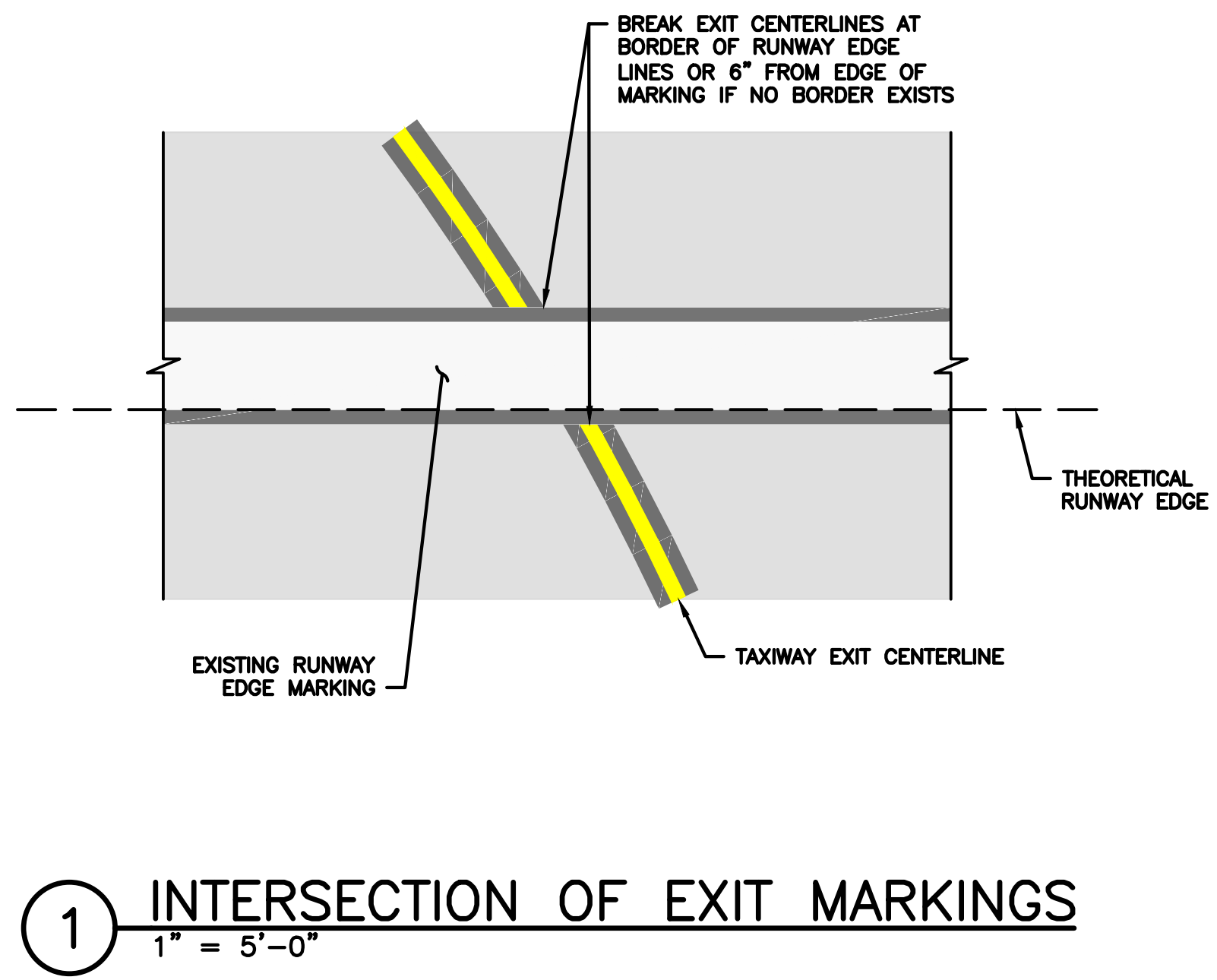
PROJECT MGR:	S. CHILDERS
DESIGNER:	D. CRAWFORD
DRAWN BY:	B. BARTLETT
CHECKED BY:	R. EHTESHAM
SCALE:	AS SHOWN
DATE:	02/24/2023



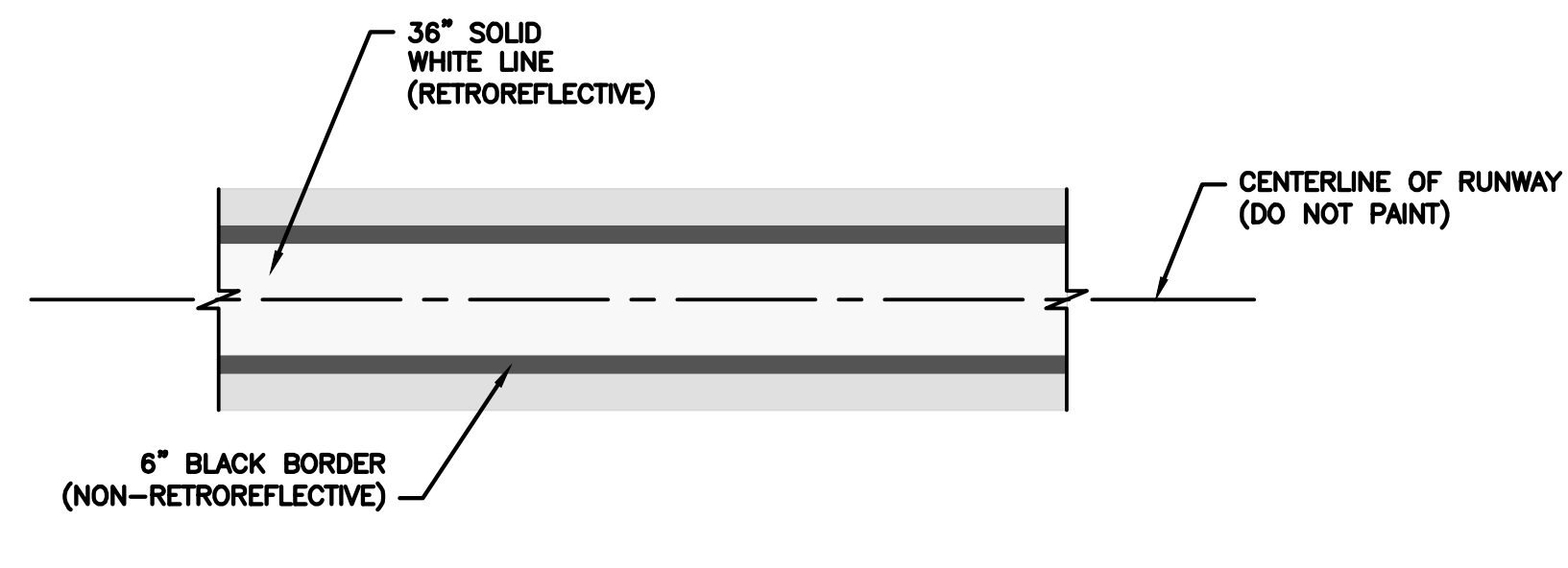
APPROVED BY: \_\_\_\_\_ DATE: \_\_\_\_\_

DIRECTOR  
HOUSTON AIRPORT SYSTEM

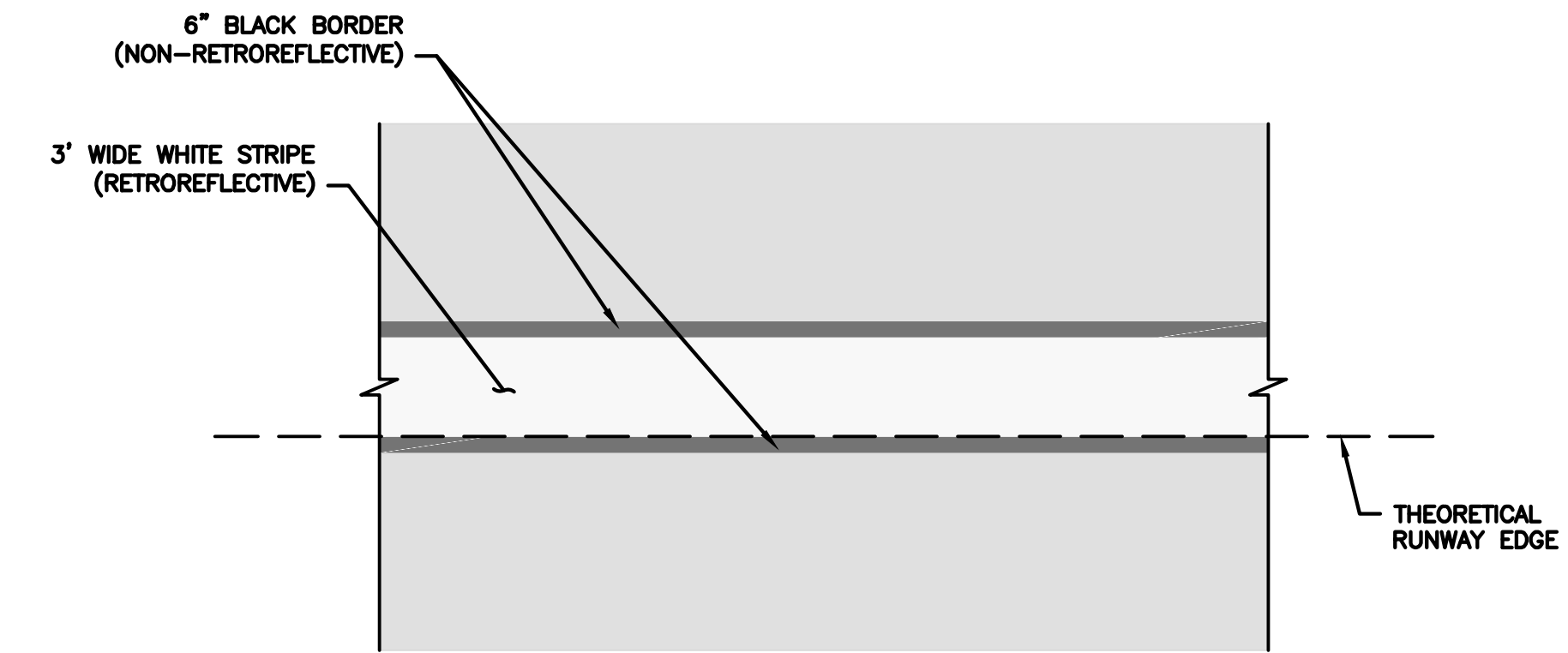
PROJECT NO:	770
C.I.P. NO:	3-48-0110-044
H.A.S. NO:	N/A
SHEET NO:	CM502



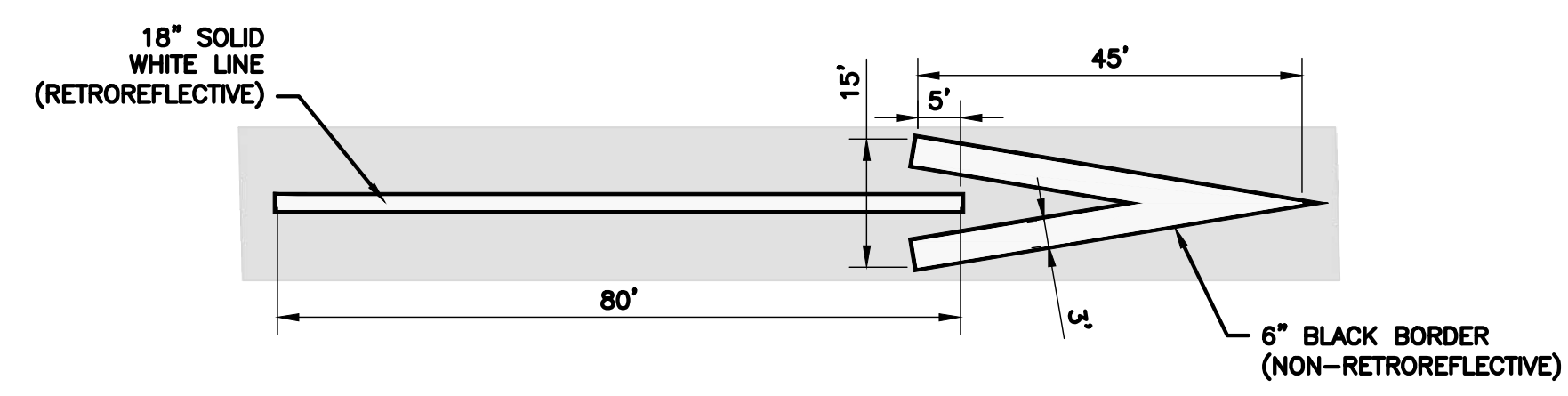
3 12" RUNWAY CENTERLINE  
 1" = 5'-0"



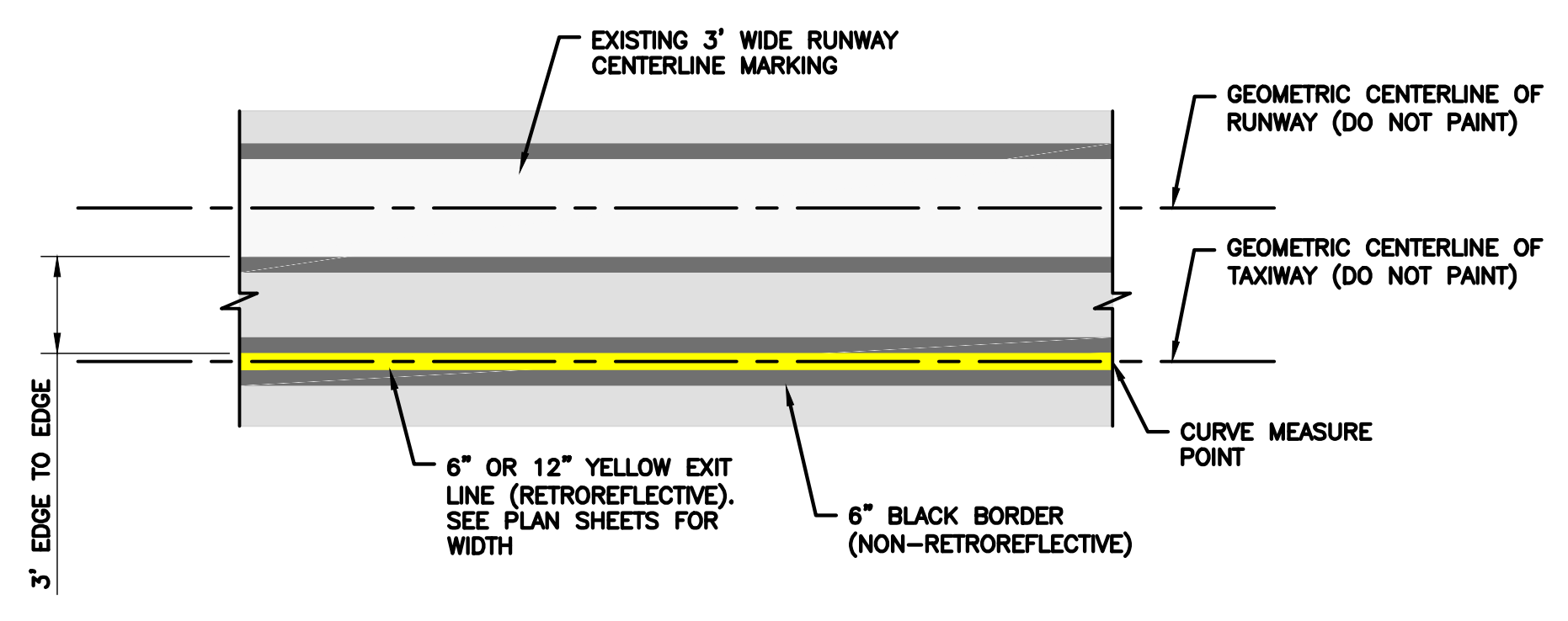
4 36" RUNWAY CENTERLINE  
 1" = 5'-0"



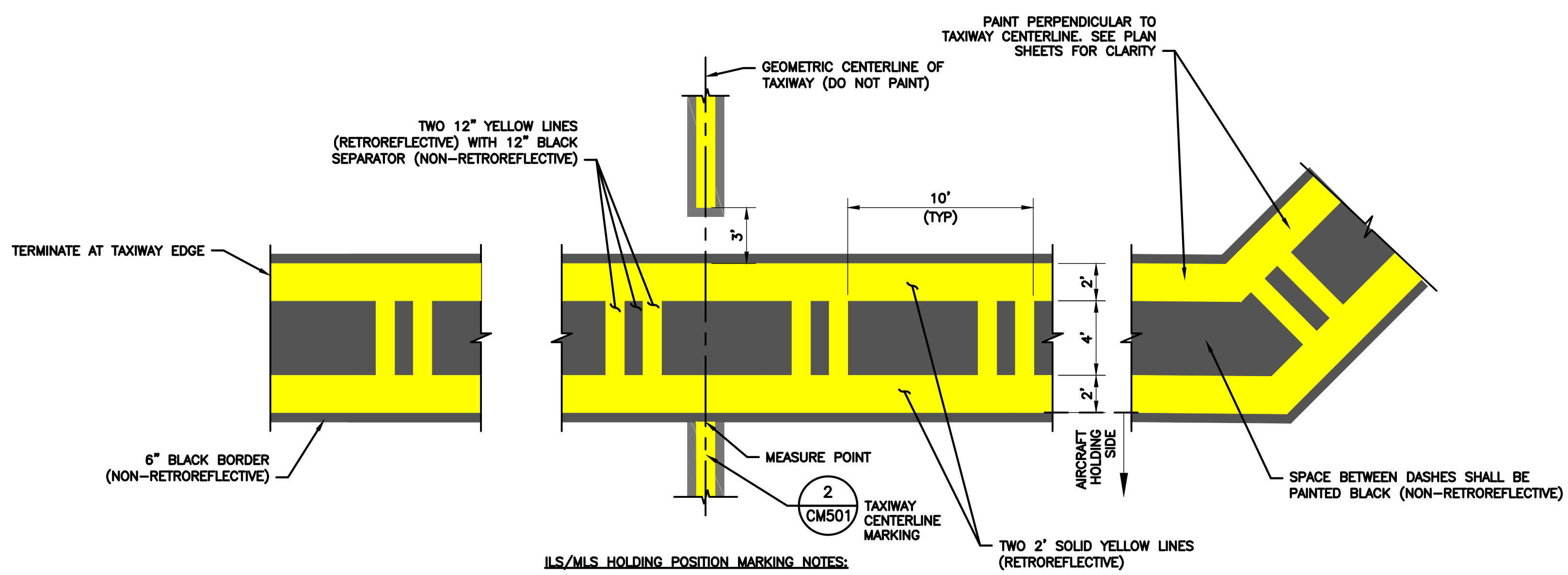
5 RUNWAY EDGE MARKINGS  
 1" = 5'-0"



6 DISPLACED THRESHOLD MARKING  
 1" = 20'-0"



2 TAXIWAY EXIT AT RUNWAY CENTERLINE  
 1" = 5'-0"



7 TAXIWAY K AND M1 ILS HOLD POSITION MARKING  
 1" = 5'-0"

- ILS/MLS HOLDING POSITION MARKING NOTES:
1. THE TAXIWAY K AND M1 ILS HOLDING POSITION MARKINGS SHALL BE PAINTED USING WATERBORNE PAINT. SEE P-620 FOR CLARIFICATION.
  2. TAXIWAY CENTERLINE MARKING



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0	ISSUED FOR BID	02/24/2023	SC

FAA NON-STANDARD TAXIWAYS PROJECT  
 PAVEMENT MARKING DETAILS

PROJECT MGR:	S. CHILDERS
DESIGNER:	D. CRAWFORD
DRAWN BY:	B. BARTLETT
CHECKED BY:	R. EHTESHAM
SCALE:	AS SHOWN
DATE:	02/24/2023

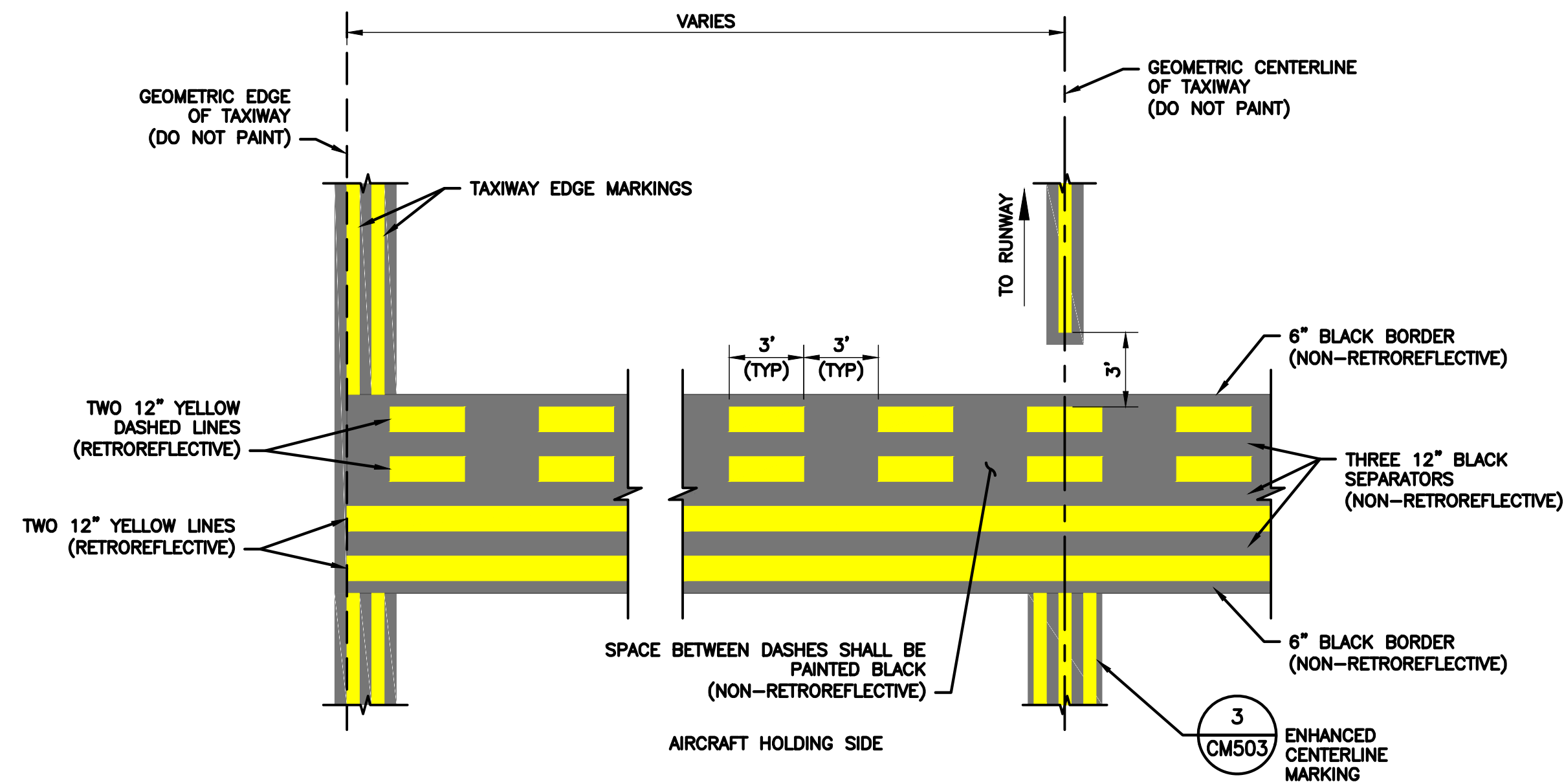


APPROVED BY: \_\_\_\_\_ DATE: \_\_\_\_\_  
 DIRECTOR  
 HOUSTON AIRPORT SYSTEM

PROJECT NO:	770
C.I.P. NO:	3-48-0110-044
H.A.S. NO:	N/A
SHEET NO:	CM503

**RUNWAY HOLDING POSITION MARKING NOTES:**

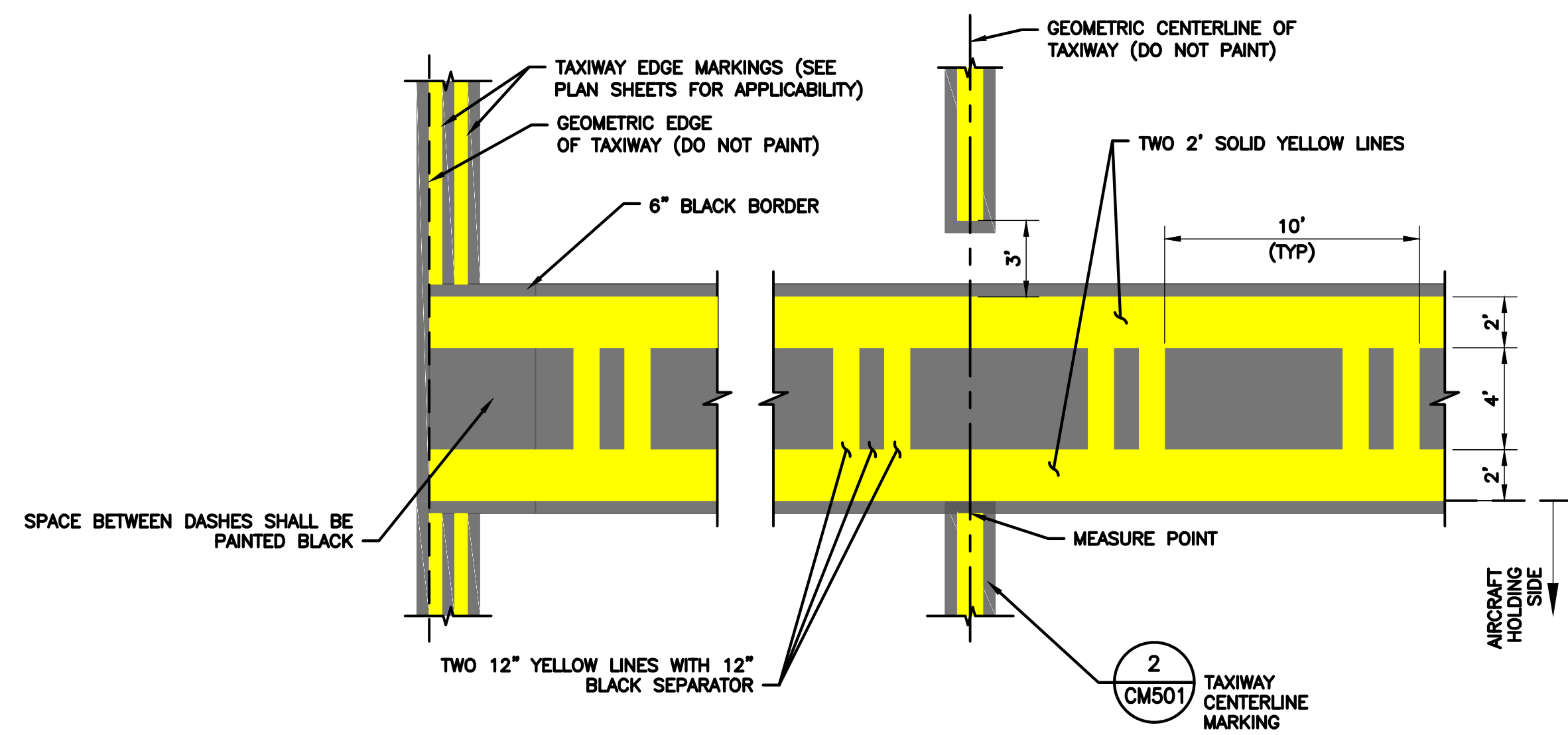
1. RUNWAY HOLDING POSITION MARKINGS SHALL BE PAINTED USING PREFORMED THERMOPLASTIC, UNLESS OTHERWISE NOTED. SEE SPECIFICATION P-620 FOR CLARIFICATION



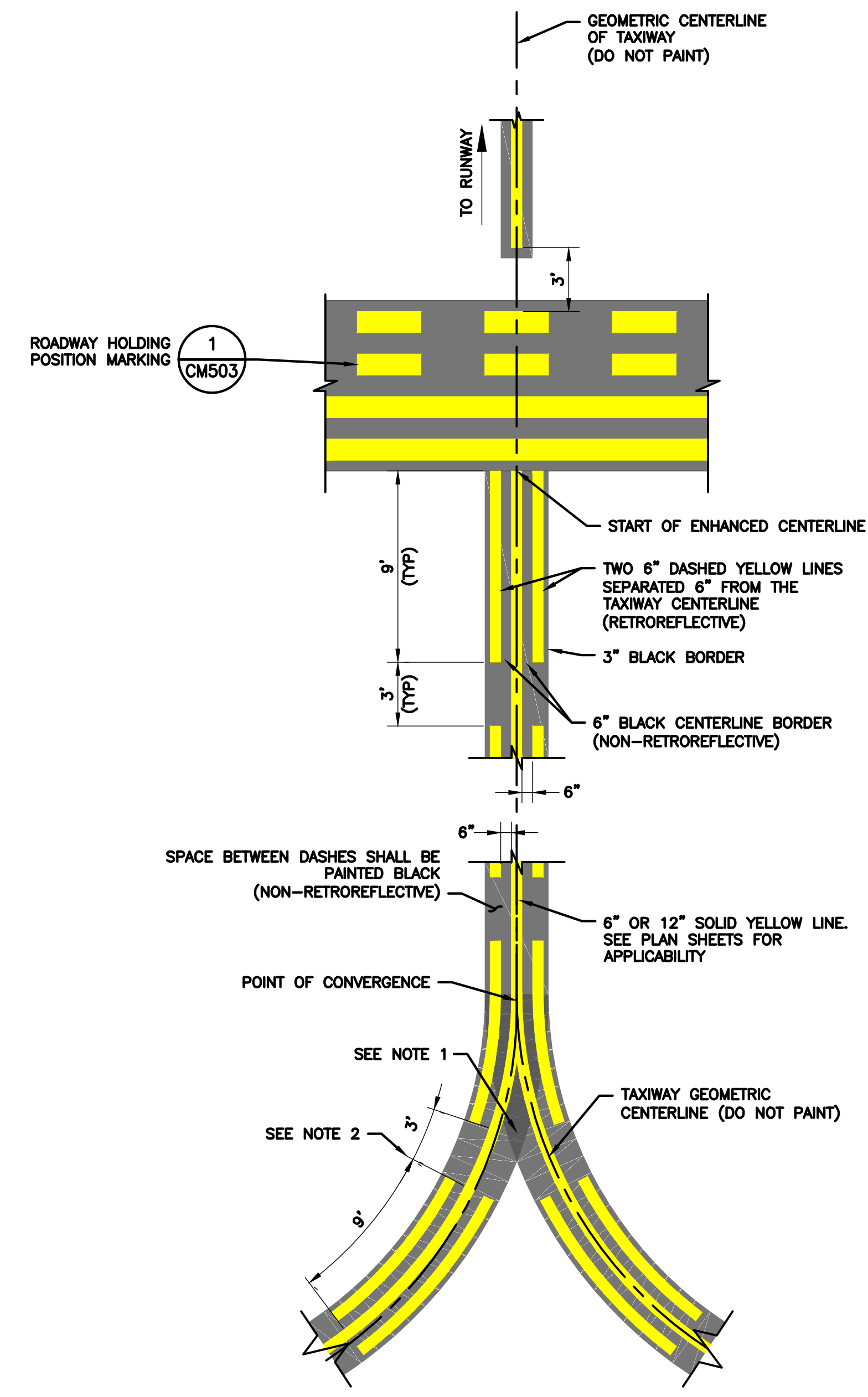
**1 RUNWAY HOLDING POSITION MARKING**  
 1" = 5'-0"

**ILS/MLS HOLDING POSITION MARKING NOTES:**

1. ILS HOLDING POSITION MARKINGS SHALL BE PAINTED USING PREFORMED THERMOPLASTIC, UNLESS OTHERWISE NOTED. SEE SPECIFICATION P-620 FOR CLARIFICATION



**2 ILS/MLS HOLDING POSITION MARKING**  
 1" = 5'-0"



**3 ENHANCED TAXIWAY CENTERLINE**  
 1" = 5'-0"

**ENHANCED TAXIWAY CENTERLINE NOTES:**

1. V-SHAPED INNER DASHES START AND STOP WITH THE OUTSIDE 9' DASHES. IF THE V-SHAPED DASHED ARE LESS THAN 5' AT THE POINT OF CONVERGENCE, THEY MAY BE OMITTED.
2. RADIAL ENHANCED CENTERLINE MEASUREMENTS ARE TAKEN ALONG THE TAXIWAY GEOMETRIC CENTERLINE STRIPE.
3. TAXIWAY CENTERLINES ARE ENHANCED WITH TWELVE 9'-0" LONG STRIPES WITH 3'-0" SPACES ENDING WITH ONE 6'-0" LONG STRIPE TOTALING 150'-0" PRIOR TO RUNWAY HOLD BAR.
4. IF THE ENHANCED CENTERLINE INTERSECTS ANOTHER TAXIWAY LOCATED WITHIN 150'-0" OF THE RUNWAY HOLD POSITION MARKING, THE ENHANCED CENTERLINE SHALL TERMINATE 5'-0" PRIOR TO THE POINT OF INTERSECTION. SEE DETAIL 2 FOR CLARIFICATION.
5. END OF THE LAST SEGMENT SHALL BE CAPPED WITH A 6" BLACK BORDER

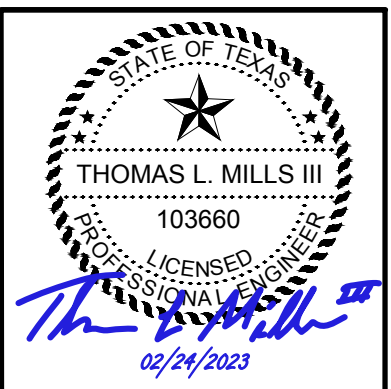


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FAA NON-STANDARD TAXIWAYS PROJECT

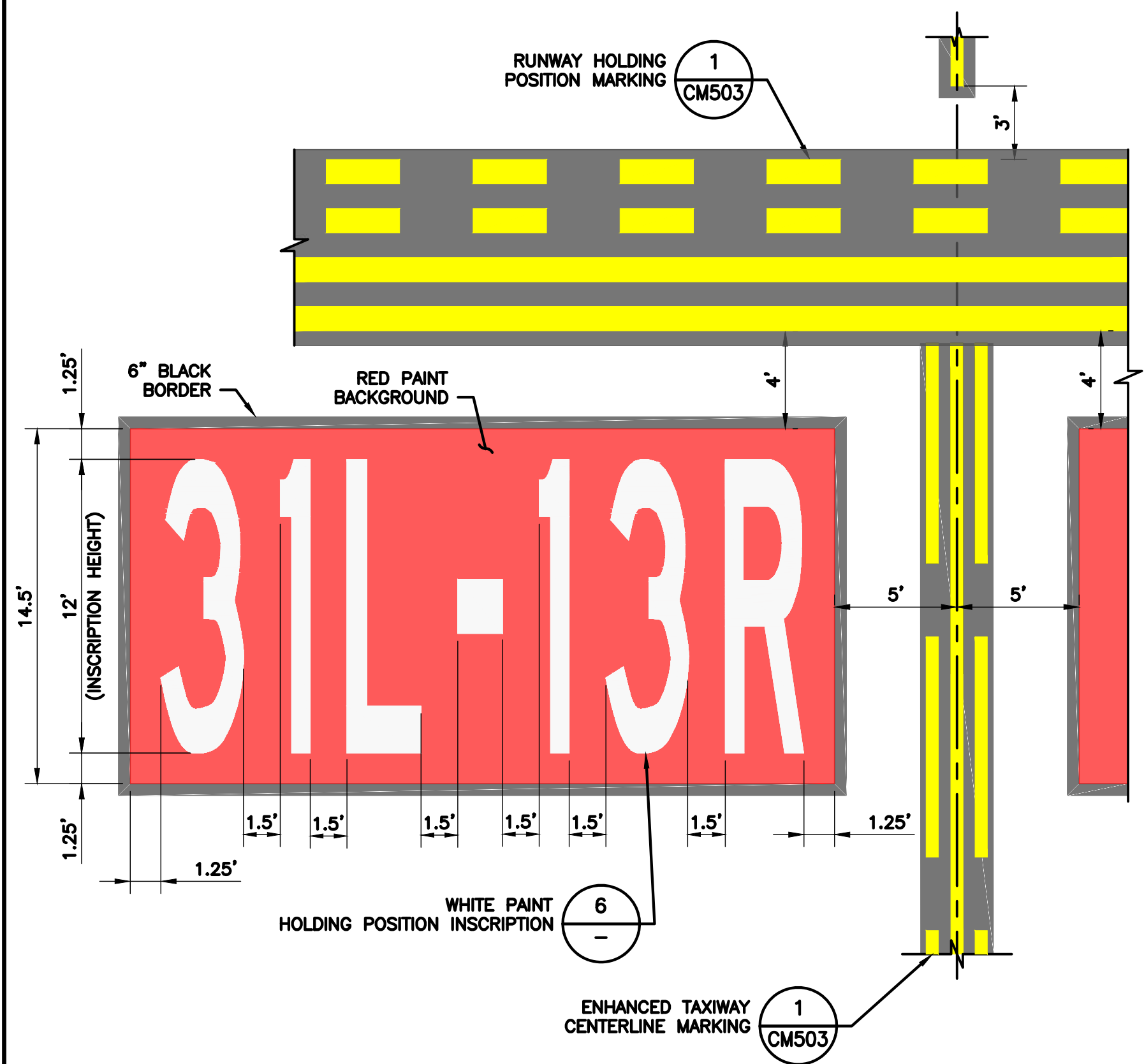
PAVEMENT MARKING DETAILS

PROJECT MGR:	S. CHILDERS
DESIGNER:	D. CRAWFORD
DRAWN BY:	B. BARTLETT
CHECKED BY:	R. EHTESHAM
SCALE:	AS SHOWN
DATE:	02/24/2023

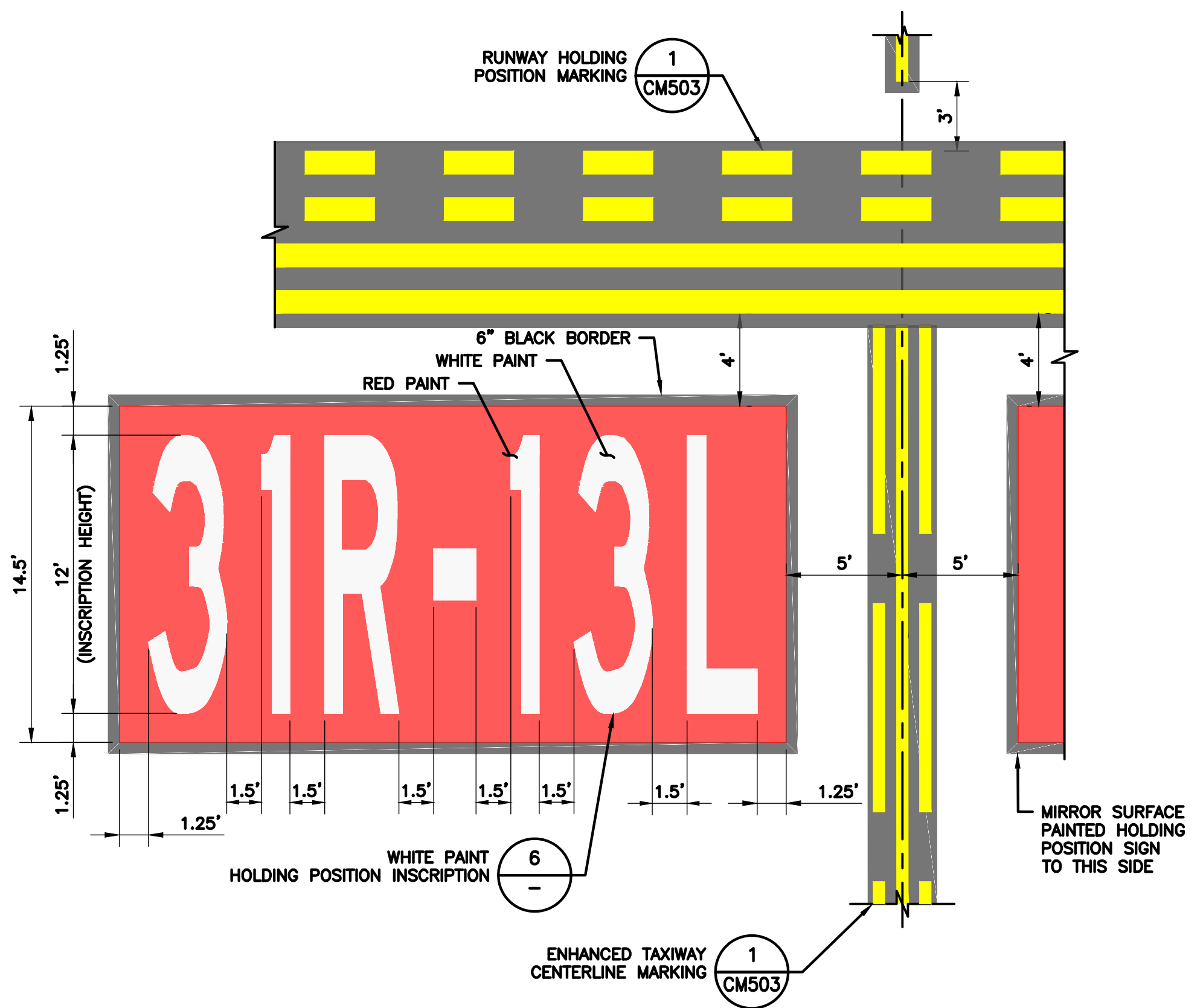


APPROVED BY: \_\_\_\_\_ DATE: \_\_\_\_\_

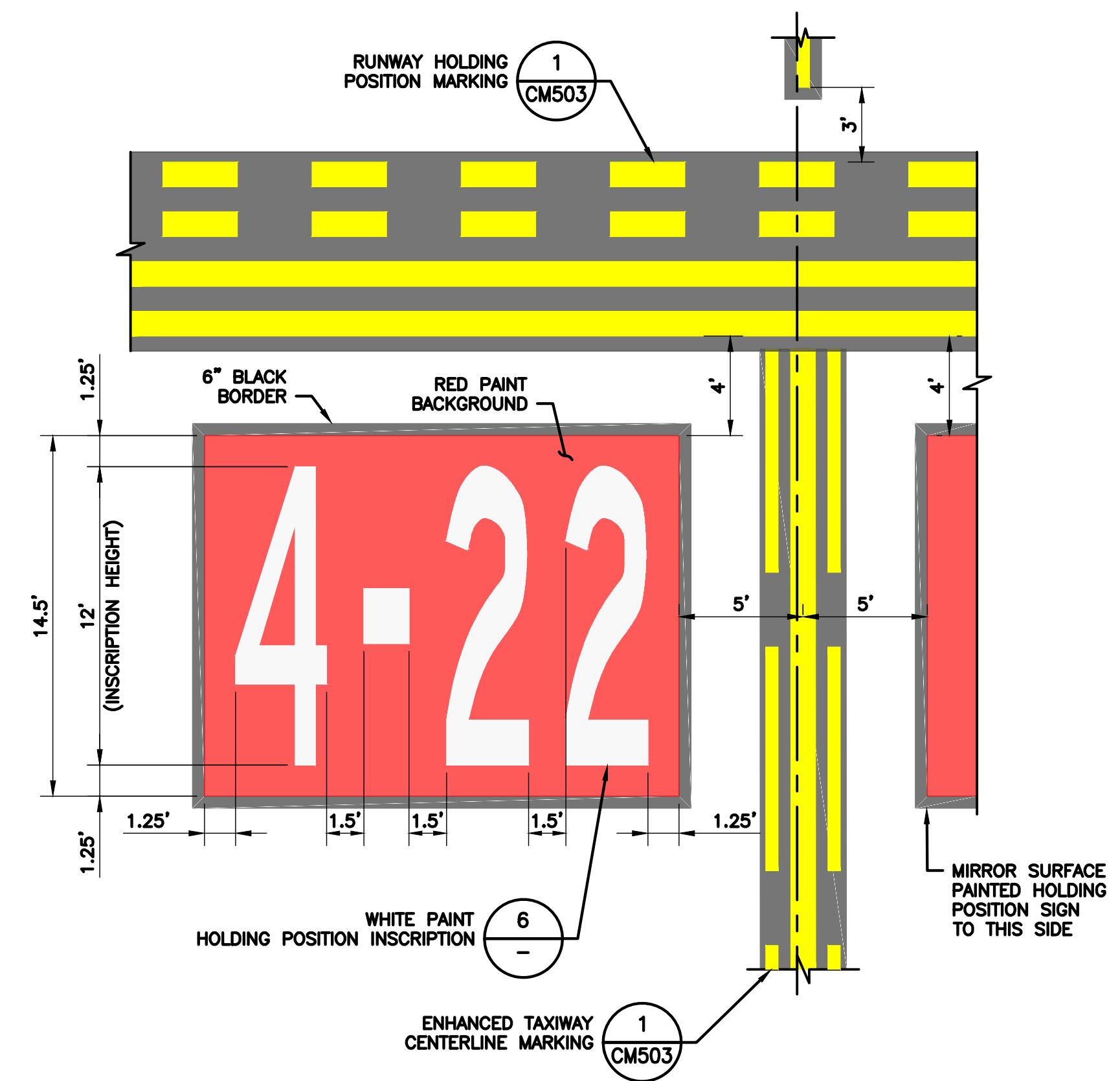
PROJECT NO:	770
C.I.P. NO:	3-48-0110-044
H.A.S. NO:	N/A
SHEET NO:	CM504



1 31L-13R SURFACE PAINTED HOLD SIGN  
1" = 5'-0"

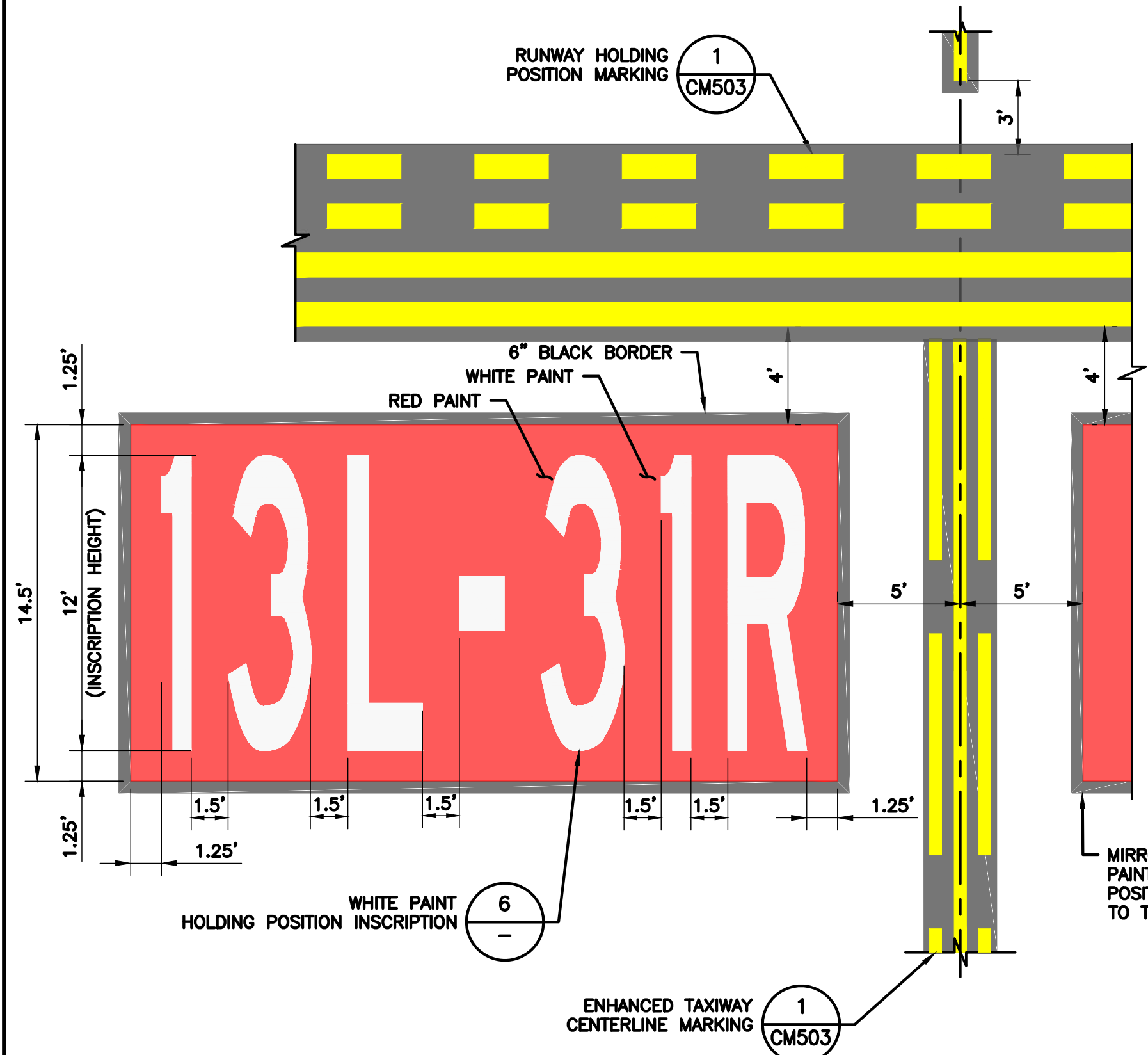


3 31R-13L SURFACE PAINTED HOLD SIGN  
1" = 5'-0"

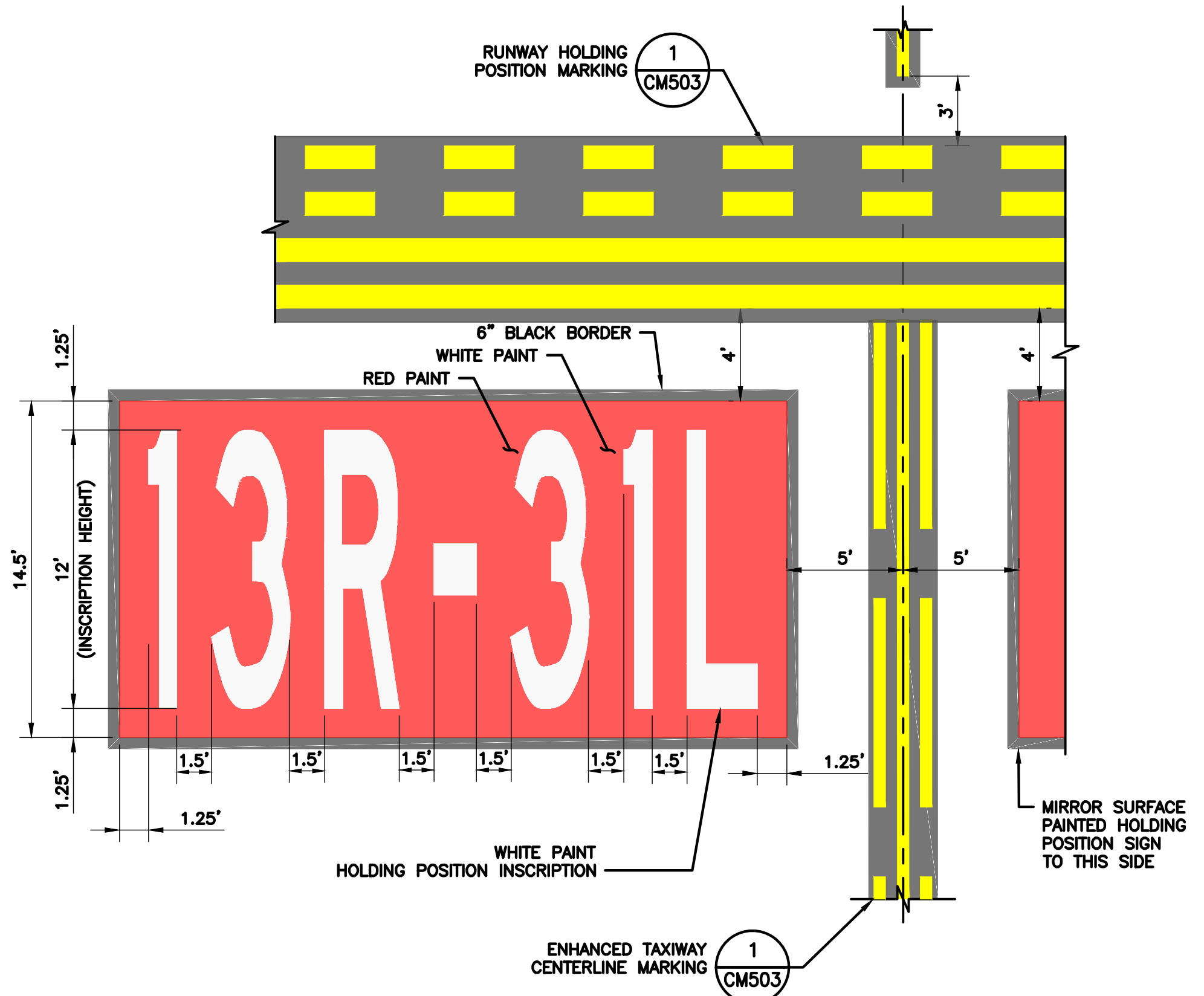


5 4-22 SURFACE PAINTED HOLD SIGN  
1" = 5'-0"

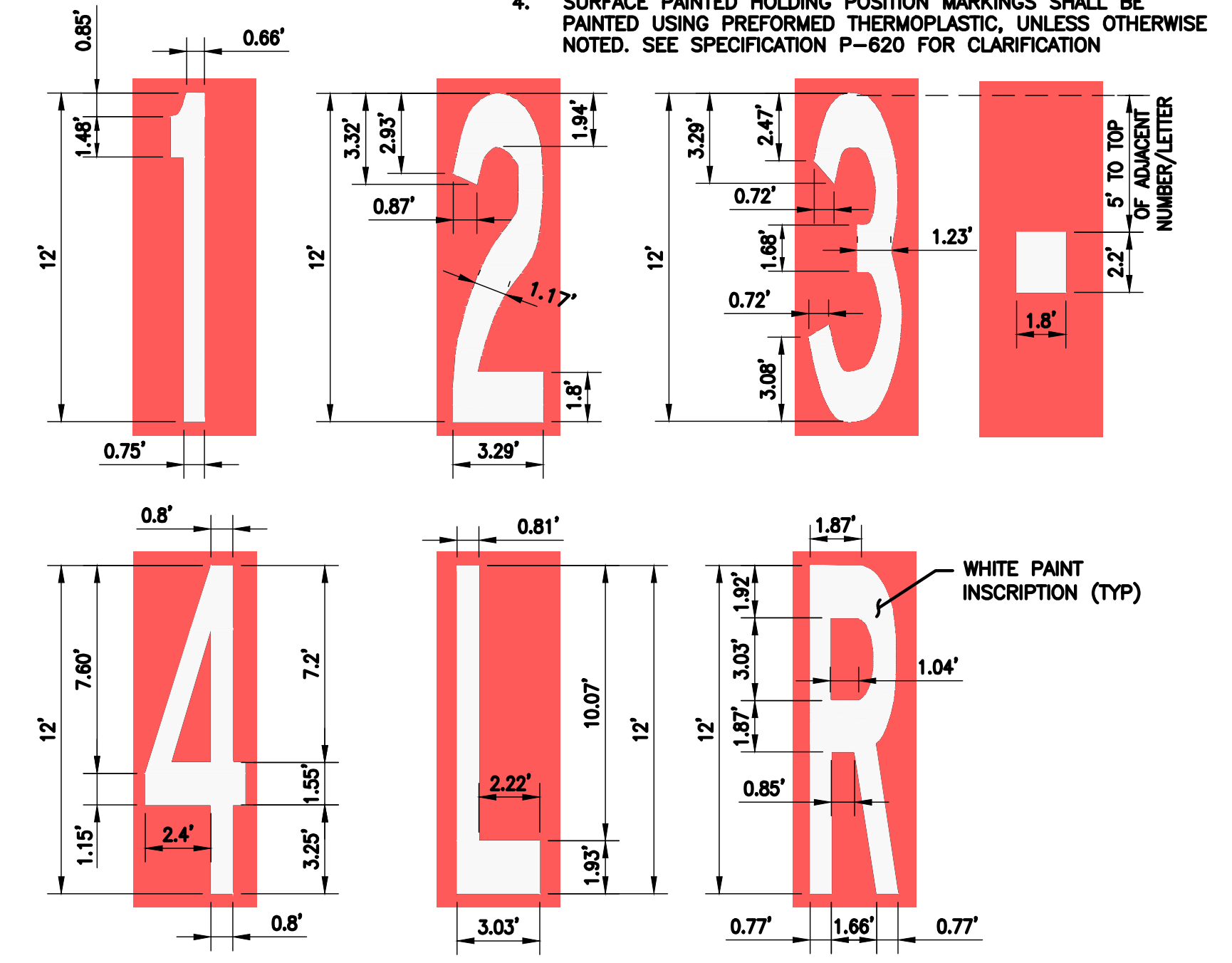
- SURFACE PAINTED HOLDING POSITION SIGN NOTES:**
1. SURFACE PAINTED HOLDING POSITION SIGN SHALL BE MARKED IN ACCORDANCE TO AC 150/5340-1M.
  2. FOR ALL SURFACE PAINTED LETTERING, USE STENCILS. THE CONTRACTOR SHALL PROVIDE THE PAVEMENT MARKING STENCILS TO THE AIRPORT AFTER PAVEMENT MARKINGS HAVE BEEN COMPLETED.
  3. BOTH RED AND WHITE PAINT SHALL RECEIVE REFLECTIVE MEDIA; HOWEVER RED SHALL BE AT A REDUCED RATE AS SHOWN IN SPECIFICATION P-620. THE 6" BLACK BORDER DOES NOT RECEIVE REFLECTIVE MEDIA.
  4. SURFACE PAINTED HOLDING POSITION MARKINGS SHALL BE PAINTED USING PREFORMED THERMOPLASTIC, UNLESS OTHERWISE NOTED. SEE SPECIFICATION P-620 FOR CLARIFICATION.



2 13L-31R SURFACE PAINTED HOLD SIGN  
1" = 5'-0"

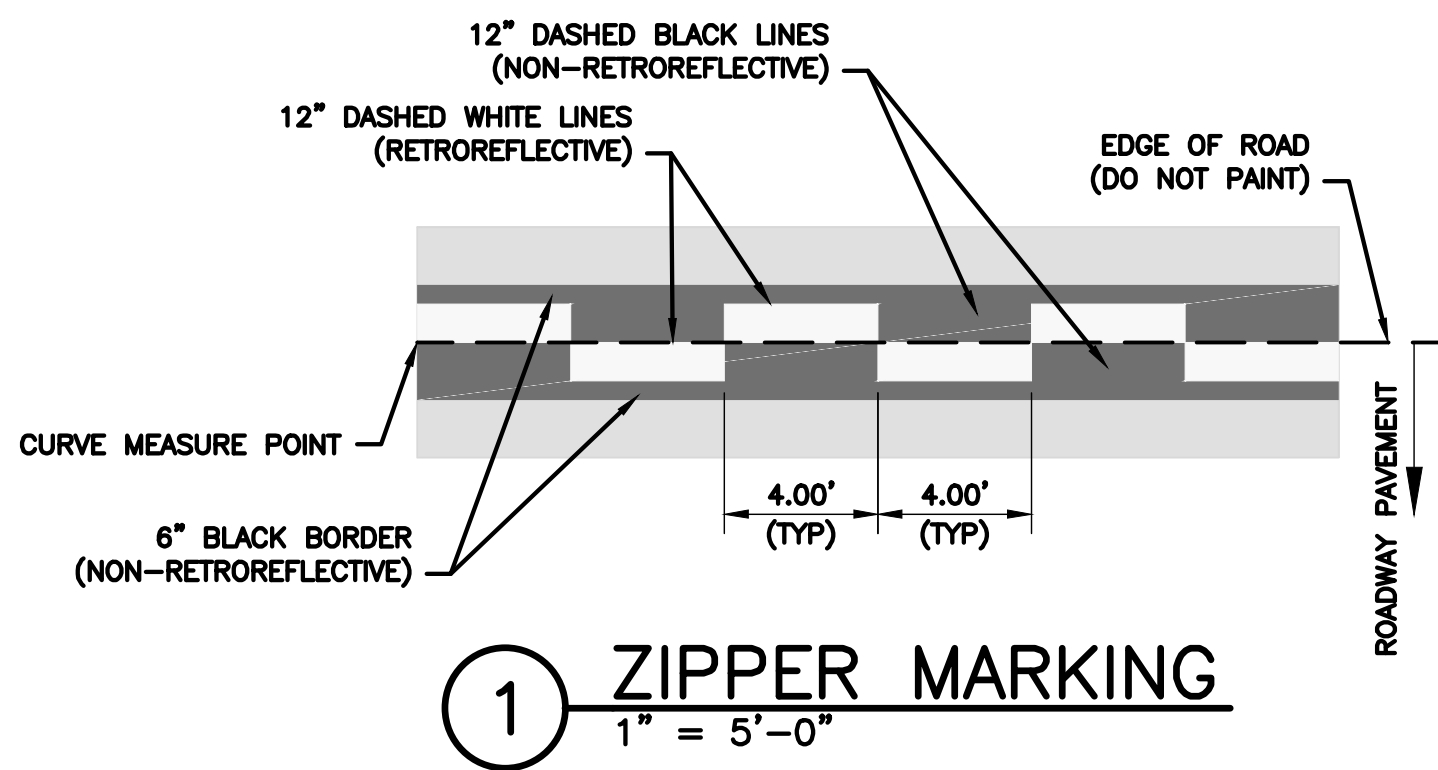


4 13R-31L SURFACE PAINTED HOLD SIGN  
1" = 5'-0"

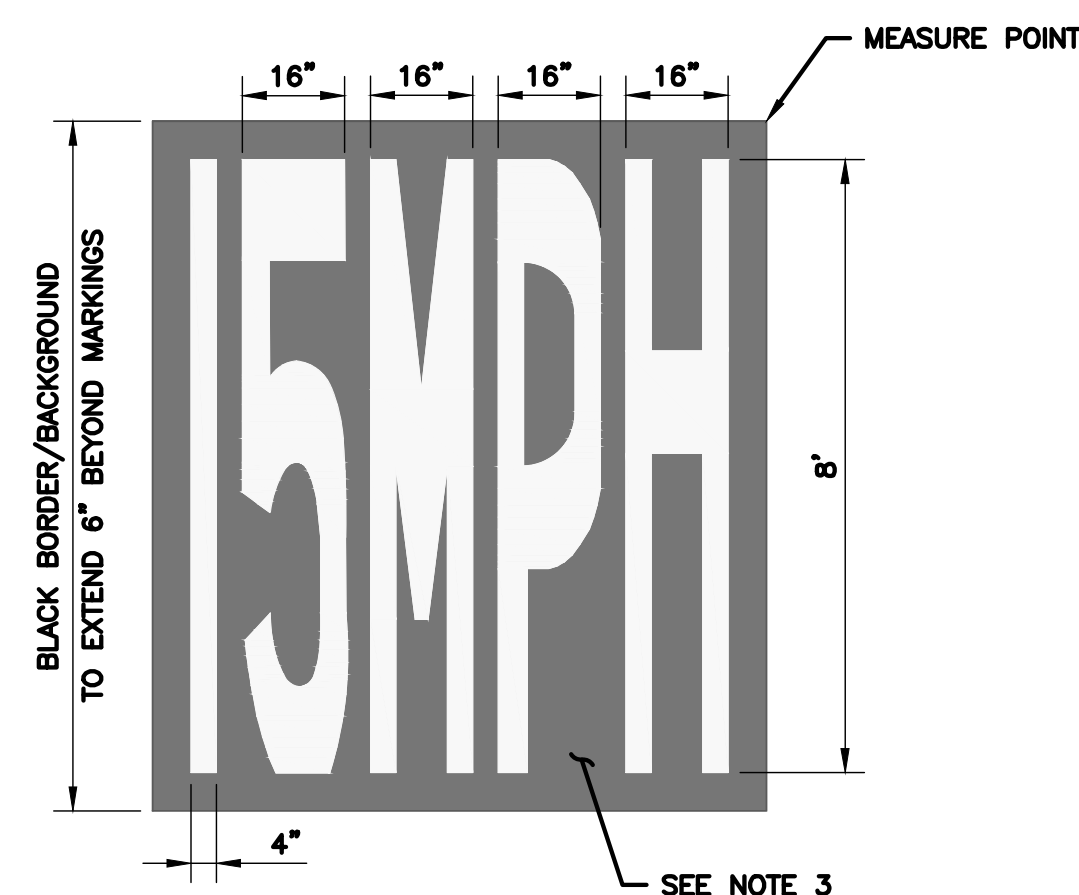


6 SURFACE PAINTED HOLDING POSITION INSCRIPTIONS  
1" = 5'-0"

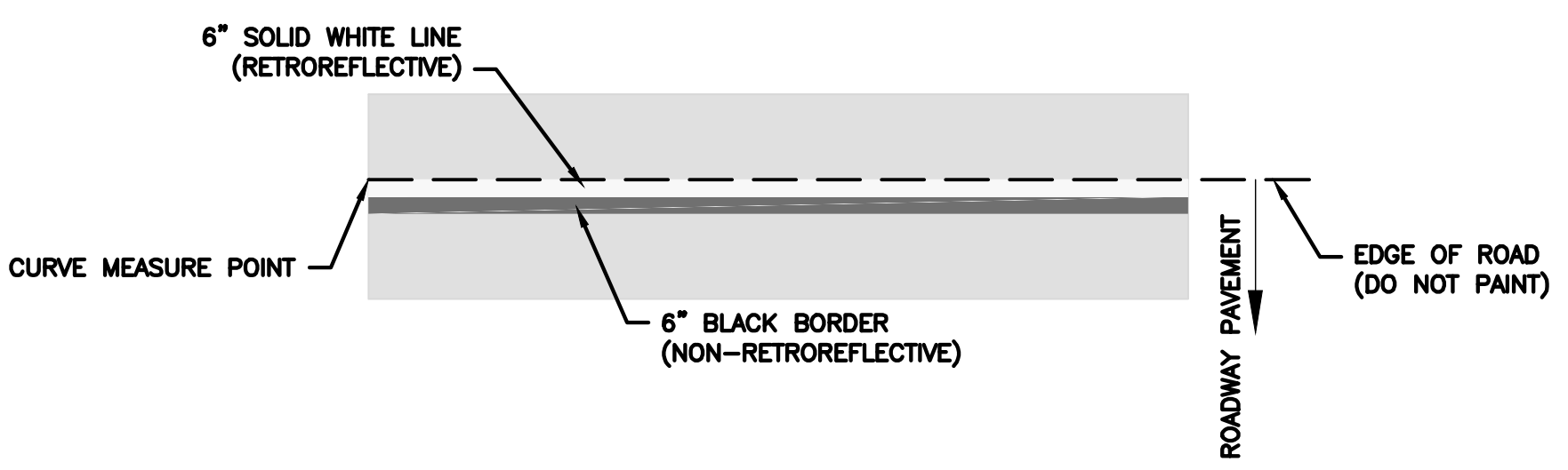




① ZIPPER MARKING  
1" = 5'-0"

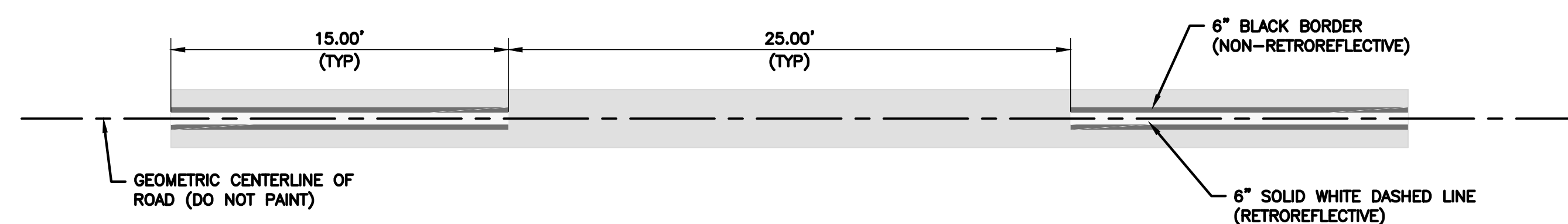


④ 15 MPH SPEED LIMIT MARKING  
1" = 5'-0"

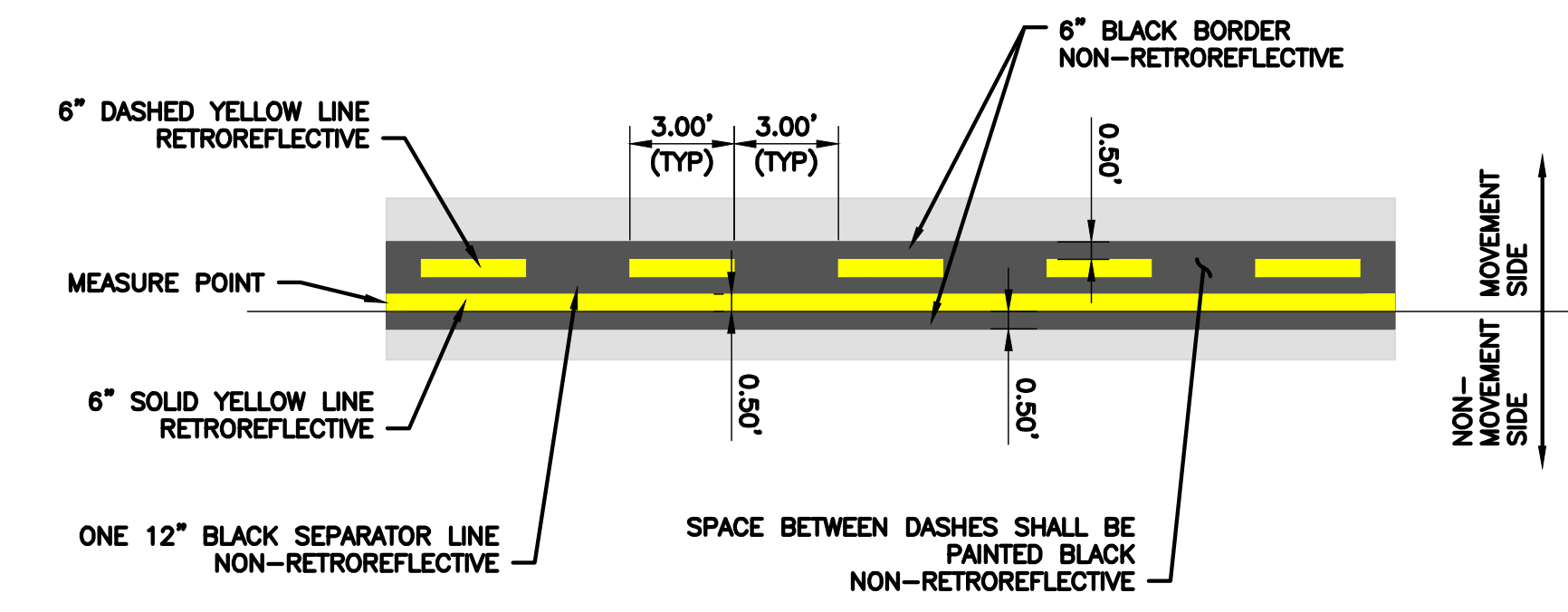


② ROADWAY EDGE MARKING  
1" = 5'-0"

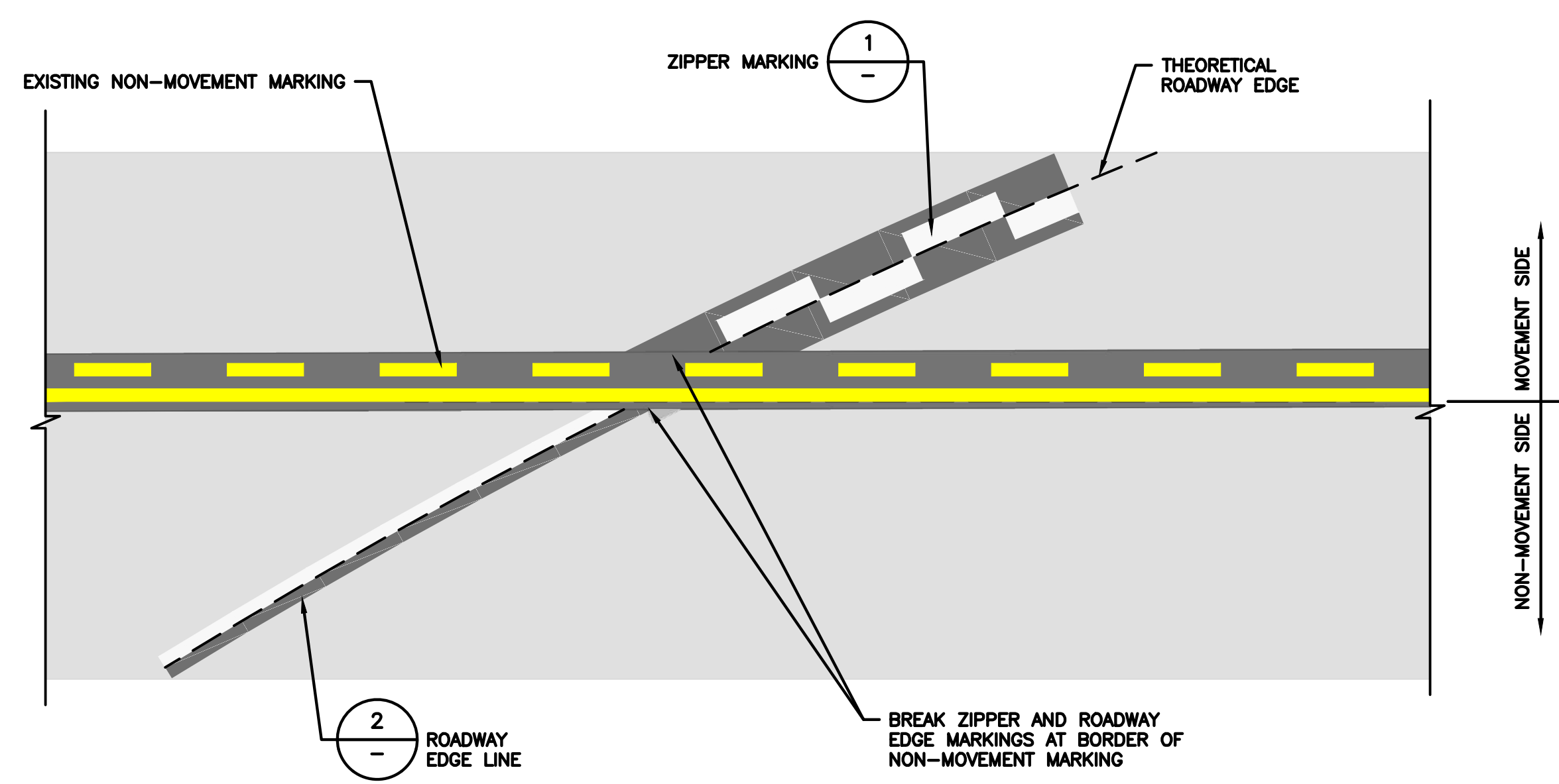
- ROADWAY MARKING NOTES:**
1. LETTERING SHALL CONFORM TO THE LATEST VERSION OF STANDARD ALPHABETS FOR HIGHWAY SIGNS AND PAVEMENT MARKINGS (AASHTO, A POLICY ON GEOMETRIC DESIGN OF HIGHWAYS AND STREETS).
  2. LETTERING SHALL BE CENTERED IN LANE.
  3. SPACES BETWEEN LETTERS OR NUMBERS SHALL BE 4".
  4. ALL LETTERS, ARROWS AND NUMBERS SHALL BE PAINTED WHITE ((RETROREFLECTIVE)).
  5. 6" BLACK OUTLINE OR BACKGROUND IS REQUIRED ON CONCRETE PAVEMENTS AND SHALL BE (NON-RETROREFLECTIVE) UNLESS OTHERWISE NOTED.



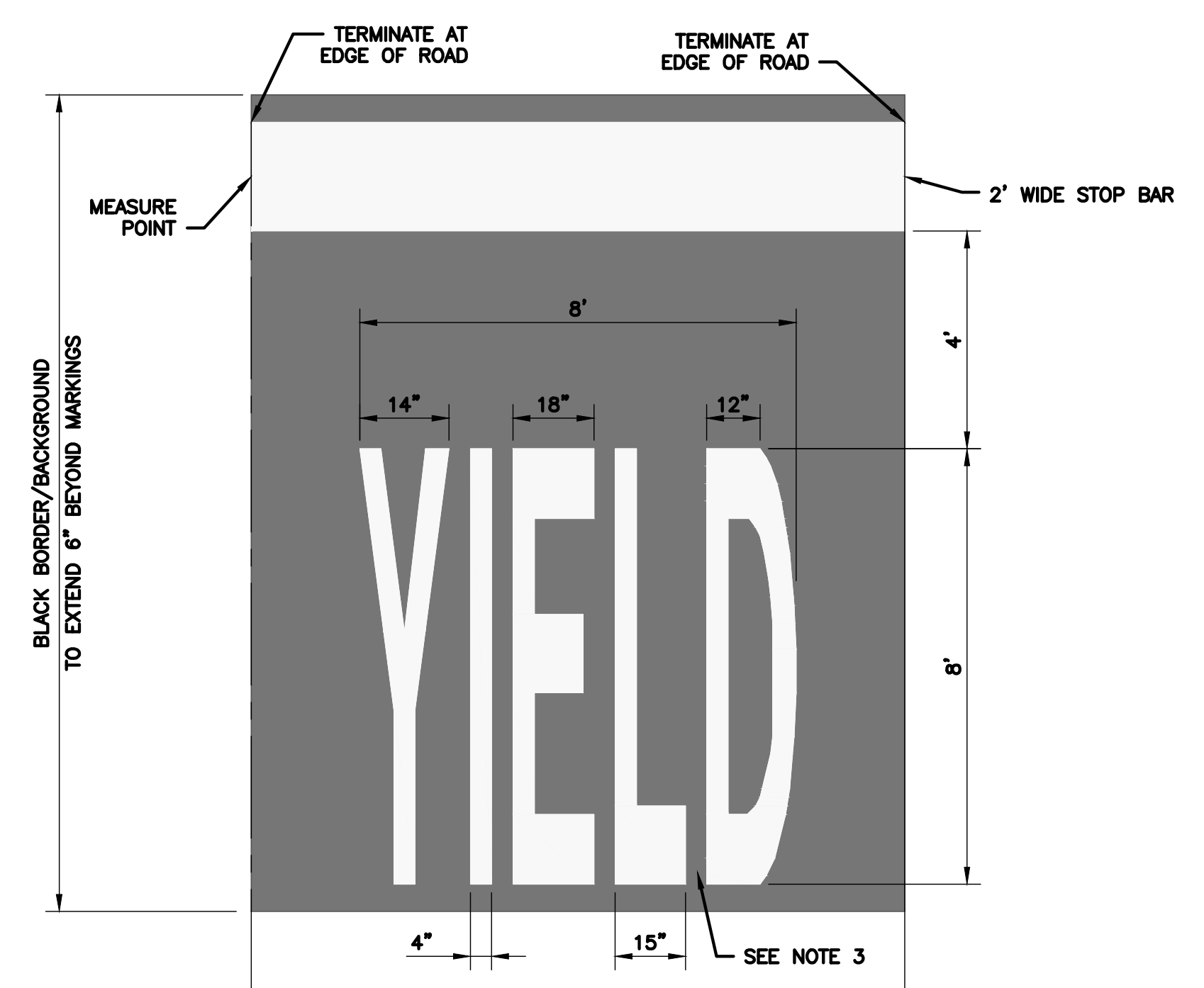
⑤ ROADWAY DASHED CENTERLINE MARKING  
1" = 5'-0"



⑥ NON-MOVEMENT AREA MARKING DETAIL  
1" = 5'-0"



③ ROADWAY EDGE/NON-MOVEMENT MARKING INTERSECTION  
1" = 5'-0"

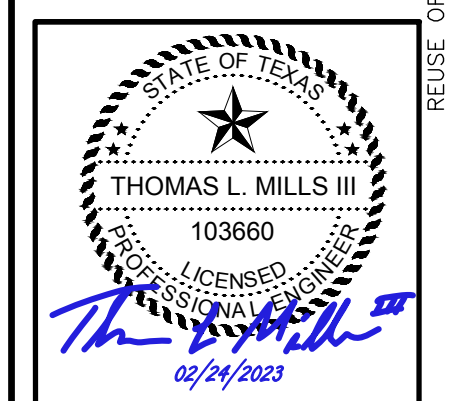


⑦ YIELD BAR MARKING DETAIL  
NTS

NO.	DESCRIPTION	DATE	BY
0	ISSUED FOR BID	02/24/2023	SC

FAA NON-STANDARD TAXIWAYS PROJECT  
 PAVEMENT MARKING DETAILS

PROJECT MGR:	S. CHILDERS
DESIGNER:	D. CRAWFORD
DRAWN BY:	B. BARTLETT
CHECKED BY:	R. EHTESHAM
SCALE:	AS SHOWN
DATE:	02/24/2023



APPROVED BY: \_\_\_\_\_ DATE: \_\_\_\_\_

PROJECT NO:	770
C.I.P. NO:	3-48-0110-044
H.A.S. NO:	N/A
SHEET NO:	CM505



NO.	DESCRIPTION	DATE	BY
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FAA NON-STANDARD TAXIWAYS PROJECT  
 PAVEMENT MARKING DETAILS

PROJECT MGR:  
 DESIGNER:  
 DRAWN BY:  
 CHECKED BY:  
 SCALE: AS SHOWN  
 DATE: 02/24/2023

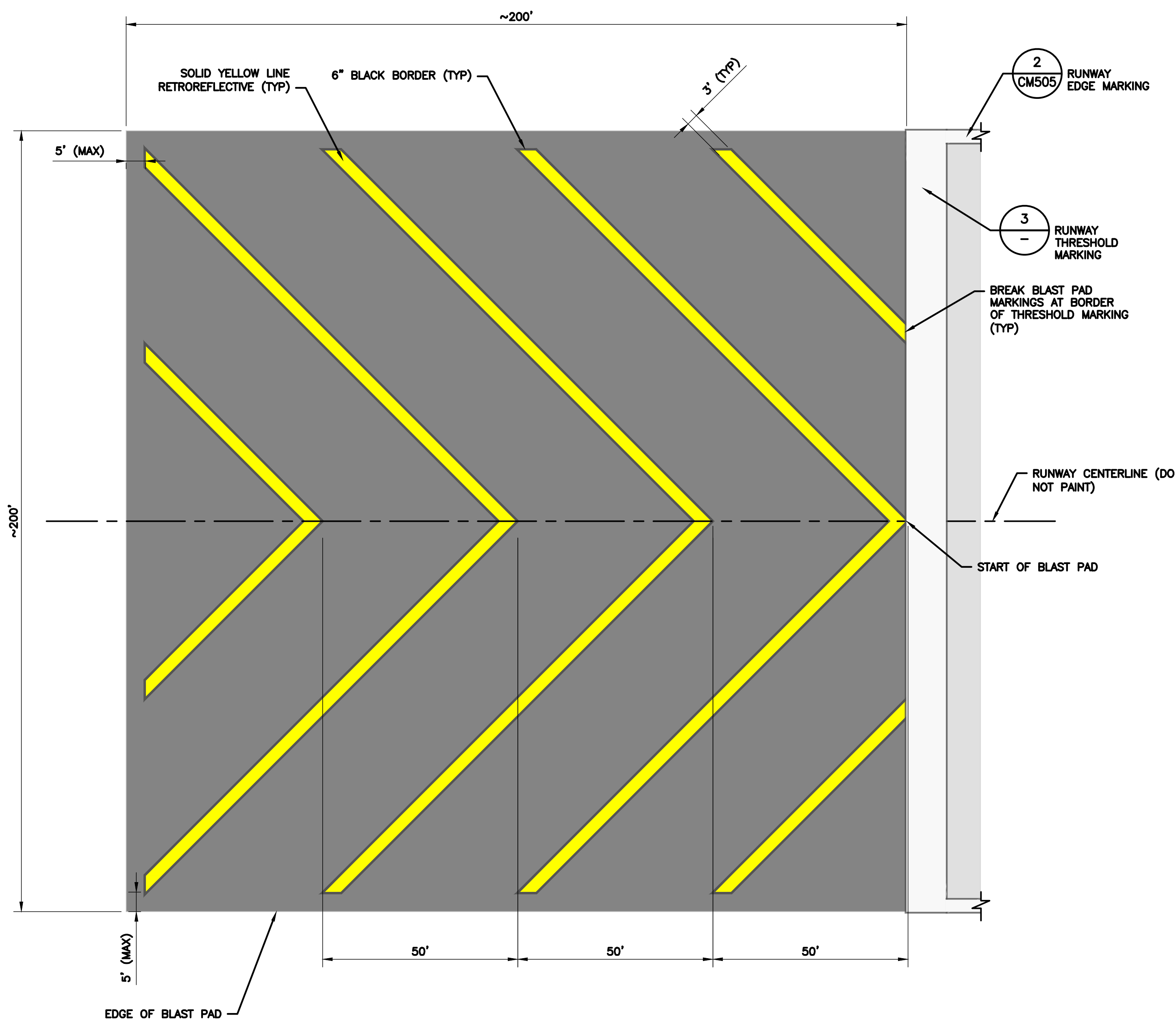


APPROVED BY: DATE:

DIRECTOR  
 HOUSTON AIRPORT SYSTEM

PROJECT NO: 770  
 C.I.P. NO: 3-48-0110-044  
 H.A.S. NO: N/A  
 SHEET NO:

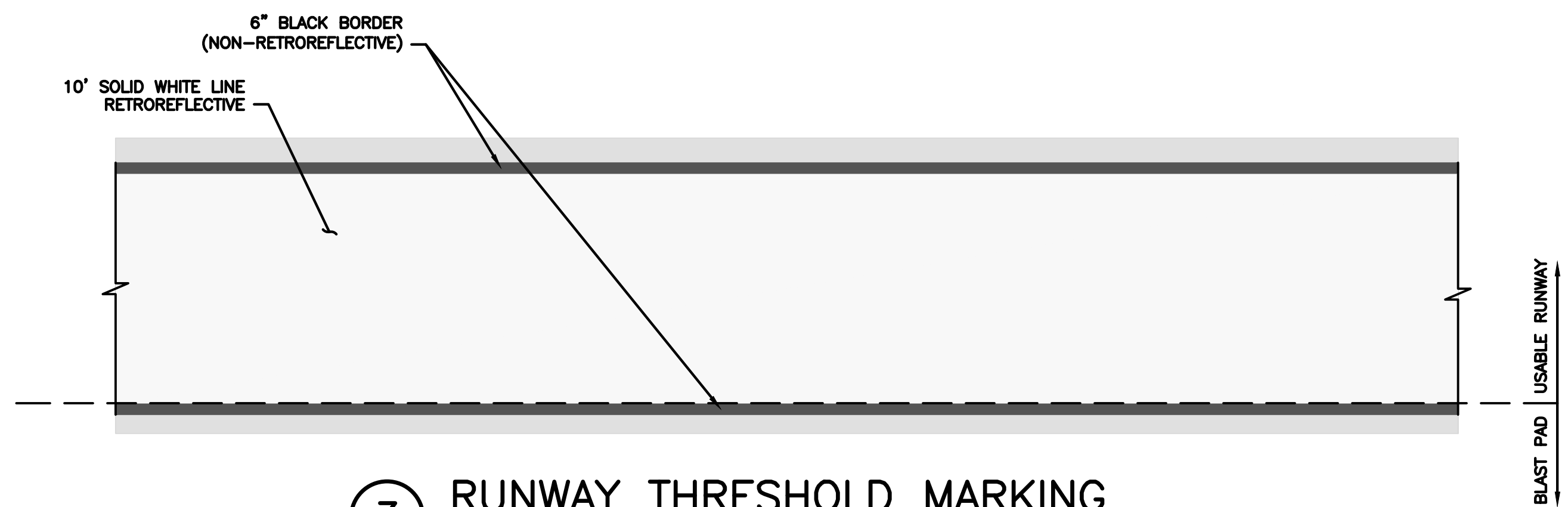
CM506  
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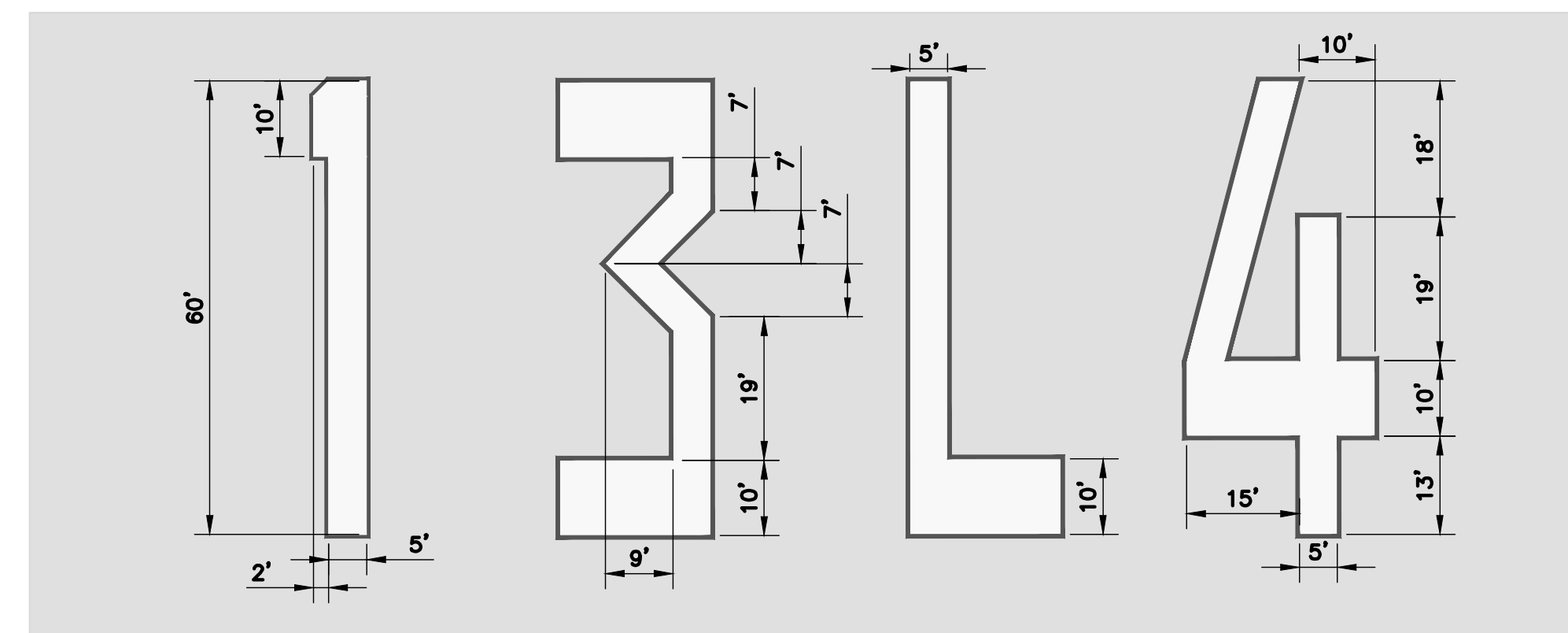
1 BLAST PAD MARKINGS  
 1" = 20'-0"



2 RUNWAY TOUCHDOWN MARKING  
 1" = 5'-0"



3 RUNWAY THRESHOLD MARKING  
 1" = 5'-0"



RUNWAY LETTERING AND NUMBERING NOTE:

- FOLLOW DIMENSIONS FROM FAA ADVISORY CIRCULAR 150-5340-1M APPENDIX B WHEN PAINTING LETTERS AND NUMBERS.
- ALL MARKINGS SHALL HAVE A 6" BLACK BORDER.

4 RUNWAY LETTERS AND NUMBERS  
 1" = 20'-0"